



HILLINGDON
LONDON



Major Applications Planning Committee

Date: WEDNESDAY, 6 AUGUST
2014

Time: 7.30 PM

Venue: COUNCIL CHAMBER
CIVIC CENTRE
HIGH STREET
UXBRIDGE
UB8 1UW

**Meeting
Details:** Members of the Public and
Press are welcome to attend
this meeting

To Councillors on the Committee

Eddie Lavery (Chairman)
John Hensley (Vice-Chairman)
Peter Curling
Jazz Dhillon
Janet Duncan (Labour Lead)
Ian Edwards
Henry Higgins
John Morgan
Brian Stead

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Published: Tuesday, 29 July 2014

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This Agenda is available online at:

<http://modgov.hillingdon.gov.uk/ieListDocuments.aspx?CIId=325&MIId=2009&Ver=4>

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Lloyd White
Head of Democratic Services
London Borough of Hillingdon,
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A useful guide for those attending Planning Committee meetings

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Petitions and Councillors

Petitions - Those who have organised a petition of 20 or more borough residents can speak at a Planning Committee in support of or against an application. Petitions must be submitted in writing to the Council in advance of the meeting. Where there is a petition opposing a planning application there is also the right for the applicant or their agent to address the meeting for up to 5 minutes.

Ward Councillors - There is a right for local councillors to speak at Planning Committees about applications in their Ward.

Committee Members - The planning committee is made up of the experienced Councillors who meet in public every three weeks to make decisions on applications.

How the Committee meeting works

The Planning Committees consider the most complex and controversial proposals for development or enforcement action.

Applications for smaller developments such as householder extensions are generally dealt with by the Council's planning officers under delegated powers.

An agenda is prepared for each meeting, which comprises reports on each application

Reports with petitions will normally be taken at the beginning of the meeting.

The procedure will be as follows:-

1. The Chairman will announce the report;
2. The Planning Officer will introduce it; with a presentation of plans and photographs;
3. If there is a petition(s), the petition organiser will speak, followed by the agent/applicant

followed by any Ward Councillors;

4. The Committee may ask questions of the petition organiser or of the agent/applicant;
5. The Committee debate the item and may seek clarification from officers;
6. The Committee will vote on the recommendation in the report, or on an alternative recommendation put forward by a Member of the Committee, which has been seconded.

About the Committee's decision

The Committee must make its decisions by having regard to legislation, policies laid down by National Government, by the Greater London Authority - under 'The London Plan' and Hillingdon's own planning policies as contained in the 'Unitary Development Plan 1998' and supporting guidance. The Committee must also make its decision based on material planning considerations and case law and material presented to it at the meeting in the officer's report and any representations received.

Guidance on how Members of the Committee must conduct themselves when dealing with planning matters and when making their decisions is contained in the 'Planning Code of Conduct', which is part of the Council's Constitution.

When making their decision, the Committee cannot take into account issues which are not planning considerations such as the effect of a development upon the value of surrounding properties, nor the loss of a view (which in itself is not sufficient ground for refusal of permission), nor a subjective opinion relating to the design of the property. When making a decision to refuse an application, the Committee will be asked to provide detailed reasons for refusal based on material planning considerations.

If a decision is made to refuse an application, the applicant has the right of appeal against the decision. A Planning Inspector appointed by the Government will then consider the appeal. There is no third party right of appeal, although a third party can apply to the High Court for Judicial Review, which must be done within 3 months of the date of the decision.

Agenda

CHAIRMAN'S ANNOUNCEMENTS

- 1 Apologies for Absence
- 2 Declarations of Interest in matters coming before this meeting
- 3 To sign and receive the minutes of the previous meeting
- 4 Matters that have been notified in advance or urgent
- 5 To confirm that the items marked in Part 1 will be considered in public and those items marked in Part 2 will be heard in private

PART I - Members, Public and Press

Items are normally marked in the order that they will be considered, though the Chairman may vary this. The name of the local ward area is also given in addition to the address of the premises or land concerned.

Major Applications with a Petition

	Address	Ward	Description & Recommendation	Page
6	Land to the West of Laurel Lane, West Drayton 70019/APP/2014/1807	West Drayton	New 2 storey Junior School (5 forms of entry) including new vehicular and pedestrian accesses, alterations to an existing footpath, creation of pick-up/drop-off area, associated car parking, landscaping, playground, provision of a Multi-Use Games Area (MUGA) and ancillary development. Recommendation : Approval subject to a S106 Agreement	1 - 52 176 -189

7	Former RAF Eastcote, Lime Grove, Eastcote 10189/APP/2014/1842	Eastcote & East Ruislip	Section 73 Amendment Application to vary the internal layout of the Community Centre, to retain pillars and subdivide the approved open plan layout (varying condition 11 of permission ref: 10189/APP/2007/3383 which approved the re-development of the former RAF Eastcote site). Recommendation: Refusal	53 - 66 190 - 193
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Major Applications without a Petition

	Address	Ward	Description & Recommendation	Page
8	Enterprise House, 133 Blyth Road, Hayes 11623/APP/2013/3606	Botwell	Erection of extensions at roof level, erection of external bridge links on the rear elevation and internal works associated with the change of use of part of enterprise house to create 96 residential units (class C3) and associated car parking, retention of approximately 4,500 sqm (GIA) of employment use (Use classes B1 (a-c) and B2 with ancillary cafe) at ground and first floor levels and cafe. Recommendation : Approval subject to a S106 Agreement	67 - 120 194 - 205
9	Enterprise House, 133 Blyth Road, Hayes 11623/APP/2013/3592	Botwell	Erection of extensions at roof level, erection of external bridge links on the rear elevation and internal works associated with the change of use of part of enterprise house to create 96 residential units (class C3) and associated car parking, retention of approximately 4,500 sqm (GIA) of employment use (Class B1 and B8) at ground and first floor levels and cafe. (Application for Listed Building Consent). Recommendation : Approval	121 - 128 206 - 220

10	5	Station Road, West Drayton 65480/APP/2014/1018	West Drayton	Demolition of existing public house and erection of 38 flats and 237sqm of retail floorspace with parking, landscaping and amenity space. Recommendation : Approval subject to a S106 Agreement	129 - 174 221 - 240
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PART II - Members Only

The reports listed below are not made public because they contain confidential or exempt information under paragraph 6 of Part 1 of Schedule 12 A to the Local Government (Access to Information) Act 1985 as amended.

PART I - Plans for Major Applications Planning Committee

Pages - 175-240

Report of the Head of Planning, Sport and Green Spaces

Address LAND TO THE WEST OF LAUREL LANE WEST DRAYTON

Development: New 2 storey Junior School (5 forms of entry) including new vehicular and pedestrian accesses, alterations to an existing footpath, creation of pick-up/drop-off area, associated car parking, landscaping, playground, provision of a Multi-Use Games Area (MUGA) and ancillary development.

LBH Ref Nos: 70019/APP/2014/1807

Drawing Nos: LLD692-01_LaurelLanePrimarySchool_Masterplan.Rev05
PL001 C- Design and Access Statement
PL003 - Location and Site Plan
PL004 - Topographical and Tree Survey
PL005 D- Proposed Site Layout
PL006 - Tree Retention Plan
PL007 - Proposed Ground Floor Plan
PL008 D- Proposed First Floor Plan
009 - Proposed Roof Plan
PL010 - Proposed Elevations
PL010 - Proposed Elevations
PL011 - Proposed Street Elevations
PL012 - Proposed Sections
PL013 - Proposed Site Sections
PL110 Planning Statement Rev.A
114 Transport Assessment Part 1
PL114 Transport Assessment Part 2
PL115 Energy Strategy
PL116 BREEAM Pre-Assessment
PL115 Energy Strategy
PL120 Aboricultural Report
PL121 Ecology Report
PL122-C Landscape Strategy Report
PL123 Environment Noise Assessment
Aboricultural Impact Assessment
PL127 Ventilation and Air Extraction Statement
PL128 Lighting Assessment reduced
PL117 Flood Risk Assessment.
PL118 Drainage Strategy part 1
PL118 Drainage Strategy part2

Date Plans Received: 27/05/2014 **Date(s) of Amendment(s):**

Date Application Valid: 30/05/2014

1. SUMMARY

The proposal seeks full planning permission for the erection of a new five form of entry junior school to occupy an irregularly shaped plot located at the western side of Laurel Lane. The school building and grounds will occupy an area of 1.23 hectares. The proposal would encompass the provision of hard and soft playspace for the children, extensive landscaping works and the provision of associated car parking, including a

drop off/pick up facility.

The Education Act 1996 states that Local Authorities have a duty to educate children within their administrative area. The Hillingdon Primary Capital Schools Programme is part of the Council's legal requirement to meet the educational needs of the borough. In recent years the borough has seen a rise in birth rates. This growth in the birth rate, combined with net immigration and new large scale housing developments in the borough has meant that there is now a significant need for additional primary school classrooms across the borough and the need for a new primary school in West Drayton.

The proposal would result in the loss of a portion of open space. However, the increased demand for school places is compelling and well evidenced and it has been demonstrated that there are no alternative sites which could reasonably be utilised to meet this need and on balance this need outweighs the harm which would arise from the development in other respects, including through loss of open space.

Nevertheless, the layout would reflect the established suburban character of the townscape context to the site and the design of the school building minimises the impacts of the development and would sit comfortably within its surrounding context. Landscaping has been incorporated within the application site and contributions were offered towards tree planting and landscaping enhancements in the adjacent Closes Recreational Grounds in an attempt to mitigate the impact of the loss of open space.

In relation to ecology the application is supported by detailed ecological information and aspects of the development, such as the green roof, swales and extensive landscaping, would serve to mitigate the loss of habitat which would occur. Subject to conditions the proposal would not be unacceptable in terms of ecological impacts.

The proposal is supported by a detailed Transport Assessment and provides for a combination of physical and management measures to reduce its impact on the public highway. The Council's Highways Engineers have reviewed this information in detail and conclude that, subject to conditions and a legal agreement, the proposed development would be acceptable in terms of traffic impact, pedestrian and highway safety.

The school is well separated from surrounding residential properties and subject to appropriate conditions it would have no unacceptable impacts on the amenity of neighbouring residential occupiers by way of dominance, loss of light, loss of privacy or noise.

Subject to conditions, the development has also been designed to fully accord with all requirements relating to accessible design, sustainable design and flood risk.

In conclusion, the benefits of providing a school in this location outweigh the harm which would arise from the development in other respects, including through loss of open space. Having regard to this educational need, the development has been well designed to minimise its impacts and would comply with all other relevant planning policies.

The application is recommended for approval, subject to the conditions and statement of intent offering a monetary contribution towards tree planting and landscaping improvement at the Closes Recreational Grounds and a non-monetary contribution towards highway studies and highway improvements.

2. RECOMMENDATION

That delegated powers be given to the Head of Planning, Green Spaces and

Culture to grant planning permission, subject to the following:

A. That the Council enter into a legal agreement with the applicant under Section 106 of the Town and Country Planning Act 1990 (as amended) or any other legislation to secure the following:

1. **Tree Planting and Landscape Improvements to the Closes Recreational Grounds:** A contribution in the sum of £5,000.00 is sought.

2. **Study and Road Improvement Works:** A study to be undertaken at 50% occupation of the junior school. The studies to examine actual highway conditions at the junction of Laurel Lane with Harmondsworth Road, the full scope of work to be agreed by the Council. Within 6 months of the studies, the applicant is required to identify, agree and implement appropriate remediation measures (if any), which shall be first agreed with the Local Planning Authority. The studies and identified mitigation works shall be undertaken and funded by the developer.

3. The Applicant shall use best endeavours to procure an extension to the existing 20 MPH zone along Laurel Lane by submitting an application to the Local Highways Authority. The Applicant shall not occupy or permit or cause to be occupied any part of the development until such application has been submitted and approved by the Local Authority. The studies and identified works shall be undertaken and funded by the Applicant. The Applicant shall be responsible for the full costs of the highways works including the studies and any traffic orders the Local Highways Authority may require to implement the highways works whether or not such orders are successfully made.

4. **Project Management and Monitoring Sum:** a contribution equal to 5% of the total cash contributions secured to enable the management and monitoring of the resulting agreement.

B. That the applicant meets the Council's reasonable costs in the preparation of the Section 106 agreement and any abortive work as a result of the agreement not being completed.

C. That the officers be authorised to negotiate the terms of the proposed agreement.

D. That, if the S106 agreement has not been finalised within 6 months, under the discretion of the Head of Planning, Green Spaces and Culture, the application is refused under delegated powers on the basis that the applicant has refused to address planning obligation requirements.

E. That if the application is approved, the following conditions be attached:

1 SP01 **Council Application Standard Paragraph**

This authority is given by the issuing of this notice under Regulation 3 of the Town and Country Planning General Regulations 1992 and shall enure only for the benefit of the land.

2 COM3 **Time Limit**

The development hereby permitted shall be begun before the expiration of three years

from the date of this permission.

REASON

To comply with Section 91 of the Town and Country Planning Act 1990.

3 COM4 Accordance with Approved Plans

The development hereby permitted shall not be carried out except in complete accordance with the details shown on the submitted plans, numbers PL003-B, PL005-D, PL006-B, PL007-C, PL008-D, PL009-C, PL010-C, PL011-A, PL012-C, PL0013-B and LLD692/01 Rev.07 and shall thereafter be retained/maintained for as long as the development remains in existence.

REASON

To ensure the development complies with the provisions Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and the London Plan (July 2011).

4 COM5 General compliance with supporting documentation

The development hereby permitted shall not be occupied until the following has been completed in accordance with the specified supporting plans and/or documents:

- (1) PL115 Rev-B Energy Strategy Recommendations - Energy Strategy 23 May 2014
- (2) PL121 Rev-B Recommendations Habitat Survey Phase I 20 May 2014
- (3) PL122-C Recommendations Landscaping Strategy Report 18 July 2014
- (4) PL123 Rev-A Sound insulation measures Environment Noise Assessment 06 May 2014
- (5) PL124 Rev-A Protection of Retained Trees - Arboricultural Impact Assessment 23 May 2014
- (6) PL127 Rev-A Measures in the Ventilation & Air Extraction Statement 23 May 2014
- (7) PL128 Rev-A Recommended Lighting Cover Area - Lighting Assessment 27 May 2014
- (8) Mitigation Measures in the Air Quality Report by Air Quality Consultants J2026/2/F1
- (9) Mitigations Measures in Robert West Transport Assessment ref. 3249/001/1201A June 2014

Thereafter the development shall be retained/maintained in accordance with these details for as long as the development remains in existence

REASON

To ensure that the development complies with the objectives of Policies PT1.HE1, BE38, OE1, OE5, R10, R16 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and policies 3.1, 3.8 and Chapters 6 and 7 of the London Plan (2011).

5 COM7 Materials

Within 3 months from the date of this permission the details of all materials and external surfaces have been submitted to and approved in writing by the Local Planning Authority. Unless otherwise agreed in writing, thereafter the development shall be constructed in accordance with the approved details and be retained as such.

Details should include information relating to make, product/type, colour and photographs/images.

REASON

To ensure that the development presents a satisfactory appearance in accordance with

6 COM9 Landscaping (car parking & refuse/cycle storage)

Within 3 months from the date of this permission a landscape scheme shall be submitted in writing to the Local Planning Authority. The building shall not be occupied until the approved scheme has been implemented as approved by the Local Planning Authority. Unless otherwise agreed in writing the scheme shall include: -

1. Details of Soft Landscaping
 - 1.a Planting plans (at not less than a scale of 1:100),
 - 1.b Written specification of planting and cultivation works to be undertaken,
 - 1.c Schedule of plants giving species, plant sizes, and proposed numbers/densities where appropriate

2. Details of Hard Landscaping
 - 2.a Refuse Storage
 - 2.b Cycle Storage
 - 2.c Means of enclosure/boundary treatments
 - 2.d Car Parking Layouts for 55 vehicles (including demonstration that 12 of all parking spaces located within the staff car park are served by electrical charging points)
 - 2.e Hard Surfacing Materials
 - 2.f External Lighting
 - 2.g Other structures (such as play equipment and furniture), which shall include details of bat and bird boxes across the site along with a habitat wall for educational purposes.

3. Living Walls and Roofs
 - 3.a Details of the inclusion of roofs which shall include nectar rich planting in a mix of no less than 30% in line with the recommendations in the ecology report
 - 3.b Justification as to why no part of the development can include living walls and roofs

4. Details of Landscape Maintenance
 - 4.a Landscape Maintenance Schedule for a minimum period of 5 years.
 - 4.b Proposals for the replacement of any tree, shrub, or area of surfacing/seeding within the landscaping scheme which dies or in the opinion of the Local Planning Authority becomes seriously damaged or diseased.

5. Schedule for Implementation

6. Other
 - 6.a Existing and proposed functional services above and below ground
 - 6.b Proposed finishing levels or contours

Thereafter the development shall be carried out and maintained in full accordance with the approved details.

REASON

To ensure that the proposed development will preserve and enhance the visual amenities of the locality and provide adequate facilities in compliance with policies BE13, BE38 and AM14 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and Policies 5.11 (living walls and roofs) and 5.17 (refuse storage) of the London Plan (July 2011)

7 COM8 Tree Protection

No site clearance or construction works shall take place until all the tree protection measures specified in the Patrick Stileman's Arb Reports PL120 and PL125 have been fully implemented. The tree protection measures shall be retained in position until the development is completed and, unless otherwise agreed in writing, the areas within the protective fencing shall remain undisturbed during the course of the works and in particular in these areas:

- 1.a There shall be no changes in ground levels;
- 1.b No materials or plant shall be stored;
- 1.c No buildings or temporary buildings shall be erected or stationed;
- 1.d No materials or waste shall be burnt; and,
- 1.e No drain runs or other trenches shall be dug or otherwise created, without the prior written consent of the Local Planning Authority.

REASON

To ensure that trees and other vegetation can and will be retained on site and not damaged during construction work and to ensure that the development conforms with policy BE38 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012)

8 COM10 Tree to be retained

Trees, hedges and shrubs shown to be retained on the approved plan shall not be damaged, uprooted, felled, lopped or topped without the prior written consent of the Local Planning Authority. If any retained tree, hedge or shrub is removed or severely damaged during construction, or is found to be seriously diseased or dying another tree, hedge or shrub shall be planted at the same place or, if planting in the same place would leave the new tree, hedge or shrub susceptible to disease, then the planting should be in a position to be first agreed in writing with the Local Planning Authority and shall be of a size and species to be agreed in writing by the Local Planning Authority and shall be planted in the first planting season following the completion of the development or the occupation of the buildings, whichever is the earlier. Where damage is less severe, a schedule of remedial works necessary to ameliorate the effect of damage by tree surgery, feeding or groundwork shall be agreed in writing with the Local Planning Authority. New planting should comply with BS 3936 (1992) 'Nursery Stock, Part 1, Specification for Trees and Shrubs'

Remedial work should be carried out to BS 3998:2010 'Tree work - Recommendations' and BS 4428 (1989) 'Code of Practice for General Landscape Operations (Excluding Hard Surfaces)'. The agreed work shall be completed in the first planting season following the completion of the development or the occupation of the buildings, whichever is the earlier.

REASON

To ensure that the trees and other vegetation continue to make a valuable contribution to the amenity of the area in accordance with policy BE38 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and to comply with Section 197 of the Town and Country Planning Act 1990.

9 NONSC Bird Management Plan

Construction works to the roof of the hereby approved school building shall not commence until a Bird Hazard Management Plan has been submitted to and approved in writing by the Local Planning Authority. The submitted plan shall include details of:
- management of any flat/shallow pitched/green roofs on buildings within the site which may be attractive to nesting, roosting and loafing birds. The management plan shall comply with Advice Note 8 Potential Bird Hazards from Building Design

The Bird Hazard Management Plan shall be implemented as approved, on completion of the development and shall remain in force for the life of the building. No subsequent alterations to the plan are to take place unless first submitted to and approved in writing by the Local Planning Authority.

REASON

It is necessary to manage the flat/shallow/green pitched roofs in order to minimise its attractiveness to birds which could endanger the safe movement of aircraft and the operation of Heathrow Airport in accordance with Policy A6 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

10 NONSC Energy Strategy

The development hereby permitted shall not be occupied until the energy strategy incorporating 200m² of photovoltaic panels at roof level and the recommended Combined Heat and Power (CHP) facility have been constructed, unless otherwise agreed in writing by the Local Planning Authority. Thereafter the CHP and photovoltaic panels shall be maintained in full accordance with the approved details and managed in accordance with the approved details for the life of the development.

REASON

To ensure appropriate carbon savings are delivered in accordance with London Plan Policy 5.2.

11 NONSC Air Quality 1

Within 3 months from the date of this permission a scheme designed to minimise the ingress of polluted air shall be submitted for approval in writing by the Local Planning Authority. The design must take into account climate change pollutants. Any suitable ventilation systems will need to address the following:

- Take air from a clean location or treat the air and remove pollutants;
- Be designed to minimise energy usage;
- Be sufficient to prevent summer overheating;
- Have robust arrangements for maintenance.

Thereafter and prior to occupation, the scheme shall be completed in strict accordance with the approved details and thereafter maintained for the life of the development.

REASON

In order to safeguard the amenities of the area, in accordance with Policy OE1 of the Hillingdon Unitary Development Plan Saved Policies (September 2007) and London Plan (July 2011) Policy 7.14.

12 NONSC Air Quality 2

Within 3 months from the date of this permission the details of any plant, machinery or fuel burnt, as part of the energy provision and the location of the flue for the development shall be submitted to the LPA for approval. This shall include pollutant emission rates at the flue with or without mitigation technologies. The use of ultra low NOx emission gas CHPs and boilers is recommended. The development should as a minimum be 'air quality neutral' and below the building emissions benchmark.

Thereafter and prior to occupation, the scheme shall be completed in strict accordance with the approved details and thereafter maintained for the life of the development.

REASON:

To safeguard the amenity of neighbouring properties in accordance with policy OE1 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012).

13 COM30 Contaminated Land

(i) The foundation works for the school building hereby permitted shall not commence until a scheme to deal with contamination has been submitted in accordance with the Supplementary Planning Guidance Document on Land Contamination and approved by the Local Planning Authority (LPA). The scheme shall include all of the following measures unless the LPA dispenses with any such requirement specifically and in writing:

(a) A desk-top study carried out by a competent person to characterise the site and provide information on the history of the site/surrounding area and to identify and evaluate all potential sources of contamination and impacts on land and water and all other identified receptors relevant to the site;

(b) A site investigation, including where relevant soil, soil gas, surface and groundwater sampling, together with the results of analysis and risk assessment shall be carried out by a suitably qualified and accredited consultant/contractor. The report should also clearly identify all risks, limitations and recommendations for remedial measures to make the site suitable for the proposed use; and

(c) A written method statement providing details of the remediation scheme and how the completion of the remedial works will be verified shall be agreed in writing with the LPA prior to commencement, along with details of a watching brief to address undiscovered contamination.

(ii) If during development works contamination not addressed in the submitted remediation scheme is identified, the updated watching brief shall be submitted and an addendum to the remediation scheme shall be agreed with the LPA prior to implementation; and

(iii) All works which form part of the remediation scheme shall be completed and a comprehensive verification report shall be submitted to the Council's Environmental Protection Unit before any part of the development is occupied or brought into use unless the LPA dispenses with any such requirement specifically and in writing.

REASON

To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems and the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors in accordance with policy OE11 of the Hillingdon Unitary Development Plan Saved Policies (September 2007).

14 NONSC Imported Soils

Before any part of the development is occupied, site derived soils and imported soils shall be independently tested for chemical contamination. All soils used for gardens and/or landscaping purposes shall be clean and free of contamination.

Note: The Environmental Protection Unit (EPU) must be consulted for their advice when using this condition.

REASON

To ensure that the occupants of the development are not subject to any risks from soil contamination in accordance with policy OE11 of the Hillingdon Unitary Development Plan Saved Policies (September 2007).

15 COM29 No floodlighting

No floodlighting or other form of external lighting shall be installed unless it is in accordance with details which have previously been submitted to and approved in writing

by the Local Planning Authority. Such details shall include location, height, type and direction of light sources and intensity of illumination. Any lighting that is so installed shall not thereafter be altered without the prior consent in writing of the Local Planning Authority other than for routine maintenance which does not change its details.

REASON

To safeguard the amenity of surrounding properties in accordance with policies BE13 and OE1 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012); and To protect the ecological value of the area in accordance with Policy EC3.

16 N11 Control of plant/machinery noise

The rating level of noise emitted from the plant and/or machinery hereby approved shall be at least 5 dB below the existing background noise level. The noise levels shall be determined at the nearest residential property. The measurements and assessment shall be made in accordance with British Standard 4142 "Method for rating industrial noise affecting mixed residential and industrial areas".

REASON: To safeguard the amenity of the surrounding area in accordance with policy OE1 of the Hillingdon Unitary Development Plan Saved Policies (September 2007) and London Plan (July 2011) Policy 7.15.

17 COM31 Secured by Design

The building(s) shall achieve 'Secured by Design' accreditation awarded by the Hillingdon Metropolitan Police Crime Prevention Design Adviser (CPDA) on behalf of the Association of Chief Police Officers (ACPO). No building shall be occupied until accreditation has been achieved.

REASON

In pursuance of the Council's duty under section 17 of the Crime and Disorder Act 1998 to consider crime and disorder implications in exercising its planning functions; to promote the well being of the area in pursuance of the Council's powers under section 2 of the Local Government Act 2000, to reflect the guidance contained in the Council's SPG on Community Safety By Design and to ensure the development provides a safe and secure environment in accordance with London Plan (July 2011) Policies 7.1 and 7.3.

18 NONSC Sustainable Urban Drainage

Within 3 months from the date of this permission a detailed surface water drainage scheme for the site shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall subsequently be implemented in accordance with the approved details before the development is completed. The scheme shall include a restriction in run-off and surface water storage on site as outlined in the FRA and shall incorporate sustainable urban drainage in accordance with the hierarchy set out in Policy 5.15 of the London Plan.

Additionally it will:

- i. provide details of the surface water design and how it will be implemented to ensure no increase in flood risk from commencement of construction and during any phased approach to building.
- ii. provide a management and maintenance plan for the lifetime of the development of arrangements to secure the operation of the scheme throughout its lifetime.
- iii. provide details of the body legally responsible for the implementation of the management and maintenance plan. The scheme shall also demonstrate the use of methods to minimise the use of potable water, and will:

- iv. incorporate water saving measures and equipment
- v. provide details of water collection facilities to capture excess rainwater;
- vi. provide details of how rain and grey water will be recycled and reused in the development.

Thereafter the development shall be implemented and retained/maintained in accordance with these details for as long as the development remains in existence.

REASON

To ensure that surface water run off is controlled to ensure the development does not increase the risk of flooding contrary to Policy EM6 Flood Risk Management in Hillingdon Local Plan: Part 1- Strategic Policies (Nov 2012), Policy 5.12 Flood Risk Management of the London Plan (July 2011) and Planning Policy Statement 25. To be handled as close to its source as possible in compliance with Policy 5.13 Sustainable Drainage of the London Plan (July 2011), and conserve water supplies in accordance with Policy 5.15 Water use and supplies of the London Plan (July 2011).

19 SUS6 Green Travel Plan

Prior to first occupation of the development hereby approved, the details of the proposed mitigation measures identified within the Transport Assessment (by Robert West June 2014), including before and after school clubs, promotion of walks and cycling, car sharing clubs, road safety training, the promotion of public transport alongside the staggering of start and end schooling times, have been submitted to and approved in writing by the Local Planning Authority as part of the school's Travel Plan. This shall also include investigation of measures such as the provision of a school crossing patrol, and implementation if feasible. Thereafter a Travel Plan review shall be undertaken and submitted in writing to the Local Planning Authority for approval annually. The mitigation measures identified in the Transport Assessment and the Travel Plan review shall be implemented for the duration of the development.

REASON

To promote sustainable transport and reduce the impact of the development on the surrounding road network in accordance with London Plan (July 2011) Policies 6.1 and 6.3.

20 NONSC Non Standard Condition

A construction management plan shall be available on site at all times for the duration of the school and grounds construction works, which shall include the following measures: -

No traffic associated to construction activity for the school building and site or any delivery related to implementation of the development hereby approved shall take place between the hours of 07:30 and 09:30 and between the hours of 14:30 and 16:00 Monday to Friday. Unless otherwise agreed in writing the restrictions to the traffic activity should be maintained throughout the duration of the construction process.

All construction traffic in and out of the site including deliveries will be controlled and monitored by a qualified banks man at all times.

No construction or contractors vehicles will be permitted to park along Laurel Lane, Rowan Road or Wise Lane at any time.

REASON

To ensure the development provides an acceptable level of pedestrian and vehicular

safety in accordance with Policies AM2 and AM7 of the Local Plan Part 2 and London Plan (July 2011) Policies 6.1 and 6.3.

21 NONSC Highway Works

Within 3 months from the date of this permission the details of all traffic arrangements (including where appropriate carriageways, footways, turning space, safety strips, sight lines at road junctions, kerb radii, car parking areas and marking out of spaces, loading facilities, closure of existing access and means of surfacing) have been submitted to and approved in writing by the LPA (all works are required to be supported with stage 2 and 3 Road Safety Audits).

The approved development shall not be occupied until all such works have been constructed in accordance with the approved details. Thereafter, the parking areas, sight lines and loading areas must be permanently retained and used for no other purpose at any time. Disabled parking bays shall be a minimum of 4.8m long by 3.6m wide, or at least 3.0m wide where two adjacent bays may share an unloading area.

REASON

To ensure that the highways impacts of the proposed development are appropriately mitigated and the development provides an acceptable level of pedestrian and vehicular safety in accordance with Policies AM2 and AM7 of the Local Plan Part 2 and London Plan (July 2011) Policies 6.1 and 6.3.

22 NONSC Car Parking Management

The details of the management (plan) of the staff car park to the north and drop off/ pick up facility to the south of the development shall be submitted to, and approved in writing by the Local Planning Authority prior to occupation of the building.

The submitted details shall contain details of security measures, any parking management equipment such as barriers/ticket machines, and a details scheme of management for the areas including within and outside of peak school pick up and drop off hours.

Thereafter the area shall be maintained in full accordance with the approved details and managed in accordance with the approved details for the life of the development.

The development hereby permitted shall not be occupied until the - Drop Off/Pick Up Area facility has been constructed in accordance with the details in drawing PL005-D.

REASON

To promote sustainable transport and reduce the impact of the development on the surrounding road network in accordance with Policies AM2 and AM7 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) and London Plan (July 2011) Policies 6.1 and 6.3.

23 NONSC Disabled Car Parking

The development shall not be occupied until a scheme with details for a total of 5 disabled parking spaces to be provided within the site has been submitted to and approved in writing by the Local Planning Authority. The 5 disabled parking spaces shall be provided as approved by the Local Planning Authority and thereafter retained for the duration of the development.

Reason:

To ensure that an appropriate level of car parking provision for the disabled is provided on site in accordance with Policy AM14 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012).

24 NONSC Electric Charging Points

Prior to occupation of this development, details demonstrating the provision of 6 active and 6 passive electric vehicle charging points within the school car park (excluding the drop/off pick up area) shall be submitted to and approved by the Local Planning Authority. Thereafter the electric vehicle charging points shall be provided in accordance with the approved details prior to occupation of the development and retained for the life of the development.

REASON

To ensure compliance with Policy 6.13 of the London Plan (2011).

25 NONSC Delivery and Servicing Plan

Within 3 months from the date of this permission the details of a Delivery and Servicing Plan shall be submitted to and approved in writing by the Local Planning Authority. This shall incorporate measures to minimise vehicle deliveries during am and pm peak hours. Thereafter and prior to occupation, the scheme shall be completed in strict accordance with the approved details and thereafter maintained for the life of the development.

REASON

To encourage out of hours/off peak servicing to help mitigate the site's contribution to local congestion levels in compliance with Policy AM2 of the Hillingdon Local Plan: Part 2 - Saved UDP Policies (November 2012).

INFORMATIVES

1 I52 Compulsory Informative (1)

The decision to GRANT planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

2 I53 Compulsory Informative (2)

The decision to GRANT planning permission has been taken having regard to the policies and proposals in the Hillingdon Unitary Development Plan Saved Policies (September 2007) as incorporated into the Hillingdon Local Plan (2012) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including the London Plan (July 2011) and national guidance.

EC2	Nature conservation considerations and ecological assessments
EC5	Retention of ecological features and creation of new habitats
BE13	New development must harmonise with the existing street scene.
BE19	New development must improve or complement the character of the area.
BE20	Daylight and sunlight considerations.
BE21	Siting, bulk and proximity of new buildings/extensions.
BE24	Requires new development to ensure adequate levels of privacy to neighbours.
BE38	Retention of topographical and landscape features and provision of

OE1	new planting and landscaping in development proposals. Protection of the character and amenities of surrounding properties and the local area
OE11	Development involving hazardous substances and contaminated land - requirement for ameliorative measures
OE3	Buildings or uses likely to cause noise annoyance - mitigation measures
OE8	Development likely to result in increased flood risk due to additional surface water run-off - requirement for attenuation measures
R4	Proposals that would involve the loss of recreational open space
R5	Proposals that involve the loss of sports, leisure, community, religious, cultural or entertainment facilities
AM13	AM13 Increasing the ease of movement for frail and elderly people and people with disabilities in development schemes through (where appropriate): - (i) Dial-a-ride and mobility bus services (ii) Shopmobility schemes (iii) Convenient parking spaces (iv) Design of road, footway, parking and pedestrian and street furniture schemes
AM14	New development and car parking standards.
AM15	Provision of reserved parking spaces for disabled persons
AM2	Development proposals - assessment of traffic generation, impact on congestion and public transport availability and capacity
AM7	Consideration of traffic generated by proposed developments.
AM9	Provision of cycle routes, consideration of cyclists' needs in design of highway improvement schemes, provision of cycle parking facilities

3 11 Building to Approved Drawing

You are advised this permission is based on the dimensions provided on the approved drawings as numbered above. The development hereby approved must be constructed precisely in accordance with the approved drawings. Any deviation from these drawings requires the written consent of the Local Planning Authority.

4 112 Notification to Building Contractors

The applicant/developer should ensure that the site constructor receives copies of all drawings approved and conditions/informatives attached to this planning permission. During building construction the name, address and telephone number of the contractor (including an emergency telephone number) should be clearly displayed on a hoarding visible from outside the site.

5 115 Control of Environmental Nuisance from Construction Work

Nuisance from demolition and construction works is subject to control under The Control of Pollution Act 1974, the Clean Air Acts and other related legislation. In particular, you should ensure that the following are complied with:-

A. Demolition and construction works which are audible at the site boundary shall only be carried out between the hours of 08.00 and 18.00 hours Monday to Friday and between the hours of 08.00 hours and 13.00 hours on Saturday. No works shall be carried out on Sundays, Bank or Public Holidays.

B. All noise generated during such works shall be controlled in compliance with British Standard Code of Practice BS 5228:2009.

C. Dust emissions shall be controlled in compliance with the Mayor of London's Best Practice Guidance 'The Control of dust and emissions from construction and demolition.

D. No bonfires that create dark smoke or nuisance to local residents.

You are advised to consult the Council's Environmental Protection Unit (www.hillingdon.gov.uk/noise Tel. 01895 250155) or to seek prior approval under Section 61 of the Control of Pollution Act if you anticipate any difficulty in carrying out construction other than within the normal working hours set out in (A) above, and by means that would minimise disturbance to adjoining premises.

6 116 Directional Signage

You are advised that any directional signage on the highway is unlawful. Prior consent from the Council's Street Management Section is required if the developer wishes to erect directional signage on any highway under the control of the Council.

7 119 Sewerage Connections, Water Pollution etc.

You should contact Thames Water Utilities and the Council's Building Control Service regarding any proposed connection to a public sewer or any other possible impact that the development could have on local foul or surface water sewers, including building over a public sewer. Contact: - The Waste Water Business Manager, Thames Water Utilities plc, Kew Business Centre, Kew Bridge Road, Brentford, Middlesex, TW8 0EE.
Building Control Service - 3N/01, Civic Centre, High Street, Uxbridge, UB8 1UW (tel. 01895 250804 / 805 / 808).

8 123 Works affecting the Public Highway - Vehicle Crossover

The development requires the formation of a vehicular crossover, which will be constructed by the Council. This work is also subject to the issuing of a separate licence to obstruct or open up the public highway. For further information and advice contact: - Highways Maintenance Operations, 4W/07, Civic Centre, Uxbridge, UB8 1UW.

9 124 Works affecting the Public Highway - General

A licence must be obtained from the Highway Authority before any works are carried out on any footway, carriageway, verge or other land forming part of the public highway. This includes the erection of temporary scaffolding, hoarding or other apparatus in connection with the development for which planning permission is hereby granted. For further information and advice contact: - Highways Maintenance Operations, 4W/07, Civic Centre, Uxbridge, UB8 1UW

10 147 Damage to Verge - For Council Roads:

The Council will recover from the applicant the cost of highway and footway repairs, including damage to grass verges.

Care should be taken during the building works hereby approved to ensure no damage occurs to the verge or footpaths during construction. Vehicles delivering materials to this development shall not override or cause damage to the public footway. Any damage will require to be made good to the satisfaction of the Council and at the applicant's expense.

For further information and advice contact - Highways Maintenance Operations, Central

Depot - Block K, Harlington Road Depot, 128 Harlington Road, Hillingdon, Middlesex, UB3 3EU (Tel: 01895 277524).

11 162 Potential Bird Hazards from Buildings

The applicant is advised that any flat/shallow pitched or green roof on buildings have the potential to attract gulls for nesting, roosting and loafing purposes. The owners/occupiers of the building must ensure that all flat/shallow pitched roofs be constructed to allow access to all areas by foot using permanent fixed access stairs ladders or similar.

The owner/occupier must not allow gulls to nest, roost or loaf on the building. Checks must be made weekly or sooner if bird activity dictates, during the breeding season. Outside of the breeding season gull activity must be monitored and the roof checked regularly to ensure that gulls do not utilise the roof. Any gulls found nesting, roosting or loafing must be dispersed by the owner/occupier when detected or when requested by BAA Airside Operations staff. In some instances it may be necessary to contact BAA Airside Operations staff before bird dispersal takes place. The contact would be Gary Hudson, The Development Assurance Deliverer for Heathrow Airport on 020 8745 6459.

The owner/occupier must remove any nests or eggs found on the roof. The breeding season for gulls typically runs from March to June. The owner/occupier must obtain the appropriate licences where applicable from Natural England before the removal of nests and eggs. For further information please see the attached Advice Note 8 - 'Potential Bird Hazards From Building Design'

3. CONSIDERATIONS

3.1 Site and Locality

This application relates to a broadly triangular site with approximately 1.23 hectares of public open space. The site is located adjacent to the east of Longmead Primary School, to the west of Laurel Lane and to the north of Rowan Road, West Drayton. The development site includes an area of land within the grounds of the existing Longmead Primary School, to the south-east of the school buildings.

The site is mainly used by residents for recreational purposes with a public right of way running along a north-south axis on the western edge of the site.

The site itself is relatively flat with localised mounding along the southern boundary and south-east corner area. The open space features a number of specimen trees which contribute to the visual amenity and environmental quality of the area. However, none of these are protected by Tree Preservation Orders (TPO). The character of the area is further enhanced by off-site trees, notably to the north, south and west of the site. Other than for maintenance, there is currently no vehicular access to the site.

To the north, the site tapers towards the entrance to Longmead Primary School with the Closes Recreational Grounds together with the St Georges Meadows to the north forming 9 hectares of open parkland which lies within the West Drayton Conservation Area and Green Belt land.

Laurel Lane is immediately adjacent to the site and is a typical two storey residential area with a small shopping parade with flatted development above which defines the east boundary while Rowan Road curves around the southern end of the site. The Rowan

Arms Public House is located prominently on the southern side of the junction of Laurel lane with Rowan Road.

There are bus stops serving the U3 bus route in the vicinity of the site with West Drayton Railway Station being approximately 1.2km from the site and being enhanced by Crossrail services from 2019. The site has a Public Transport Accessibility Level (PTAL) of 1b (on a scale of 1 to 6, where 6 is the most accessible).

3.2 Proposed Scheme

Background

This report relates to the proposed development of a new five form of entry junior school for primary education in West Drayton, which is part of the Council's Primary School Expansion Programme. This programme seeks to satisfy the educational needs identified on evidence provided by the Greater London Authority (GLA) and the Office of National Statistics (ONS) and seeks to meet the educational needs of students at this level mainly through the expansion of existing educational facilities together with the erection of three new schools. The programme to extend existing schools has already been completed in West Drayton and to meet the identified needs for further primary education in West Drayton a new school is required in the area. This proposal has therefore been submitted by Hillingdon Council in its capacity as sponsor of the school.

Proposal

Full planning permission is sought for the erection of a new two storey Junior School (five forms of entry) including new vehicular and pedestrian accesses, alterations to an existing footpath, creation of pick-up/drop-off area, associated car parking, landscaping, playground, provision of a Multi-Use Games Area (MUGA) and ancillary development, on the green space to the west of Laurel Lane.

The five form of entry junior school would be capable of accommodating up to 600 students, plus a SEN/SRP (Special Educational Resources Provision) unit for approximately 12 pupils with special education needs. The proposals would expect to see the number of staff working from the school up to a maximum of 75 full time employees on the basis of the school opening hours proposed to be 0800h -1800h Mondays to Fridays and 0900h to 1800h on Saturdays with a maximum estimated 62 (combined full and part time) employees on-site at any one time.

The building would be T-shaped with the school front elevation set parallel to Laurel Lane with the flanks set at right angles so that the main bulk of the building would be in the sections to the front and rear. The proposed building would be centrally located within the site and two storeys in height with a rising flat roof profile that reads as a shallow arch with an apex at its centre set at 9 metres height.

The building would have a frontage with a maximum length of 91 metres (fronting Laurel Lane) and 18 metres (along Rowan Road), although the bulk of the school's facade is 'broken' into three elements set with varying degrees of proximity to Laurel Lane and artificially separated by stepped in elements. The rear projecting limb of the building would be centrally located and part one, part two storeys in height as to extend the building to the west by a further 13 metres.

The school building itself is proposed with a combination of facing brickwork, timber cladding and vertical timber weatherboarding. In addition, coloured laminated board fins set vertically adjacent to the fenestration in sensitive positions within the front and rear elevations further articulate the elevations and maximise opportunities for temperature

regulation and shading within the building.

The basic layout would consist of two no. two storey teaching wings comprising 23 classrooms, music room, ICT, studio and library, staff rooms, therapy rooms and ancillary facilities such as staff rooms, hygiene rooms, WCs and stores. Within its core the school provides a large double height main hall arranged as a flexible central space with an adjacent smaller hall and kitchen that would provide adaptable catering, performance and open plan learning. The sports hall would be designed to be separated from the smaller hall for simultaneous independent use. A roof terrace for outdoor learning is proposed to the west (rear) of the building with access from the Library.

A Multi Use Games Area is proposed adjacent to the north west of the school building and the use of this facility would be shared with pupils attending Longmead Primary School.

The proposed parking layout for the school aims to separate staff and student/ operational/ visitors circulation areas by concentrating 32 parking spaces within a staff parking zone to the north of the school building (accessed from the existing access to Longmead Primary School) and 23 parking bays within a pupil drop off/ pick up area and visitors & operational vehicles zones on the southern end of the site (accessed via Rowan Road), allowing for a total of 55 parking spaces to be provided, including provision of disabled parking. A new vehicular access would be provided from Rowan Road and access to both parking areas would be via gated access.

The applicant also intends to implement a Green Travel Plan which includes use of staff car parking and implementing a permit allocation system to prioritise those with a genuine need to drive to work (those with a disability or with a caring responsibility) as to encourage and increase the percentage of non-car mode trips for the school.

The main pedestrian entrance to the site would be from adjacent to the Rowan Road with the boundary and access gates set back to ease pedestrian movements. A further segregated pedestrian access from the western end of Wise Lane and alongside the properties at nos.61 to 67 Rowan Road would also be available on the south western corner of the site.

3.3 Relevant Planning History

Comment on Relevant Planning History

There is no previous planning history that could be of relevance to the current application.

4. Planning Policies and Standards

UDP / LDF Designation and London Plan

The following UDP Policies are considered relevant to the application:-

Part 1 Policies:

- PT1.BE1 (2012) Built Environment
- PT1.EM1 (2012) Climate Change Adaptation and Mitigation
- PT1.EM2 (2012) Green Belt, Metropolitan Open Land and Green Chains
- PT1.EM4 (2012) Open Space and Informal Recreation

- PT1.EM6 (2012) Flood Risk Management
- PT1.EM7 (2012) Biodiversity and Geological Conservation
- PT1.EM8 (2012) Land, Water, Air and Noise

Part 2 Policies:

- EC2 Nature conservation considerations and ecological assessments
- EC5 Retention of ecological features and creation of new habitats
- BE13 New development must harmonise with the existing street scene.
- BE19 New development must improve or complement the character of the area.
- BE20 Daylight and sunlight considerations.
- BE21 Siting, bulk and proximity of new buildings/extensions.
- BE24 Requires new development to ensure adequate levels of privacy to neighbours.
- BE38 Retention of topographical and landscape features and provision of new planting and landscaping in development proposals.
- OE1 Protection of the character and amenities of surrounding properties and the local area
- OE11 Development involving hazardous substances and contaminated land - requirement for ameliorative measures
- OE3 Buildings or uses likely to cause noise annoyance - mitigation measures
- OE8 Development likely to result in increased flood risk due to additional surface water run-off - requirement for attenuation measures
- R4 Proposals that would involve the loss of recreational open space
- R5 Proposals that involve the loss of sports, leisure, community, religious, cultural or entertainment facilities
- AM13 AM13 Increasing the ease of movement for frail and elderly people and people with disabilities in development schemes through (where appropriate): -
 - (i) Dial-a-ride and mobility bus services
 - (ii) Shopmobility schemes
 - (iii) Convenient parking spaces
 - (iv) Design of road, footway, parking and pedestrian and street furniture schemes
- AM14 New development and car parking standards.
- AM15 Provision of reserved parking spaces for disabled persons
- AM2 Development proposals - assessment of traffic generation, impact on congestion and public transport availability and capacity
- AM7 Consideration of traffic generated by proposed developments.
- AM9 Provision of cycle routes, consideration of cyclists' needs in design of highway improvement schemes, provision of cycle parking facilities

5. Advertisement and Site Notice

Not applicable

5.1 Advertisement Expiry Date:- **24th June 2014**

5.2 Site Notice Expiry Date:- Not applicable

6. Consultations

External Consultees

Site Notice: Erected 03rd June 2014

Press Advertisement: 03rd June 2014

Some 827 neighbouring households, amenity groups, and local businesses were notified of the proposal on 29th May 2014 with the consultation period ending on 19th June 2014. A display of the proposals was simultaneously placed in West Drayton Library.

The consultation period was subsequently extended to 4th July 2014, which involved the erection of a notice in a prominent location in West Drayton's Library and a notice in the front page of the Council's website.

So far 30 letters have been received objecting to the proposals and 4 letters commenting on the proposals.

The letters in objection and commenting on the application raise the following concerns:

- (i) The proposal would involve the loss of green open space and trees
- (ii) Existing and committed school expansions are sufficient to meet the need and questions the applicants evidence on this.
- (iii) View that existing schools elsewhere should be extended instead (in particular within the adjacent Longmead Primary school grounds)
- (iv) Have detrimental impact on the character and appearance of the area
- (v) Noise and air pollution
- (vi) Traffic impacts and parking, insufficient car spaces within the drop off/ pick up facility, including concerns with the impacts of the proposed school in combination with the impacts of the existing school
- (vii) Highway, pedestrian and cyclist safety
- (viii) Impact on bus flow
- (ix) The proposal would be detrimental to the recreational use of the park, which is used by local people for a multitude of recreational purposes, including dog walking.
- (x) The proposal will lead to the loss of significant trees and be harmful to ecology and a wide range of wildlife
- (xi) Impact on health and well being of residents
- (xii) Question the adequacy of the consultation process regarding the use of the internet to submit responses and timeframes for response.
- (xiii) The proposal would give rise to further anti-social behaviour
- (xiv) Believe this should be put to the local residents in a public meeting to air their views.
- (xv) Impacts from construction works
- (xvi) Proposal will lead to further secondary education facilities being required
- (xvii) Concerns regarding the loss of a right of way
- (xviii) Impact property prices and therefore decrease the value of many peoples homes
- (xix) Lane between nos. 115 and 117 Wise Lane should not be re-opened

1 petition of objection with 96 signatures has been received objecting on the following grounds:

- (xx) Amount of traffic and parking congestion created by the scheme and the impact on local residents and major roads in the vicinity
- (xxi) Inadequate parking provision

- (xxii) Location of proposed new access relates poorly to existing access to Longmead Primary School
- (xxiii) Proposed location for the educational facility will result in loss of land needed for highway improvements
- (xxiv) Increased air, noise and dust pollution from extra traffic
- (xxv) Potential detrimental impact on future pupils due to aircraft noise associated with Heathrow Airport activity.

A letter has also been received from John McDonnell MP raising the following concerns:

I wish to query why this site was chosen when the recent large scale housing development sites containing the main growth in child population are elsewhere in the ward and some distance from this site. This will inevitably result in a large proportion of the pupils attending the new school travelling some distance and most likely by car. I am concerned therefore about the increase in traffic in an already busy road system around Laurel Lane, and the impact this will have on road safety, congestion and air pollution. If this application is to proceed there is a significant need to explore what potential there is for effective traffic calming measures to address the road safety and congestion issues and specific measures to address air pollution in this heavily polluted area. I would also wish to query what consultation has been undertaken to fully inform and take into account the views of local residents and existing local schools.

A letter received from Councillor Jan Sweeting raises the following concerns:

Whilst welcoming the provision of additional primary school places in West Drayton to meet local need, I object to the plans as presented due to the following issues which I ask to be considered by the relevant planning committee.

1. The site in question is highways land and should therefore be used to address existing traffic problems from the existing Laurel Lane School which has increased its numbers considerably in the last few years. There should be investigations as to how the amenity land could be used to improve access and congestion problems before it is considered for any other use. It is highways land for highways use.
2. The Laurel Lane School site is a large site and this site should have been considered for the new school. There needs to be a comprehensive look at the whole education use of the existing school site and a better layout configured to ameliorate existing and potential traffic problems. The current plans would seem to maximize access and traffic congestion problems for the existing school, nearby shops and residents.
3. The access and parking arrangements for the new school are inadequate. A new layout is required in relation to the new and existing school to address these problems.
4. The new school offers an opportunity for much needed community use facilities to be provided in the area which have not been explored and which could have been provided within the overall school site.

Further Background Information and Issues for Both Planning and Children's Services to Consider.

- A significant number of people on the Wise Lane Estate will be affected by this proposal as it will increase the capacity of Laurel Lane Infant and Junior Schools from 420 pupil places plus 120 children of nursery age, to 1050 pupil places plus 300 children of nursery age. The number of teaching and non-teaching staff will therefore increase in proportion. Therefore, what has been proposed will merely add to the area's traffic congestion problem.

- The location of the school is not in the location where additional primary school places are most urgently needed in West Drayton. Child number increases are coming predominately from the significant number of new properties built and being built on the former Air Traffic Control and RAF West Drayton sites, which is some distance from the amenity land in question. Therefore, there is the likelihood that a large proportion of the intake to the new school will travel to the school by car, thus creating significant traffic increases along the narrow suburban roads of Laurel Lane and

Rowen Road. This area already suffers from high level of traffic gridlock and pollution from cars and other vehicles. Therefore any increase is bound to be disadvantageous to the resident population.

- Nowhere in the plans submitted are there any measures to mitigate against the increase in traffic. The amenity land in its entirety is being used, therefore leaving no possible improvement to the current highway outside Laurel Lane School.

- It is recognised that there is a need for additional primary school places in West Drayton and that suitable sites are hard to find. However, a suitable site on either the Air Traffic Control or the RAF West Drayton sites should have been identified and set aside, as it was very likely that additional primary school capacity would be required in the area immediately surrounding the hundreds of new flats and houses built on these sites. This obvious solution was adopted for the RAF site Uxbridge, where a new primary school is being built to accommodate children from the new development. I would ask that detailed work is undertaken in all areas of the borough to identify potential sites for new schools so that in future better solutions to lack of capacity can be identified as early as possible.

- The residents of Wise Lane require that traffic management solutions are sought in the vicinity of the new school. This to include investigations as to the use of some of the amenity site to deliver traffic alleviation measures. This could mean that some of the Laurel Lane School site, which has over capacity of land, could be used to site some or all of the new school building.

- Laurel Lane school will, if this development proceeds, become one of the largest primary schools in London. Five forms of entry primary schools are now in several areas in the south of the borough, and it is yet to be proved that the life chances of children are improved within such a large school. If the Council continues with the policy of school expansions by merely increasing existing schools up to 5 forms of entry, it is important that investigations are undertaken at existing 5FE primary schools in Hillingdon and the few that are in other areas of London in order to establish that standards are being maintained and improved and not diminished. This body of work is most vital to the educational chances of thousands of young lives.

- One final point. The residents around the new 5FE school will need to see that their views and wishes are taken into account. Therefore it may be a worthwhile exercise for the Council to undertake an audit of residents' views in order to ascertain what additional facilities they would like to see in the area and in the school which would in part mitigate against the disruption which this new school, and its vastly increased roll will create.

ENVIRONMENT AGENCY

The site lies in Flood Zone 1 it would be the Council flood water specialist who would review the surface water flood risk for this site. We therefore have no comments to make.

BAA

The proposed development has been examined from an aerodrome safeguarding perspective and could conflict with safeguarding criteria unless any planning permission is subject to the condition/s detailed below:

Submission of a Bird Hazard Management Plan

We refer to your email dated 30/05/14, received in this office on 02/06/14.

Development shall not commence until a Bird Hazard Management Plan has been submitted to and approved in writing by the Local Planning Authority. The submitted plan shall include details of:

- management of any flat/shallow pitched/green roofs on buildings within the site which may be attractive to nesting, roosting and "loafing" birds. The management plan shall comply with Advice Note 8 'Potential Bird Hazards from Building Design' attached * See para below for information *

The Bird Hazard Management Plan shall be implemented as approved, on completion of the development and shall remain in force for the life of the building. No subsequent

alterations to the plan are to take place unless first submitted to and approved in writing by the Local Planning Authority.

Reason: It is necessary to manage the flat roofs in order to minimise its attractiveness to birds which could endanger the safe movement of aircraft and the operation of Heathrow Airport.

Information

The Bird Hazard Management Plan must ensure that flat/shallow pitched roofs be constructed to allow access to all areas by foot using permanent fixed access stairs, ladders or similar. The owner/occupier must not allow gulls, to nest, roost or loaf on the building. Checks must be made weekly or sooner if bird activity dictates, during the breeding season. Outside of the breeding season gull activity must be monitored and the roof checked regularly to ensure that gulls do not utilise the roof. Any gulls found nesting; roosting or loafing must be dispersed by the owner/occupier when detected or when requested by BAA Airside Operations staff. In some instances it may be necessary to contact BAA Airside Operations staff before bird dispersal takes place.

The owner/occupier must remove any nests or eggs found on the roof. The breeding season for gulls typically runs from March to June. The owner/occupier must obtain the appropriate licences where applicable from Natural England before the removal of nests and eggs.

If the overall size of the flat/shallow pitched roof/s exceeds 500Sq.M then we would apply a condition to ensure the roof space was monitored for bird activity throughout the year and for the life of the building. To avoid any such condition then we would advise the roofs to be pitched greater than 15 degrees. We would also make the following observation:

Cranes

Given the nature of the proposed development it is possible that a crane may be required during its construction. We would, therefore, draw the applicant's attention to the requirement within the British Standard Code of Practice for the safe use of Cranes, for crane operators to consult the aerodrome before erecting a crane in close proximity to an aerodrome. This is explained further in Advice Note 4, 'Cranes and Other Construction Issues' (available at [http://www.aoa.org.uk/operation & safety/safeguarding.htm](http://www.aoa.org.uk/operation%20&%20safety/safeguarding.htm))

I, therefore, have no aerodrome safeguarding objection to this proposal, provided that the above condition is applied to any planning permission.

It is important that any conditions requested in this response are applied to a planning approval. Where a Planning Authority proposes to grant permission against the advice of Heathrow Airport Ltd, or not to attach conditions which Heathrow Airport Ltd has advised, it shall notify Heathrow Airport Ltd, and the Civil Aviation Authority as specified in the Town & Country Planning (Safeguarded Aerodromes, Technical Sites and Military Explosive Storage Areas) Direction 2002.

Subsequent Comments: I agree with an alteration to the condition to state:

The roof of the development shall not commence until a Bird Hazard Management Plan has been submitted to and approved in writing by the Local Planning Authority. The submitted plan shall include details of:

- management of any flat/shallow pitched/green roofs on buildings within the site which may be attractive to nesting, roosting and "loafing" birds. The management plan shall comply with Advice Note 8 'Potential Bird Hazards from Building Design' attached * See para below for information *

Internal Consultees

EDUCATION SERVICES - SCHOOLS POLICY & STRATEGY OFFICER

The proposal is part of a large, borough-wide programme of additional primary school places. The need for these places has arisen due to rising birth rates and changes to migration patterns and the Council has in place a robust system for forecasting future demand. The Council has a legal responsibility to ensure that there are sufficient school places in its area. For primary school place planning purposes, the borough is divided into 14 areas. This is because there needs to be sufficient places in each area so that pupils can be offered places within a reasonable distance from their homes. The proposed site falls within the West Drayton place planning area. Despite the expansion of other local schools, there is a need for further places and the current proposal is intended to address this deficit. Therefore a new school is needed. The proposed site is the only suitable site in the area. The proposal is to create a new 5 form of entry junior school adjacent to the existing Laurel Lane Primary school. This is related to a separate proposal for the existing primary school to become a 5 form of entry infant school. These two projects will together provide an additional 3 forms of entry (90 extra admission places per year).

FLOODWATER AND DRAINAGE OFFICER

The proposals are acceptable subject to a detailed Sustainable Urban Drainage Condition (as forwarded).

HIGHWAYS

The development is for the construction of a junior school on land adjacent to the existing Laurel Lane Primary School, located along Laurel Lane, West Drayton. The proposed junior school will accommodate 600 pupils and employ approximately 95 members of staff. The school will be occupied on a year on year basis, commencing in September 2015, with full occupation being reached in September 2021.

Under the proposals, a car park will be located to the north of the site, which will provide 32 parking spaces including 1 disabled space that will be allocated for the use of staff. Access to the proposed car park will be provided over the existing vehicle access that serves Laurel Lane Primary school, which will be widened to 5.5m.

A drop off/pick up area will be provided within the site, which will accommodate 23 parking spaces for the dropping off and picking up of pupils at associated peak times. The drop off/pick up area will also be used for servicing and will provide for an element of visitor parking at all other times.

Access to the drop off/pick up area will be provided from Rowan Road, which will form the primary entrance to the school. Pedestrian access to the school will also be provided from Rowan Road and via a public foot-path connecting to Wise Lane, which will be segregated from the vehicle access to the site.

Under the proposals, the existing 20 MPH Zone, located along Laurel Lane will be extended along Rowan Road, adjacent to the proposed junior school.

In addition, 52 cycle parking spaces (26 stands) will be provided within the site, that will be allocated for the use of pupils and staff.

In order to assess the development in relation to the expected impact along the adjacent highway network, a Transport Assessment (TA) has been submitted in support of the development, the details of which are discussed below.

Parking

An assessment has been undertaken within the TA of the existing and future on street parking capacity along the adjacent highway network. The assessment has been based on the current modal share in relation to staff and pupils at the existing Laurel Lane Primary school and by providing a comparison assessment of the expected modal share, based on similar existing junior schools within Hillingdon. In addition, a parking beat survey has been undertaken during the peak periods associated with the existing and proposed schools, within a 5 minute walking distance of the site.

From reviewing the parking assessment, it is considered that the theoretical on street car parking capacity is slightly less than that specified within the TA. Nevertheless, it has been identified that during the morning and afternoon peak periods associated with the school, there is available on street parking capacity within the surrounding area, when the existing primary and proposed junior schools are at full occupation.

In addition, the capacity of the proposed drop off/pick up facility within the site has been assessed considering maximum dwell times of 3 and 5 minutes in the AM and 10 minutes in the PM peak periods. This has demonstrated that (provided the drop off/pick up facility is managed by the school to ensure the maximum dwell times) during the AM peak period, nearly all of the increased car parking demand can be accommodated within the site, with some overspill parking taking place on street during the PM peak period.

Traffic Generation

The TA has undertaken comparative capacity analyses of the priority junction of Laurel Lane and Harmondsworth Road, in order to assess the impact relating to the increase in vehicle trips associated with the development.

The assessment has considered the operation of the junction in 2016 and the future year - 2021, taking account of the existing and proposed schools, with and without development traffic assigned to the highway network.

The analyses has shown that in 2016, during the AM peak period, the junction will be approaching its operational capacity when development traffic is assigned to the highway network. In the future year - 2021, when the junior school will be at full occupation, the junction will operate over capacity in both the AM and PM peak periods, with noticeable queuing predicted along Laurel Lane. However, the results of the analyses do not take into account the mitigation measures that will be implemented as part of a Travel Plan, which will help to reduce the impact along the adjacent highway.

Accident Analysis

Assessment of accidents along the highway network adjacent to the site has been undertaken for a three year period, ending October 2013. The study area included all roads within a 500m radius of the school. From the analysis, 10 accidents have been identified within the study area, all of which are classified as slight.

Along Laurel Lane, adjacent to the school, 2 accidents were recorded, one involving an elderly bus passenger who fell while in the bus and another involving a pedestrian who was struck by scaffolding on a passing HGV, where the vehicle was travelling too close to the kerb based on the load being carried. At the junction of Rowan Road and Wise Lane a 4 year old child was struck by a car turning at the junction, who ran out into the road without looking. In addition, at the junction of Laurel Lane and Harmondsworth Road, a cyclist was struck by a car turning right out of the junction. The remainder of accidents involved vehicles and were located away from the site.

As a result, it has been demonstrated that there are no established patterns identifying specific road safety issues within the surrounding area of the school and all accidents relate to road user behaviour.

Mitigation

The TA has identified a number of measures to address the issues associated with increased parking demand, traffic generation and highway safety, as a result of the development.

A Travel Plan will be produced specifically for the proposed junior school, separate to that of the existing primary school. The Travel Plan will focus on achieving and promoting sustainable means of travel to the school by pupils and staff and will support the reduction in the use of private cars.

The measures that will be provided within the Travel Plan will include setting up a Travel Plan Working Group, the promotion of walking, cycling, car sharing and the use of public transport.

In addition, "before and after school" activities will be operated and promoted to spread the peak arrival and departure demand relating to the dropping off and picking up of pupils, which will reduce congestion along the adjacent highway.

Furthermore, in order to promote highway safety, the existing 20 MPH Zone along Laurel Lane will be extended adjacent to the proposed junior school, along Rowan Road. While the principle of the proposed extension to the existing 20 MPH Zone is considered acceptable, the scheme is required to be amended to relocate and provide additional speed reducing features and to include "School Keep Clear" road markings adjacent to the access to the site.

Conclusion

From the parking assessment undertaken within the TA, it has been demonstrated that there is available capacity within the local area during the AM and PM peak periods associated with the school. However, it is considered that the demand for on-street car parking and traffic generation along Rowan Road will be increased as a result of the development.

When considering the analysis undertaken of the priority junction of Laurel Lane and Harmondsworth Road, it is noted that the junction will operate over capacity in 2021 when the school will be at full capacity, resulting in noticeable queuing and vehicle delays along Laurel Lane.

As a result, it is considered that a further assessment in relation to the operational capacity of the junction should be undertaken at 70% occupation (420 pupils) of the site, in order to identify and implement improvement works including options (but not limited to) changes to the junction operation, which should be covered through a suitable planning condition or legal agreement. The agreed works to occur within 6 months of the assessment.

In addition, it is considered that all of the mitigation measures identified within the draft Travel Plan are required implemented to ensure a robust approach is undertaken to reduce private car use to and from the site.

Conditions/S106 Agreement

The mitigation measures detailed within the draft Travel Plan submitted in support of the proposals shall be implemented upon first occupation of the site. Thereafter, the Travel Plan is required to be reviewed annually to monitor and if required, update and/or amend the document in order that the aims and objectives are achieved. Therefore, a travel plan review should be undertaken and submitted to the LPA for approval annually, commencing 1 year after first occupation of pupils and

staff.

Staff car parking within the school should be provided with 20% (active and passive) electrical charging points in accordance with the London Plan.

A Car Parking Management Strategy detailing how staff car parking and the proposed drop off/pick up area within the site will be operated is required to be submitted and approved in writing by the LPA prior to first occupation.

An amended extension to the existing 20 MPH Zone along Laurel Lane shall be submitted and approved in writing by the LPA. Thereafter the development shall not be occupied until the proposed 20 MPH Zone has been completed in accordance with the agreed details.

A Servicing Management Plan is required to be submitted and approved in writing by the LPA prior to first occupation. The Servicing Management Plan should detail how the development will be serviced and managed on a daily basis and to utilise joint servicing between both the junior and primary schools to minimise disruption along the adjacent highway.

A Construction Management Plan is required to be submitted to and agreed in writing by the LPA before commencement of works at the site in order to minimise the impact along the adjacent highway network during construction. Thereafter, all works shall be carried out in accordance with the approved document.

Development shall not commence until details of all traffic arrangements (including where appropriate carriageways, footways, turning space, safety strips, sight lines at road junctions, kerb radii, car parking areas and marking out of spaces, loading facilities, closure of existing access and means of surfacing) have been submitted to and approved in writing by the LPA (all works are required to be supported with stage 2 and 3 Road Safety Audits). The approved development shall not be occupied until all such works have been constructed in accordance with the approved details. Thereafter, the parking areas, sight lines and loading areas must be permanently retained and used for no other purpose at any time. Disabled parking bays shall be a minimum of 4.8m long by 3.6m wide, or at least 3.0m wide where two adjacent bays may share an unloading area.

SUSTAINABILITY OFFICER

I have no objections to the proposed development subject to the following compliance conditions:

1 - The development must proceed in accordance with the energy strategy incorporating the recommended CHP unit along with 200m of photovoltaic panels.

REASON

To ensure appropriate carbon savings are delivered in accordance with London Plan Policy 5.2.

2 - The proposed green roof must include nectar rich planting in a mix of no less than 30% in line with the recommendations in the ecology report.

REASON

To ensure the development contributes to ecological enhancement in accordance with Policy EM7 (Local Plan) and Policy 7.28 of the London Plan.

3 - Prior to occupation, the applicant shall provide a plan showing the inclusion of bat and bird boxes across the site along with a habitat wall in the area for education. The development must proceed in accordance with the approved plan unless otherwise agreed in writing with the local planning authority.

REASON

To ensure the development contributes to ecological enhancement in accordance with Policy EM7 (Local Plan) and Policy 7.28 of the London Plan.

ACCESS OFFICER

This initial design is fundamentally acceptable from an accessibility position, however, the following observations should be incorporated into revised plans:

1. Plans indicate that accessible parking would be provided to serve the main entrance, and no concerns are raised in terms of parking and the drop-off arrangements for parents/guardians and other visitors to school. However, the staff car parking area should be designed such to allow an accessible parking space to be created should a need arise at some future point.
2. Although the Design & Access Statement confirms that a changing facility to support those with complex personal care requirements would be provided in accordance with 'Changing Places' criteria, the plans do not appear to incorporate a 12 m² cubicle to allow for such a facility.
3. An emergency evacuation plan/fire strategy that is specific to the evacuation of persons unable to escape by stairs should be submitted and reviewed prior to any grant of planning permission. Provisions could include: a) a stay-put policy within a large fire compartment (e.g. within a classroom at first floor with suitable fire resisting compartmentalisation); b) provisions to allow the lift to be used during a fire emergency (e.g. uninterrupted power supply attached to the lift); c) contingency plans to permit the manual evacuation of disabled people should other methods fail.

Recommended Informatives

- a) The Equality Act 2010 seeks to protect people accessing goods, facilities and services from discrimination on the basis of a 'protected characteristic', which includes those with a disability. As part of the Act, service providers are obliged to improve access to and within the structure of their building, particularly in situations where reasonable adjustment can be incorporated with relative ease. The Act states that service providers should think ahead to take steps to address barriers that impede disabled people.
- b) Fixtures, fittings and furnishings, particularly hard materials should be selected to ensure that sound is not adversely reflected. The design of all learning areas should be considerate to the needs of people who are hard of hearing or deaf. Reference should be made to BS 8300:2009+A1:2010, Section 9.1.2, and, BS 223 in selecting an appropriate acoustic absorbency for each surface.
- c) Care should be taken to ensure that the internal decoration achieves a Light Reflectance Value (LRV) difference of at least 30 points between floor and walls, ceiling and walls, including appropriate decor to ensure that doors and door furniture can be easily located by people with reduced vision.
- d) Induction loops should be specified to comply with BS 7594 and BS EN 60118-4, and a term contract planned for their maintenance.
- e) Care must be taken to ensure that overspill and/or other interference from induction loops in different/adjacent areas does not occur.
- f) Flashing beacons/strobe lights linked to the fire alarm should be carefully selected and installed to ensure they remain within the technical thresholds not to adversely affect people with epilepsy.

Officer Comment: Amended plans have been submitted and the applicant has clarified the strategy with regard to changing facilities which is considered wholly appropriate.

ENVIRONMENTAL PROTECTION UNIT - AIR QUALITY

The application site is located within the Hillingdon AQMA, declared for Nitrogen Dioxide (NO₂).

The air quality assessment has not considered the air quality impact at the application site as a receptor, however a number of nearby locations including residential receptors along the route to school have been considered. The impact from the development is considered 'negligible', although it should be noted the modelling indicates exceedances of the EU limit value for nitrogen dioxide at almost all of the locations even without development. Negligible refers to increases of 0.1 to 0.2 mg/m³ in NO₂ as a consequence of the development.

The air quality assessment has used high background levels for all years considered, (monitored data for Laurel Lane in 2010 was slightly below the background level used) and the assessment appears to be based on traffic data only. It appears to be conservative in the assumptions made, which may go some way to explaining the exceedances indicated, where monitoring data does not indicate an exceedance. CERC modelling carried out for Hillingdon in 2011 indicates at the worst locations at the application site, NO₂ levels are likely to be above the limit value of 40 mg/m³, although even in this conservative assessment most of the site is below or just below the limit value.

The air quality assessment did not consider NO₂ levels at the facades of the proposed building. It is possible there may be exceedances at the facades in areas closer to the main road and the motorway. The assessment also does not look at the contribution from the gas-fired CHP proposed for the development either, as this information does not appear to have been provided to the consultants.

It is understood from the Ventilation and Air Extraction Statement dated May 2014 (revision A - 23 May 2014) that M(mechanical)VHR is being used on the ground floor due to the proximity to the airport (relating to noise), but N(natural)VHR is being used on the first floor, from structures mounted above the area to be ventilated. I am not clear how the latter would help with noise. We would advice with regard to ventilation design that air should be drawn in (mechanically or naturally) from clean locations (e.g. areas away from the adjacent road, car park, drop-off zone, motorway, any air exhausts and flues) to avoid drawing in polluted air. The ingress of polluted air condition is recommended for the development to ensure this is properly addressed in the design of the building.

Condition 1: Ingress of Polluted Air

Before the development is commenced a scheme designed to minimise the ingress of polluted air shall be submitted for approval in writing by the Local planning Authority. The design must take into account climate change pollutants. Any suitable ventilation systems will need to address the following:

- Take air from a clean location or treat the air and remove pollutants;
- Be designed to minimise energy usage;
- Be sufficient to prevent summer overheating;
- Have robust arrangements for maintenance.

Thereafter and prior to occupation, the scheme shall be completed in strict accordance with the approved details and thereafter maintained for the life of the development.

REASON: In order to safeguard the amenities of the area, in accordance with Policy OE1 of the Hillingdon Unitary Development Plan Saved Policies (September 2007) and London Plan (July 2011) Policy 7.14.

The air quality assessment looked at the building emissions bench mark and traffic emissions bench mark for the development and indicated the development could be 'air quality neutral' if a suitably low NO_x CHP is used and would be 'air quality neutral' based on the traffic data. The BREEAM 2011 pre-assessment Rev1 May 2014 indicates only 2 point would be picked up for NO_x emission reduction, rather than the full 3 points. Further information is required with regard to the CHP and the meeting of the building emissions benchmark. The following condition is

recommended for inclusion in any permission that may be given.

Air Quality Condition 2 - Details of Energy Provision

Before the development is commenced details of any plant, machinery and fuel burnt, as part of the energy provision and the location of the flue at the development shall be submitted to the LPA for approval. This shall include pollutant emission rates at the flue with or without mitigation technologies. The use of ultra low NOx emission gas-fired CHPs and boilers is recommended. The development should as a minimum be 'air quality neutral' and demonstrably below the building emissions benchmark.

REASON: To safeguard the amenity of neighbouring properties in accordance with policy OE1 of the Hillingdon Unitary Development Plan.

Notes: This condition relates to the operational phase of residential and commercial development and is intended for the protection of future residents in a designated AQMA and Smoke Control Area. Advice on the assessment of CHPs is available from EPUK at: http://www.iaqm.co.uk/text/guidance/epuk/chp_guidance.pdf. An area up to a distance of 10 times the appropriate stack height needs to be assessed. Guidance on air quality neutral is available at: <https://www.london.gov.uk/priorities/planning/consultations/draft-sustainable-design-and-construction>. They should contact Planning Specialists if they have any queries.

The air quality assessment refers to no mitigation being required due to negligible impacts. It refers to a draft school travel plan, also referred to in the Transport Assessment. It should be noted the air quality assessment indicated exceedances in NO2. We would encourage the school to minimise emissions and minimise exposure to emissions as much as possible. With reference to the Management Plan for the drop-off zone, or the Travel Plan in general, we would recommend the inclusion of a no idling policy to cut down of exhaust emissions.

ENVIRONMENTAL PROTECTION UNIT - LAND CONTAMINATION

No information has been submitted with the application regarding land contamination, even though the application form indicates all or part of the site is suspected of being contaminated, and the proposed development is sensitive to contamination. The standard contaminated land condition is recommended for inclusion in any permission given. It is advisable suitable ground gas monitoring is undertaken as a minimum. The soils condition is also recommended, and soil testing needs to be carried out to demonstrate the development is suitable for use.

Contaminated Land Condition

(i) The development hereby permitted shall not commence until a scheme to deal with contamination has been submitted in accordance with the Supplementary Planning Guidance Document on Land Contamination and approved by the Local Planning Authority (LPA). The scheme shall include all of the following measures unless the LPA dispenses with any such requirement specifically and in writing:

(a) A desk-top study carried out by a competent person to characterise the site and provide information on the history of the site/surrounding area and to identify and evaluate all potential sources of contamination and impacts on land and water and all other identified receptors relevant to the site;

(b) A site investigation, including where relevant soil, soil gas, surface and groundwater sampling, together with the results of analysis and risk assessment shall be carried out by a suitably qualified and accredited consultant/contractor. The report should also clearly identify all risks, limitations and recommendations for remedial measures to make the site suitable for the proposed use; and

(c) A written method statement providing details of the remediation scheme and how the

completion of the remedial works will be verified shall be agreed in writing with the LPA prior to commencement, along with details of a watching brief to address undiscovered contamination.

(ii) If during development works contamination not addressed in the submitted remediation scheme is identified, the updated watching brief shall be submitted and an addendum to the remediation scheme shall be agreed with the LPA prior to implementation; and

(iii) All works which form part of the remediation scheme shall be completed and a comprehensive verification report shall be submitted to the Council's Environmental Protection Unit before any part of the development is occupied or brought into use unless the LPA dispenses with any such requirement specifically and in writing.

REASON: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems and the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors in accordance with policy OE11 of the Hillingdon Unitary Development Plan Saved Policies (September 2007).

The following condition needs to be included in any permission given to ensure the soils are suitable for use, if any areas of soft landscaping or planting are created as part of the development.

Condition to minimise risk of contamination from garden and landscaped areas

Before any part of the development is occupied, site derived soils and imported soils shall be independently tested for chemical contamination, and the results of this testing shall be submitted and approved in writing by the Local Planning Authority. All soils used for gardens and/or landscaping purposes shall be clean and free of contamination.

Note: The Environmental Protection Unit (EPU) must be consulted for their advice when using this condition.

REASON

To ensure that the occupants of the development are not subject to any risks from soil contamination in accordance with policy OE11 of the Hillingdon Unitary Development Plan Saved Policies (September 2007).

Subsequent comments: Instead of pre-commencement the ventilation/CHP/ could state: "Within 3 months from the date of this permission details of ventilation/ CHP shall be provided to...".

Similarly, land contamination conditions can be worded to allow commencement of enabling works and details shall be submitted prior to the commencement of the school building construction works.

ENVIRONMENTAL PROTECTION UNIT - Noise and lighting

Due to the proximity of nearby sensitive premises I would recommend that the following conditions are attached.

Lighting MUGA

Please attach a condition which prevents them from installing external lighting to the MUGA until details have been submitted and approved.

Lighting General

Please attach a condition requiring that any changes to the proposed lighting scheme are submitted for approval.

N11B Noise affecting residential property

The rating level of noise emitted from the plant and/or machinery hereby approved shall be at least 5 dB below the existing background noise level. The noise levels shall be determined at the nearest residential property. The measurements and assessment shall be made in accordance with British Standard 4142 "Method for rating industrial noise affecting mixed residential and industrial areas".

REASON: To safeguard the amenity of the surrounding area in accordance with policy OE1 of the Hillingdon Unitary Development Plan.

TREES & LANDSCAPE OFFICER

Saved policy BE38 seeks the retention and utilisation of topographical and landscape features of merit and the provision of new planting and landscaping wherever it is appropriate.

- An Arboricultural Report (document PL120), by Patrick Stileman, dated February 2014, has assessed the condition and value of 86No. individual trees and 10No. groups.
- This is analyzed by an Arboricultural Impact Assessment (AIS) and supported by a Method Statement (AMS).
- Hunters South drawing Nos. PL004 Rev B Topographical & Tree Survey, and PL006 Rev B Tree Retention Plan reflects the outcome of the arboricultural recommendations.
- The tree schedule includes off-site trees which are close to the site and therefore influence the setting and visual amenity of the area, although they should not be affected, either directly or indirectly, by the proposed development.
- The off-site trees which will be retained include: T1-6 at the southern end of the site (junction of Wise Lane / Rowan Road), T51-T86 within the Longmead School grounds along the west boundary, G1 and T15-T18 to the east of Laurel Lane and T29-T46 to the north of the site within the grass verge / Longmead School site.
- The Arboricultural Report confirms, in section 4, that there should be a presumption of retaining high quality (category 'A') trees on development sites. Category 'B' trees should be viewed as a constraint on development and retained if possible. 'C' category trees are of low quality and should not impose a significant constraint on development.
- Within the site, drawing No. PL006 Rev B Tree Retention Plan, confirms that 7No. existing trees will be retained as part of the site layout: T7 Horse Chestnut (B2) and T8 Horse Chestnut (C), T13 and T14 Horse Chestnut (B2), T25 and T26 Tree of Heaven (B2) and T29 Tree of Heaven (A1).
- The same drawing indicates that 14No. trees will be removed to facilitate the development: T9 and T10 Horse Chestnut (C), T11 and T12 Horse Chestnut (B2), T19 - T23 Norway Maples (B2), T24, T27, T28 Tree of Heaven (B2), T30 Tree of Heaven (C) and T50 Indian Bean Tree (A1).
- To summarise, 1No. A category tree, 5No. B category and 1No. C category tree will be retained on site, while 1No. A, 10No. B and 3No. C category trees are scheduled to be removed to enable the development.
- Document ref. PL125, Arboricultural Impact Assessment, explains the rationale behind the tree removal and retention strategy. Some of the tree loss is regrettable but unavoidable due to the spatial constraints of the site. The retention of selected trees will be reliant on the specified methodology and tree protection.
- Specified safeguarding measures include the provision of log retaining walls around specific trees (to avoid changes of levels around the sensitive root plates), the use of no dig / cellular confinement systems and (section 8) on-going supervision by a qualified arboriculturalist at critical (specified) time during the development process.
- Document ref 121, Habitat Survey (Phase 1), by SES, concludes that a bat activity survey should be carried out. It also considers that any potential adverse effects from the proposed development can be mitigated by a sensitive / ecologically -lead landscape design. (Please refer to Ian Thynne).
- Document ref. PL001, Design & Access Statement, confirms the landscape design rationale and objectives. The landscape proposals are described further in document PL122 Landscape Strategy Report and illustrated in the Landscape Masterplan, ref. LLD692/01 Rev 05.
- The layout includes the mature trees 'to be retained' (as identified in the Tree Report), the provision of new / replacement trees, boundary hedges, the provision of native wildflower

meadows, indicative plant lists and schedules of hard materials. A brown roof has been specified for part of the new building - which will also support photovoltaic panels.

- Further details and /or amendments will be required regarding the location of the boundary hedge, the route of the public footpath, plant schedules / species selection, existing / proposed levels, the drainage arrangements and underground services.
- If the application is recommended for approval, landscape conditions should be imposed to ensure that the proposals preserve and enhance the character and local distinctiveness of the surrounding natural and built environment.

RECOMMENDATIONS:

This application has been subject to pre-application discussions and the proposed tree retention and landscape proposals reflect the outcome of the discussions.

No objection, subject to the above observations and conditions COM7, COM9 (parts 1,2,3,4,5, and 6), COM10.

Informatives

- All tree work should be carried out in accordance with the recommendations of BS3998:2010 'Tree Work-Recommendations'.
- The Wildlife and Countryside Act 1981: Note that it is an offence under the Wildlife and Countryside Act 1981 to disturb roosting bats or nesting birds or other species. It is advisable to consult your tree surgeon/consultant to agree an acceptable time for carrying out any work.
- New planting should seek to enhance biodiversity, by including species of known value to wildlife which produce berries and / or nectar.
- Re-usable spoil should be handled and stored in an appropriate manner to ensure that it is of sufficient quality to be used in landscape mitigation earthworks. Appropriate handling and storage should adhere to recognised good practice. Soil should be stored in accordance with British Standard 3882:2007.

RIGHTS OF WAY OFFICER

The Public Footpath Y8 is recorded on the Definitive Map & Statement as a Public Rights of Way and all recorded rights must be diverted or extinguished legally in the event of any planning application for a development that affects the footpath.

In respect of the Laurel Lane development the application should be to divert the part of the footpath that will be affected by the development as this will ensure that the present linkage is retained between other Public Rights of Way Y4, Y5 and Y6. The diversion should be around the development using the adopted public footway bordering the site / Laurel lane and then linking back to the other Public Rights of Way.

The diversion would be achieved under s257 of the Town & Country Planning Act and the Order would need to be made by the Planning Department (Legal Section) on application from the developer at the developers cost.

As to the adopted public highway that the development will sit on, this area needs to be 'stopped up' under s247 of the Town and Country Planning Act. The 'stopping up' would also be dealt with by the Planning Department (Legal Section) on receipt of an application, but it must be borne in mind that this can only be done before the development is substantially completed.

7. MAIN PLANNING ISSUES

7.01 The principle of the development

The subject site, comprising 1.23 hectares of open land to the west of Laurel Lane, is highway land and consists of a sizeable highway verge that is used mainly by local residents for recreational purposes.

The proposal seeks the erection of a new two storey Junior School (five form of entry) within this land and would include vehicular and pedestrian accesses, creation of pick-up/drop-off area, associated car parking, landscaping, playground, provision of a Multi-Use Games Area (MUGA) and ancillary development, on the green space to the west of Laurel Lane.

Loss of Green Space

It is noted that residents have raised concerns over the loss of public open space which would occur as a result of the scheme if planning permission was granted.

The site is highway land and essentially comprises, albeit a sizeable one, a highway grass verge, which is not identified or designated as a protected open space within the Local Plan. Although the shape, size, undulating morphology, position (bounded by roads to the north, east and south) and the presence of trees, mean that this green space is generally unsuitable for formal sports or recreational use, it is acknowledged that it has a visual value and an amenity role to the residents and is therefore of some local importance. Hillingdon's Open Space Strategy 2011-2026 confirms that "Open Space should be taken to mean all open space of public value, [not just including land], which offers important opportunities for sport and recreation and can also act as a visual amenity."

The Open Space Strategy seeks to provide an understanding of the current and future supply and demand for open space; develop local standards and allow the identification of areas of deficiency; identify opportunities to address these deficiencies; and identify open spaces for improvement. It assesses the deficiency in access to small, local, district and metropolitan level open space at 400 metres, and also the deficiency in access to district (and higher) level open spaces within 1,200 metres.

Likewise, Policy R4 and Policy EM4 the Hillingdon Local Plan: Part One seek to prevent the loss of land used for recreational open space, particularly if there is (or would be) a local deficiency in accessible open space. This is also reiterated in London Plan policies 7.16 and 7.18, which confirm that the loss of open space should be resisted, except in very special circumstances, giving it the same level of protection as the Green Belt. Paragraph 74 of the National Planning Policy Framework (NPPF) reaffirms that the strongest protection should be given to preserving open space.

In this instance, although the eastern part of West Drayton ward is generically identified as having some deficiency in district and metropolitan spaces the strategy does not identify any deficiency in small, local or district level open spaces within the vicinity of the application site itself.

Moreover, the site lies in very close proximity to the Closes Recreation Ground with its 9 hectares of high quality parkland providing formal and informal recreational space, which immediately abuts the existing Laurel Lane Primary School's northern boundary.

As such, this proposal, which includes 3,750m² of modern educational facilities, seeks to replace the existing open land use. Given that the open space constituting the application site is not afforded specific protection in the Hillingdon UDP and that the area is not deficient in open space while high quality parkland and recreational grounds are available immediately to the north of the site no reasonable objections can be offered to its replacement with an educational development which includes a new five form junior school promoting a community use on site, provided that very special circumstances exist to justify its replacement.

In addition, it is proposed to make a financial contribution towards the provision of new tree planting within the Closes recreation Ground to further enhance the visual amenity and quality of the space. It is considered that the enhancement of the Closes Recreation Ground, which would be secured through a Statement of Intent, assists in offsetting the loss of the existing highway grass verge subject of the current application.

Given the size of and close proximity of the Closes Recreation Ground, at approximately 9 hectares, and the enhancements proposed, it is considered that the loss of the open space will have a negligible impact on overall open space provision within this part of the borough.

Educational Facility

Policy R10 of the Council's Unitary Development Plan Saved Policies (September 2007) seeks to encourage the provision of enhanced educational facilities across the borough, stating:

"The Local Planning Authority will regard proposals for new meeting halls, buildings for education, social, community and health services, including libraries, nursery, primary and secondary school buildings, as acceptable in principle subject to other policies of this plan."

This is reiterated in the London Plan Policy 3.18 which states:

"Development proposals which enhance education and skills provision will be supported, including new build, expansion of existing facilities or change of use to educational purposes. Those which address the current projected shortage of primary school places will be particularly encouraged."

Furthermore, on 15/08/11 the DCLG published a policy statement on planning for schools development, which is designed to facilitate the delivery and expansion of state-funded schools.

It states:

"The Government is firmly committed to ensuring there is sufficient provision to meet growing demand for state-funded school places, increasing choice and opportunity in state-funded education and raising educational standards. State-funded schools - which include Academies and free schools, as well as local authority maintained schools (community, foundation and voluntary aided and controlled schools) - educate the vast majority of children in England. The Government wants to enable new schools to open, good schools to expand and all schools to adapt and improve their facilities. This will allow for more provision and greater diversity in the state-funded school sector to meet both demographic needs and the drive for increased choice and higher standards."

It goes on to say that:

"It is the Government's view that the creation and development of state-funded schools is strongly in the national interest and that planning decision-makers can and should support that objective, in a manner consistent with their statutory obligations. We expect all parties to work together proactively from an early stage to help plan for state-school development and to shape strong planning applications. This collaborative working would help to ensure that the answer to proposals for the development of state-funded schools should be, wherever possible, "yes."

The statement clearly emphasises that there should be a presumption in favour of the development of schools and that "Local Planning Authorities should make full use of their planning powers to support state-funded schools applications."

Paragraph 72 of the NPPF reiterates the objectives set out in the DCLG Policy Statement on Planning for Schools Development. It clearly confirms that the Government attaches great importance to ensuring that a sufficient choice of school places is available to meet existing and future demand.

The applicant's assessment of future educational need in the area is considered to be well presented. Providing a detailed analysis of increasing demand for school places across Hillingdon and in particular within the southern areas of the Borough, which would be served by the proposed school. This increased demand is primarily the result of increased birth rate and changes to net migration, both of which are likely to be sustained for longer than previously anticipated.

The result is the need for the provision of approximately 26 additional forms of entry across Hillingdon. The Council has sought to meet the majority of this demand through the expansion of existing schools. Despite these expansions the Council has identified that an additional school is required to meet the pressing demand within the West Drayton area.

The increased demand for school places is compelling, well evidenced and confirmed by the Greater London Authority (GLA) and the Office of National Statistics (ONS).

The need to meet educational needs is a material planning consideration which needs to be afforded significant weight in accordance with paragraph 72 of the National Planning Policy Framework and the DCLG Policy Statement on Planning for Schools Development. The need to meet this demand is a clear planning objective, as stated in the NPPF.

Given the lack of any more appropriate alternative sites, the educational need, that this is not an area deficient in open space and the very close proximity of the approximately 9ha Closes Recreation Ground, it is not considered that the proposed development would have such a detrimental impact on residential amenity, through the loss of this relatively small space, that refusal could be justified.

In the circumstances, the educational need for an additional school is clear and compelling and it is considered that this need outweighs the minor harm which would be caused by the loss of 1.23 hectares of open space in this location. It is therefore considered that special circumstance exist which justify the loss of this open space in accordance with Policies R4 and EM4 of the Local Plan, Policies 7.16 and 7.18 of the London Plan and the NPPF.

Public Right Of Way

The proposal would result in the loss of an existing path, which runs adjacent to the eastern boundary of Longmead Primary School on the western side of the application site. This path is a public right of way known as Y8 and links the public rights of way Y4, Y5, Y6 to the north of the site with Y28 to the south of the site. With the current proposal the path will no longer be available for public use and pedestrians will need to use the existing footway along Laurel Lane instead.

To prevent the loss of the public right of way the applicant has proposed a diversion to the right of way to run on the existing footway alongside Laurel lane to the east of the application site. Albeit making use of the adopted public footway on the eastern border of the site the route of the proposed diverted right of way will ensure the continuity of the public rights of way between the paths known as Y4, Y5, Y6 and Y28 and will ensure that a link continues to exist after the development is completed. In this instance, given the

footpath is to be diverted there is little in the way of impact or harm caused that warrants a refusal of the planning permission.

Summary

Overall, it is considered that there is an identified and compelling need for a new five form of entry junior school and that there are no alternative sites which could be used to provide an appropriate facility. It is considered that the educational need represents very special circumstances and that on balance this need outweighs the harm which would arise from the development in other respects, including through the loss of open space by reason of inappropriateness.

The principle of the development of the site for educational purposes combining a five form entry junior school and ancillary facilities is therefore acceptable. However, whilst the principle of development is acceptable in land use terms, this is subject to the consideration of all other policy guidance and relevant material considerations as set out below.

7.02 Density of the proposed development

The application proposes the erection of a new five forms of entry junior school. Residential density is therefore not relevant to the consideration of this application.

7.03 Impact on archaeology/CAs/LBs or Areas of Special Character

The site is located approximately 85 metres distance from West Drayton Green Conservation Area, which is considered a sufficient distance to ensure that the school would preserve the character of the West Drayton Green Conservation Area.

The application site is not located within or in the vicinity of any other Conservation Area, Area of Special Local Character or Listed Buildings.

7.04 Airport safeguarding

The relevant airport and aerodrome safeguarding agency (BAA) have been consulted and raise no objection to the proposal, subject to a bird hazard management plan being secured by condition.

Subject to imposition of the appropriate condition it is not considered the proposal would pose a risk to the safe operation of aircraft and the development would therefore comply with Policy A6 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012).

7.05 Impact on the green belt

The application site is not located within Green Belt land. However, Green Belt land exists 85 metres to the north of the site which is considered a sufficient distance to ensure that the proposed school would not result in harm to the open character of the Green Belt.

7.07 Impact on the character & appearance of the area

The National Planning Policy Framework, Chapter 7 "Requiring Good Design" in paragraph 56 states that "The Government attaches great importance to the design of the built environment. Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people". Paragraph 58 goes on to say that planning decisions should ensure that developments: "will function well and add to the overall quality of the area, establish a strong sense of place, using streetscape and buildings to create attractive and comfortable places to live, work and visit, respond to local character and history, and reflect the identity of local surroundings and materials and are visually attractive as a result of good architecture and appropriate landscaping."

Policy BE13 of the Local Plan Part Two: Saved Policies UDP requires that the design of new development in terms of layout and appearance should harmonise with the existing street scene and the surrounding area. The context of the site consists primarily of the surrounding two storey residential properties and the Longmead Primary School grounds to the west.

The existing site occupies a prominent area alongside Laurel Lane and the proposal includes a new primary school contained within a part one, part two storey building in the central section of the site with a total of 32 car parking spaces located to the north of the school building and a pupil drop off point with 23 parking spaces to the south. The school building would be t-shaped and is arranged within the site with the frontage set parallel to Laurel Lane.

The site layout seeks to separate the staff car parking element, on the northern side of the site and the pupil drop off point, on the southern section of the site. The resulting development would therefore see a logical positioning of the school building within the site which achieves maximum retention of trees with visual amenity value and access arrangements and associated parking located further away from the residential properties. Similarly, refuse and recycle stores are proposed within a service yard on the western end of the school building where access via the drop off point from collection vehicles will have less impact on the operation of the site itself and on the amenity of neighbours. A Multi Use Games Arena (MuGa) is proposed on the north western corner of the site within what is currently part of the Longmead Primary School grounds. The facility is to be shared between the two educational facilities and its positioning reflects this arrangement.

The school building itself would be located centrally within the site and is a part single, part two storey, building with flat roofs oppositely pitched. The roof profile is architecturally developed with symmetrical shallow angles that step up in height towards the centre of the building. The opposing pitches meet within the horizontal section of the roof element, which allows for the roof profile to be perceived as a shallow arch. The material finish would be brick with timber cladding for the walls together with contrasting render and coloured vertical shading fins.

In design terms, it is considered that the scale and massing of the school building is acceptable being predominantly two storeys in height. It is felt that the proposed building with its articulated brickwork and timber elements combined with its projecting and recessed elements adding rhythm to the front elevation of the building would be both distinctive and attractive, and would integrate successfully within the site. Although generally higher, in particular at eaves level, than the predominant two storey urban form commonly found in the immediate surrounding area it is considered that due to the peninsular nature of the site, and set back from Laurel Lane the proposal would not adversely affect the character of the area. The footprint and siting of the proposed buildings is considered to be acceptable as the building placement is in part a response to the separation of activity from the nearest adjoining neighbours to the north, south, south west and east.

Summary

Whilst the proposal would introduce a built form within the site it is considered that the ratio between hard and soft landscaping is, on balance, acceptable whilst noting the potential scope for replacement planting and landscaping on the boundaries of the site.

The sharing of the existing access to Longmead Primary School to access the proposed staff car parking, subject to minor alterations, is welcomed while the pedestrian access to

the south along with the pupil drop off point on the southern side of the development allows for permeability throughout the site whilst separating public and private spaces. The integral service yard proposed to the school building is considered acceptable and will maximise the amount of area for landscaping to the front of the building.

In summary, the layout proposed is well designed with a good balance of hard and soft landscaping which would complement the surrounding area and streetscene whilst aiming to retain the site's self contained and verdant nature. The scale, footprint and massing of the proposed building is also considered to be acceptable subject to the buildings being set down into the site as detailed in the amended cross sections. The detached design and form would contribute to the character and appearance of the area.

Overall, it is considered that the proposed development would site appropriately within its surroundings in accordance with Policy BE13 of the Local Plan Part Two: Saved Policies UDP.

7.08 Impact on neighbours

Policies BE20, BE21, BE22 and BE24 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) seek to ensure that the design and layout of new buildings do not result in a significant loss of residential amenity due to overdominance, loss of light or loss of privacy.

The school building is in part one storey, part two storeys in height and proposed within the central section of the application site, with its frontage set parallel to Laurel Lane and southern flank fronting onto Rowan Road. The nearest residential properties are the flats located above the small shopping parade on the south eastern end of the Laurel Lane and at 160 Rowan Road, which are the closest neighbouring properties, at a minimum 40 metres distance of the School building. This is considered a sufficient distance to ensure that the amenity of neighbouring occupiers would not be unreasonably affected by reason of loss of outlook, loss of daylight/ sunlight or undue overlooking.

The proposed site plan also indicates the opportunity for generous landscaping and appropriate boundary treatments which would prevent any loss of privacy to neighbouring properties arising from the proposed use of the external areas and play spaces.

The impact of noise arising from the development on neighbouring properties is addressed within section 7.18 of this report, and subject to conditions the development would have no adverse impacts on the amenity of nearby occupiers in this respect.

Overall, it is not considered that the proposed development would have an unreasonable adverse impacts on the amenity of nearby residential occupiers.

7.09 Living conditions for future occupiers

This consideration relates to the quality of residential accommodation and is not applicable to this type of development. However, it is considered that the proposed school which has been designed to accord with the Disability Discrimination Act and Department for Education standards would provide an appropriate environment with adequate size for the future staff and students.

The building as proposed has been designed in accordance with Building Bulletin 93 (BB93) to provide acoustic conditions within the classrooms that facilitate clear communication of speech between teacher and student, between students and prevent interference with study activities. The proposed measures include the use of mechanical and natural ventilations systems to ensure that the building is not ventilated through

openable windows and the noise mitigation range of measures proposed include measures to sound proof against aircraft noise. Overall, it is considered that the building will provide a good standard of educational facilities that have been designed to meet the requirements within BB93 which is the Acoustic Requirements of the School Premises Regulations (2012).

7.10 Traffic impact, Car/cycle parking, pedestrian safety

Proposals

The developer wishes to erect a five forms of entry junior school with 3.750m² along with car parking for 32 staff cars (including one disabled parking space) together with 23 visitors/ drop off parking spaces with two spaces designated for people with disabilities. The proposal for the school includes covered cycle parking storage for 52 cycles. The site will have two points of vehicular access with one positioned from the Longmead Primary School existing access point off Laurel Lane and a new vehicular access from Rowan Road, located towards the south of the site with a pedestrian access positioned adjacent to the east. A new pedestrian access will be provided off Wise Lane.

A Transport Assessment (TA) was submitted in support of the application. The Councils Highways Engineer has considered the report and advised that the applicant has provided an assessment of traffic generation and parking demand demonstrating that the impact on the priority junction of Laurel Lane and Harmondsworth Road will be noticeable during AM and PM peak hours, when the school reaches full capacity in the year 2021.

However, the Highways officer also advised that the TA demonstrates that before the school reaches full capacity the junction will operate close to capacity and, likewise, that the analysis for the year when the school reaches full capacity in 2021, did not model the functioning of the junction whilst accounting for the mitigation measures identified in the TA, such as the proposed green travel plan and 52 cycle spaces for staff and pupils, which seek to promote alternative transport methods and reduce the impact of the school in terms of traffic generation and parking demand. Accordingly, the Highways Engineer recommended a planning obligation requiring a further assessment to the operational capacity of the junction, which should be undertaken when the school reaches 70% occupation (420 pupils) to identify whether improvements works are required to the junction operation.

The Highways Engineer has advised that the proposal would provide a total of 55 on-site parking spaces (of which 23 are within the visitor/ drop off point), with 32 spaces provided for school staff. The applicant also has proposed the inclusion of 52 new cycle bays within a cycle shelter, which, bearing in mind the number of pupils and employees is considered acceptable. A cycle storage condition has been recommended because the arrangement shown on the site plan is not sufficiently detailed to establish whether it would be in accordance with the requirements for provision of cycle storage to be safe and secure.

Existing Situation

Under the existing arrangements there are predominantly residential uses in the vicinity of the application site with Longmead Primary School located adjacent to the west of the site. The site is located on Laurel Lane close to the junction of Rowan Road in West Drayton in a predominantly residential area. Laurel Lane has an access off Harmondsworth Road (Y2). The site is currently in use as an open green space and therefore there is very little traffic flows associated with it in the area as a result. The existing site has a single vehicular access point to the Longmead Primary School and this access point will be retained and redesigned as part of the proposed development.

From a traffic survey undertaken within the TA, the morning and afternoon peak hours have showed that the maximum queue length of vehicles waiting to turn out into Harmondsworth Road (Y2) demonstrated that the junction is operating significantly under its maximum capacity.

There are bus services in Laurel Lane and the nearest bus stop to the site is located just outside where 1 scheduled bus service (U3) operates. This 20 metres walk to the nearest pedestrian access to site is ideal. The scheduled bus service passing along Laurel Lane contributes to the PTAL score which produces a rating of 1b, which is poor on TfL's classification. However, the site is located 300 metres from Harmondsworth Road where there is a frequency of bus services.

The applicant's consultant carried out an accident analysis for the local area using data from the previous 3 years (ending October 2013) and it was found that 10 Personal Injury Accidents (PIA) were recorded, 2 in Laurel Lane and one at the junction of Rowan Road and Wise Lane, which demonstrates that there are no demonstrable patterns identifying specific road safety issues in the surrounding area of the school.

Access

The access to both parts of the development will include some minor modification to an existing northern access point which will be shared with Longmead Primary School and a new access point at the south eastern side of the site. The applicant has provided a layout of the proposed accesses arrangements with the new development in place. The technical information provided includes details of position, widths, visibility splays and kerb radius; however the final design will require the technical approval of the Council as Highway Authority. A condition is recommended to secure this.

Parking

55 on-site car parking spaces including one staff disabled bay and 2 disabled bays within the drop off/ pick up area are provided to serve the Junior School. This car parking provision is in accordance with the TA findings. The Design and Access Statement states that the car parking will be for staff with visitors car parking provided in a segregated area in the pupil drop off area. There will be a maximum of 62 staff on the site at any one time. The TA gives a summary of the situation whereby at busy periods there is likely to be some demand for on-street parking spaces.

From the information supplied in the TA it is clear that there is sufficient on-street parking space to accommodate the parking demand from the development proposals but this would mean that staff working in the school could be parked outside residential properties for long periods of the day. As such, the staff demand for on-site car parking cannot be entirely met by the proposed provision set out in the application and relies on on-street parking supply to meet the remainder of the demand for parking. However, there is sufficient on-street parking spaces to cater for this demand within the area of the site.

In terms of the provision for pupils drop off/ pick up facility, the Council Highways Engineer has confirmed that the amended scheme provides for 23 echelon parking spaces, is sufficient to meet the demand of the school pupils during AM peak times provided that maximum dwell times do not exceed 3 to 5 minutes. Although the amended scheme for the drop off/ pick up facility provides for additional parking spaces for the parents collecting their children from school some overspill parking may still occur during the PM peak period and therefore a condition is recommended to ensure that the drop off/ pick up facility is managed by the school to reduce maximum dwell times during the AM and PM peak periods.

Deliveries/Servicing

The TA addresses the issue of servicing and deliveries to the school. Swept Paths showing that the drop off/ pick up facility layout can accommodate a refuse vehicle was provided in the TA and this demonstrated that a vehicle of this type can enter, turn around and leave the site without any difficulty. There is no information given on the time of deliveries and as the site is close to residential properties it is important that no out of hours deliveries take place.

As such, it is not considered that the deliveries will be a concern provided that the delivery hours do not coincide with school peak times. A condition is recommended to ensure this.

Traffic Generation and Impact

The TA undertook a trip generation analysis for the proposed school and from this analysis it was shown that the proposed development will increase trips on the adjacent highway network. In the TA it is assumed that the majority of additional vehicles from the school will pass through the Laurel Lane/ Harmondsworth Road junction.

The TA suggests the impact of the development will see the queue lengths increased along Laurel Lane and the junction will be over capacity when the school reaches full occupancy in 2021. This is a logical derivation but the TA did not carry out a traffic model for the junction whilst considering whether the mitigation measures proposed (including green travel plan and cycle spaces) will have a significant impact on the junction or not. On the basis of the above information there are concerns over the Traffic Impact and the situation will be monitored as the school take up increases.

The Council's Highways Engineer has recommended that the impact to the junction is assessed when the school reaches 70% occupancy capacity. However, a precautionary principle should be adopted in this instance, and a planning obligation is therefore recommended to ensure that the situation is monitored when the school reaches 50% of its maximum capacity and that any improvements works to the junction are implemented within 6 months of the corrective measures to the junction being agreed.

Notwithstanding this, it is worth noting that this situation is similar to other urban locations in London where junction delays are prevalent, and for this reason, any such concerns do not render the proposal unacceptable on these grounds.

Bus Services/ School Transport

The nearest bus services are just outside the site on Laurel Lane and Rowan Road. There are no alterations proposed to the existing public transport facilities and these will remain available for staff, pupils and visitors to the school.

Construction

There are no details provided in the TA regarding construction but this information can be subject of a Construction Management Plan condition.

The Travel Plan

A number of measures to be included in the Travel Plan was provided as part of the planning application material. In the Travel Plan the applicant seeks to promote a number of sustainable means of travel modes. These include use of non-car trips to and from the site for staff, pupils and visitors to the junior school and the details of which will be required by condition.

Impact of the Proposals and Mitigation Measures

The TA unsurprisingly states that there will be increases in the number of trips to the site so it can be assumed that these increases will be added to existing traffic in Laurel Lane/Harmondsworth Road on weekday morning and afternoon peak periods associated with the school. The TA also indicates that the applicant will provide car parking spaces to support the school staff and pupil generated trips to the proposed junior school and this is less than the maximum off-street car parking on the site according to the TA findings. The demand for car parking will result in a number of cars parking on-street in the vicinity of the school site which does not occur at present and will be of concern to local residents.

The applicant submitted a range of sustainable measures to be included in the Travel Plan but final details will be secured by condition. A detailed Travel Plan will be required to be implemented when the new educational facility opens. The Travel Plan will have to be in place for the duration of the development and will be monitored and reviewed annually.

In order to increase road safety in the vicinity of the site the applicant will be required to extend the existing 20MPH zone along Laurel Lane adjacent to the proposed school along Rowan Road whilst providing additional speed reduction features and "School Keep Clear" road markings.

From the above it is unlikely that the proposals will make a significant impact on the area in terms of traffic, transport and parking issues before the school reaches 70% of its maximum capacity (420 pupils) and it is recommended to include a clause in the legal agreement to ensure that these impacts are monitored before they are noticeable to local residents. However, given that the full impact that the school associated traffic will have in the local highway network is still uncertain a precautionary approach is recommended, in this instance. In the circumstances, it is appropriate to include a clause in the Statement of Intent requiring that studies and monitoring of the situation at the priority junction are taken up when the school reaches 50% capacity.

The impact of the construction traffic has not been identified but it is assumed the construction will take place in a relatively short time which will minimise the impact of such activities, but in any event, a condition will ensure that traffic associated with the construction works adhere to a construction management plan restricting construction traffic to the periods outside the AM and PM peak hours.

Summary

The Principal Highways Engineer considers that staff car parking for the school less than the requirements identified in the TA is acceptable in view of availability of on-street parking in the vicinity of the site. Although, not fully identified in the TA the impact of the proposed development can be minimised in traffic/transport and highway terms by measures to be included in the Travel Plan. A clause in the legal agreement is also recommend ensuring that the impact of school associated traffic will be monitored and measured when the school reaches 50% and that any improvements to the junction are approved and implemented within 6 months of the corrective measures to the junction being agreed.

As such, the proposals are considered acceptable with respect to highways considerations.

7.11 Urban design, access and security

Urban Design

Addressed in Section 7.07 of this report.

Access

Addressed in Section 7.12 of this report.

Security:

Subject to the implementation of security measures, and a condition requiring the development to achieve Secure by Design accreditation is included, it is considered that an appropriate level of security would be achieved.

7.12 Disabled access

The initial scheme was reviewed by the Council's Accessibility Officer who raised a number of minor matters.

In relation to these issues the applicant amended drawings indicating the provision of a hygiene room of an appropriate size to accommodate changing for those with complex care requirements. The applicant has indicated the intention that this be a dual use area which would accommodate this function when necessary and this is considered appropriate having regard to the size and type of junior school.

The amended ground floor plan also demonstrates that each group of children's toilets has a single enlarged cubicle to accommodate ambulant disabled pupils and fully accessible disabled persons toilets are provided within the main school and SRP unit.

The amended site plan demonstrates the provision of 2 disabled parking spaces within the proposed drop off/pick up point. The use of the drop off/pick up area for the SRP is considered appropriate having regard to the proximity to the building and users of this facility and would ensure that an appropriate and dignified approach to the unit would be available. The level of disabled parking within this area is also considered appropriate and the detailed location of the spaces could be refined through a condition.

The width of access routes is clearly shown on the proposed plan, clarifying that all access routes are greater than 1.8m in width and are to have an appropriate surface treatment. The statement also clearly sets out that the gradient of the main access route fully complies with BS8300.

Therefore, the development is considered fully acceptable from an accessibility standpoint and in compliance with Policy 7.2 of the London Plan (July 2012) and the Local Planning Authorities Supplementary Planning Document Accessible Hillingdon.

7.13 Provision of affordable & special needs housing

Policies relating to the provision of affordable and special needs housing are not pertinent to this application.

7.14 Trees, landscaping and Ecology

The NPPF states that development proposals should seek to respect and retain, where possible, existing landforms and natural features of development sites, including trees of amenity value, hedges and other landscape features. It states that development should make suitable provision for high quality hard and soft landscape treatments around buildings. Landscape proposals will need to ensure that new development is integrated and positively contributes to or enhances the streetscene. In addition, proposals should seek to create, conserve or enhance biodiversity and improve access to nature by sustaining and, where possible improving the quality and extent of natural habitat enhancing biodiversity in green spaces and among developments.

In this case, the proposal requires the removal of five of the trees within the site frontage, and the removal of a group of trees along the eastern boundary. All other trees would

remain, including the significant cluster of tree at the southern end of the site. However, additional semi-mature tree planting and a detailed hard and soft landscaping scheme has been submitted. This indicates that whilst 14 trees would be removed, 35 semi-mature trees would be planted within the site, and in addition the applicant has also given a commitment to the provision of new off-site tree planting as part of the improvements to the Closes Recreational Ground.

The applicant considers the setting of the school building to be of great importance and a high quality area is therefore proposed to the front of the school building, together with comprehensive landscaping around the building and on the site boundaries. The frontage would be primarily hard surfaced, using suitable materials and incorporating 2.1 metre high bow top boundary fencing. The priority for this area will be directed towards pupil activity, and the detailed design of this surface will reflect this requirement. Appropriate conditions will be imposed to ensure that this remains the case.

The application submission includes full details of the proposed hard and soft landscaping proposals throughout the site, and a Landscape Management Plan condition is recommended to ensure the successful establishment of new vegetation, and overall integration works within the surrounding landscape.

The landscaping strategy has had due regard to nature conservation interests, and with the addition of "brown roof" to the rooftop of the building and bat boxes it would overall improve the quality and extent of natural habitat within this location. Whilst the trees to be lost would result in the loss of some natural habitat, the replacement planting and ecological measures are considered acceptable in mitigation while replacement trees are also proposed on site and off site.

The Council's Trees Officer recognises that the new development would affect the character of the area and the streetscape, and that the new school and treatment to the front of the school building has the potential for creating a high quality and attractive space which can mitigate for the loss of the existing frontage trees.

It is noted that representations made on the application seek the retention of the trees on the site. However, whilst the proposal would result in the loss of trees and vegetation within the site, the comprehensive landscaping proposals are considered to be of a sufficiently high quality to mitigate against their loss.

In addition, a financial contribution would be secured through a Statement of Intent to enable the progression and improvements to the Closes Recreational Grounds thereby securing a considerable enhancement to the green space/ public realm in very close proximity to the application site, including the planting of semi-mature trees.

Subject to conditions to secure the final details of the landscaping scheme it is considered that the proposal would accord with Policies BE38, OL2, EC2 and EC5 of the Local Plan Part Two and Policy 7.19 of the London Plan.

No objection is therefore raised to the proposals on landscaping or ecological grounds.

7.15 Sustainable waste management

The development would be served by a refuse store located within the western limb of the proposed school building and adjacent to a servicing/delivery area. The store would accommodate 10 bins which could be appropriately divided between general waste and recycling storage and is considered adequate to serve the development.

It should be noted that the school ultimately has discretion over which waste management methods are used on site, however the proposals demonstrate that adequate provision could be made and accordingly the development would accord with Policy 5.17 of the London Plan.

7.16 Renewable energy / Sustainability

Policy 5.2 of the London Plan (July 2011) requires development proposals to make the fullest contribution possible to reducing carbon emissions. Major development schemes must be accompanied by an energy assessment to demonstrate how a 40% target reduction in carbon dioxide emissions will be achieved, where feasible.

In accordance with this policy the applicant has submitted an Energy Strategy and a BREEAM Pre-Assessment to demonstrate how the London Plan objectives will be met. The proposal incorporates a range of energy efficiency measures, and would also involve the installation of Combined Heat and Power Pump and a 200m² array of photovoltaic panels.

The applicant has correctly applied the Mayors Energy Hierarchy and these measures would achieve a 47% reduction in carbon dioxide emissions above Part L of the Building Regulations in compliance with the requirements of London Plan Policy 5.2.

London Plan policy 5.11 states that major development proposals should provide green roofs, policy 5.13 requires that new developments should employ sustainable drainage solutions, and policy 5.15 requires that new developments minimise the use of mains water and promote the use of rainwater harvesting.

The proposed development is supported by a Drainage Strategy and incorporates a number of positive measures including a green roof, permeable car parking and play areas, and the provision of extensive soft landscaped areas. The development would also incorporate rainwater harvesting to minimise the use of mains water. Accordingly, the development would accord with the aforementioned London Plan Policies.

Overall, the development is considered to provide for an appropriate level of sustainable design which would accord with the requirements set out within Chapter 5 of the London Plan.

7.17 Flooding or Drainage Issues

The site does not fall within a flood zone and no issues relating to flooding have been identified.

London Plan policy 5.13 states that development proposals should use sustainable urban drainage systems (SUDS) unless there are good reasons for not doing so. The proposed development is supported by a Surface Water Design Statement and incorporates a number of positive measures including a green roof, permeable car parking and play areas. The development would also incorporate rainwater harvesting to minimise the use of mains water.

While the indicative details are acceptable additional detailed documentation is required in relation to these matters to ensure full policy compliance and no objections can be reasonably raised, subject to conditions relating to surface water management.

The Local Planning Authority's condition is considered to be comprehensive and addresses the issues normally raised by sustainable urban drainage systems and, as such, this condition has been attached in this instance.

7.18 Noise or Air Quality Issues

NOISE

Policies OE1 and OE3 of the Local Plan Part Two: Saved Policies UDP seek to ensure that developments which have the potential to generate noise are only permitted where their impacts can be mitigated to an acceptable level. In order to address this policy requirement the application is supported by a noise assessment.

The submitted environment noise assessment has been reviewed by the Council's Environmental Protection Unit and subject to conditions to secure mitigation measures it is not considered that the proposal would have any adverse impacts on the amenity of nearby occupiers due to noise.

The applicant has proposed hours of opening between 0800h to 1800h Mondays to Fridays and between 0900h to 1800h Saturdays and not at all on Sundays and Bank Holidays. As previously explained the school buildings are offset some distance from residential boundaries. Furthermore, the new school is adjacent to an existing school. Officers have not sought to restrict hours of use of the school buildings given these factors and given that such a condition could unduly prohibit the education/ community function of the site.

The development would provide staff parking spaces for 32 vehicles and 23 spaces within the drop off/ pick up facility area. The staff parking spaces would be accessed from within an amended existing access to the north of the site with the driveway located some 20 metres distance from the nearest dwelling at no.1 Laurel Lane. The vehicular activity associated with the pupils drop off/ pick up facility would be mainly located towards the south corner of the site and would be located some 20 metres from the nearest dwelling at no. 67 Rowan Road, which would be no different from that that currently exists on the north side of the site.

As such, it is not considered that the vehicular activity through the existing and proposed access roads would cause an unacceptable increase in disturbance to the adjacent properties. It is considered that the increase in activity from within the school itself would not result in noise and disturbance that could materially harm the living conditions of the adjacent occupants whilst noting that playground areas and multi-purpose sports areas are proposed centrally within the site where they are at the maximum possible distance from nearby residential dwellings.

Overall, it is considered that the proposal would not result in such an increase in activities, noise and disturbance to the living conditions of adjacent properties and is considered to be acceptable for this reason. The proposal would therefore comply with policies OE1 and OE3 of the Local Plan Part Two: Saved Policies UDP.

AIR QUALITY

The site falls within Air Quality Management Area and, as such an Air Quality Assessment has been submitted in support of the application.

The Council's Environmental Protection Unit have reviewed the submitted Air Quality Assessment and consider that the development would not give rise to any significant impacts on local air quality, subject to the mitigation measures specified within the application documentation including compliance with a Green Travel Plan.

7.19 Comments on Public Consultations

Councillor Jan Sweeting concerns included in Points 1 and 3 of her letter, the concerns raised by John McDonnell MP together with points (i), (ii), (iv), (v), (vi), (vii), (viii), (ix), (x),

(xi), (xvii) and (xviii) are addressed within the body of the report.

Issue 2 and 4 raised in Councillor Jan Sweeting letter, the concerns raised by John McDonnell MP and points (iii) and (xii) suggest that existing schools, and in particular the adjacent Longmead Primary School, should be expanded instead of building a new school at the Land Adjacent to Laurel Lane. This site has been assessed and discounted as unsuitable in the submitted Planning Statement. Notably, Longmead Primary School has been discounted on planning grounds primarily due to the loss of playing fields, which is resisted by London Plan Policy. In addition, Longmead Primary School is an Academy and therefore it is not completely under Council's control.

The demand for additional primary school places in Hillingdon has experienced sustained growth for a number of years now. During the last six years the number of children 'on-roll' at a Hillingdon primary school has increased by 15%, equating to more than 3,500 children. The Council, working closely with schools and other partners has successfully kept pace with the demand by providing additional school places as close to home as possible.

In West Drayton, there is a need for a new primary school which is being driven by rising birth rates, new housing developments as well as families choosing to move in to the area. The proposed site for the new school is therefore well placed to meet this need. In terms of proximity from the new housing development on Porters Way, the entrance to the Laurel Lane school site is approximately 0.7 miles walking distance which is considered a reasonable distance when the Local Authority makes offers for a primary school place (this is based on walking from Porters Way, via Station Road onto Harmondsworth Road and then onto Laurel Lane).

With regard to the site at the bottom of Porters Way & Lavender Rise, this is open space, and its loss would result in a major deficiency in open space provision in that part of the Borough, in particular for those living within the Porters Way & NATS developments and the surrounding residential areas. The development of the former NATS site to provide 775 dwellings was reliant on this open space (it is referenced in the officer planning report as required to be retained to make the development acceptable). The planning consent was subject to S106 obligations requiring financial contributions towards the enhancement of this open space (which has already included investment in the children's play area).

Furthermore there are no alternative sites nearby which could offset the loss of this open space if a development here was brought forward.

Issue (xvii) raises concerns regarding the loss of a right of ways. Although the proposal would involve the loss of the path where the current right of way exists the public right of way itself would be diverted to run alongside Laurel Lane.

Point (xiii) raises concern that the proposal would give rise to anti-social behaviour. While the proposal would obviously introduce additional activity within the the area by way of creating a new school there is no evidence to suggest that the new school would cause anti-social behaviour.

Point (xiv) suggests that inadequate consultation has been carried out. The Local Planning Authority has consulted over 800 local residents and posted site and press notices. The period for consultation was extended to 4 July 2014. This exceeds statutory guidelines.

Point (xv) raises concern over impacts from the demolition and construction works. The applicant is required to have a Construction Project Plan for the duration of the construction works. The development would also be subject to environmental protection legislation.

Issue (xviii) concerns the impact of the development on property prices. This is not a material planning issue that can carry material weight in the considerations of this application.

Point (xix) is concerned with the opening of a lane between the properties at nos. 115 and 117 Wise Lane. The application site is located adjacent to Laurel Lane and there is no connection proposed to the lane between these two properties, i.e. there is no intention to re-open the footpath

The concerns raised within the objection petition are considered to be fully addressed within the body of the report.

7.20 Planning obligations

A Statement of Intent has been offered confirming the applicant is willing to make a monetary contribution totalling £5,000.00 towards tree planting and landscaping enhancements in the adjacent Closes Recreational Grounds. In addition, the final Statement of Intent shall include the applicant's commitment to further highway studies and highway improvements.

As the development is for educational use it would not necessitate a contribution towards the Council and Mayoral Community Infrastructure Levy.

7.21 Expediency of enforcement action

Not applicable.

7.22 Other Issues

The applicant has submitted a Geotechnical Investigation Report in support of the application. This confirms that some testing has been carried out and that some contamination has been identified. Officers in the Council's Environmental Protection Unit have advised that further testing should be carried out. Whilst no objections have been raised, it is recommended that conditions should be attached, should approval be granted, to require further details of testing, investigation and remediation measures.

8. Observations of the Borough Solicitor

General

Members must determine planning applications having due regard to the provisions of the development plan so far as material to the application, any local finance considerations so far as material to the application, and to any other material considerations (including regional and national policy and guidance). Members must also determine applications in accordance with all relevant primary and secondary legislation.

Material considerations are those which are relevant to regulating the development and use of land in the public interest. The considerations must fairly and reasonably relate to the application concerned.

Members should also ensure that their involvement in the determination of planning applications adheres to the Members Code of Conduct as adopted by Full Council and also the guidance contained in Probity in Planning, 2009.

Planning Conditions

Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal. Planning conditions should only be imposed where Members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

Planning Obligations

Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act 1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

Equalities and Human Rights

Section 149 of the Equalities Act 2010, requires the Council, in considering planning applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The requirement to have due regard to the above goals means that members should consider whether persons with particular protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic. Where equalities issues arise, members should weigh up the equalities impact of the proposals against the other material considerations relating to the planning application. Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be taken into account in weighing up the merits of an application. The weight to be given to any equalities issues is a matter for the decision maker to determine in all of the circumstances.

Members should also consider whether a planning decision would affect human rights, in particular the right to a fair hearing, the right to respect for private and family life, the protection of property and the prohibition of discrimination. Any decision must be proportionate and achieve a fair balance between private interests and the public interest.

9. Observations of the Director of Finance

10. CONCLUSION

The proposal seeks full planning permission for the erection of a new five form of entry junior school to occupy an irregularly shaped plot located at the eastern side of Laurel Lane. The school site itself will occupy an area of less than 1.23 hectares. The proposal would encompass the provision of hard and soft playspace for the children, extensive landscaping works and the provision of associated car parking with a drop off/pick up area also to be provided.

The Education Act 1996 states that Local Authorities have a duty to educate children within their administrative area. The Hillingdon Primary Capital Schools Programme is part of the Council's legal requirement to meet the educational needs of the borough. In recent

years the borough has seen a rise in birth rates. This growth in the birth rate, combined with net immigration and new large scale housing developments in the borough has meant that there is now a significant need for additional primary school classrooms across the borough and the need for a new primary school in the south of the borough.

The proposal would result in the loss of a portion of open space. However, the increased demand for school places is compelling and well evidenced and it has been demonstrated that there are no alternative sites which could reasonably be utilised to meet this need.

The proposed development would be subject to extensive landscaping, including substantial new tree planting, has been designed in order to minimise the use of land and minimise its impact on the streetscene. It is considered that having established the need for a facility the development, so far as is possible, the design minimises the impacts of the development and would sit comfortably within its surrounding context.

In relation to ecology the application is supported by detailed ecological information and aspects of the development, such as the green roof and extensive landscaping, would serve to mitigate the loss of habitat which would occur. Subject to conditions the proposal would not be unacceptable in terms of ecological impacts.

The proposal is supported by a detailed Transport Assessment and provides for a combination of physical and management measures to reduce its impact on the public highway. The Council's Highways Engineers have reviewed this information in detail and conclude that overall the proposed development would be acceptable in terms of traffic impact, pedestrian and highway safety.

The school is well separated from surrounding residential properties and subject to appropriate conditions it would have no unacceptable impacts on the amenity of neighbouring residential occupiers by way of dominance, loss of light, loss of privacy or noise.

Subject to conditions, the development has also been designed to fully accord with all requirements relating to accessible design, sustainable design and flood risk.

In conclusion, the provision of an educational facility in the form of a junior school in this site outweighs the harm which would arise from the development in other respects, including through loss of open space. Having regard to this educational need, the development has been well designed to minimise its impacts and would comply with all other relevant planning policies.

The application is recommended for approval, subject to conditions and a statement of intent securing tree planting and landscaping enhancements to the Closes Recreational Grounds and a non-monetary contribution towards highway studies and highway improvements.

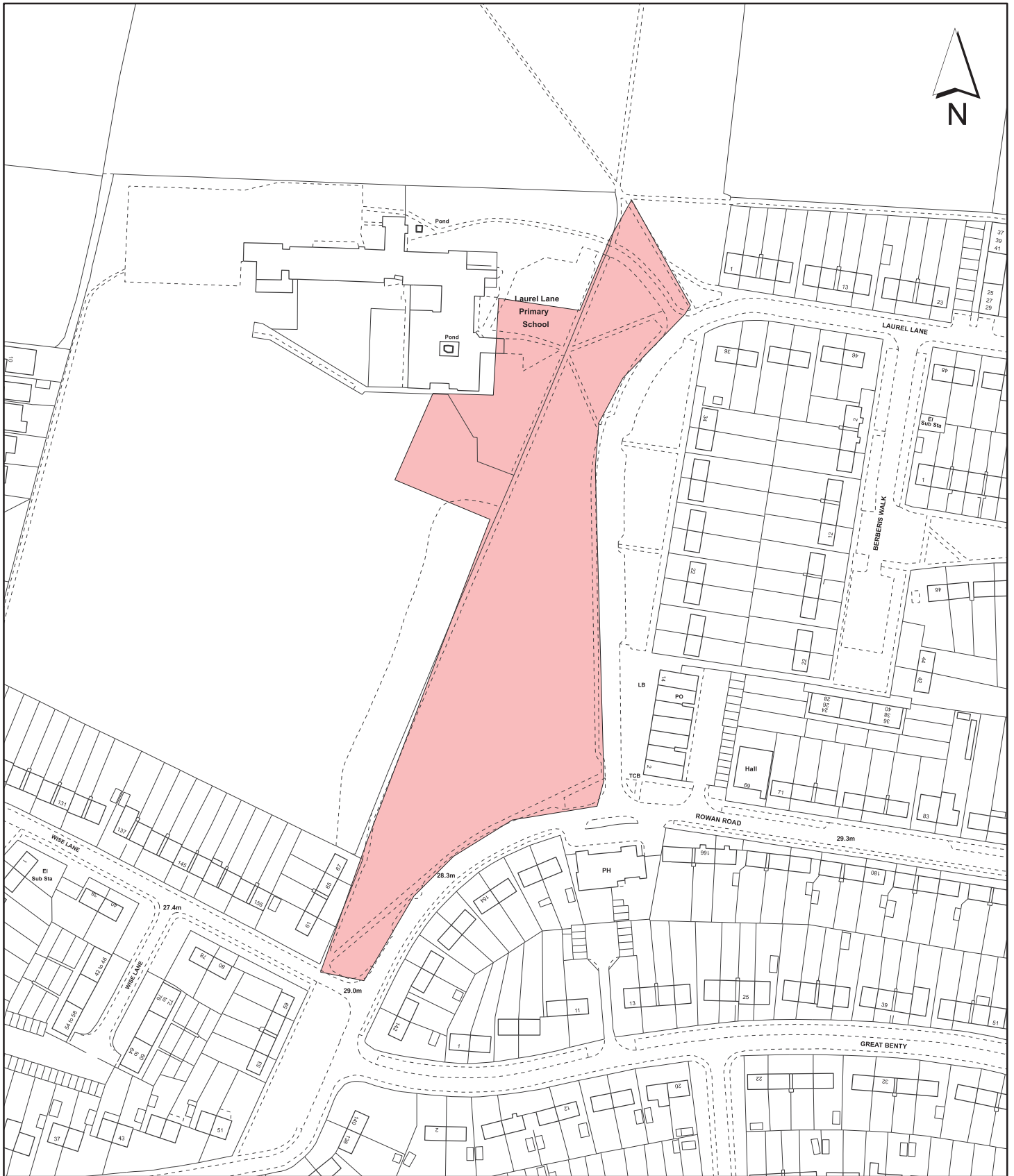
11. Reference Documents

Hillingdon Local Plan: Part 1 - Strategic Policies (November 2012)
Hillingdon Local Plan: Part 2 - Saved UDP Policies (November 2012)
Policy Statement - Planning for Schools Development (DCLG, 15/08/11)
London Plan (July 2011)
National Planning Policy Framework
Hillingdon Supplementary Planning Document: Accessible Hillingdon (May 2013)
Hillingdon Supplementary Planning Document: Residential Layouts

Hillingdon Supplementary Planning Guidance - Community Safety by Design
Hillingdon Supplementary Planning Guidance - Noise
Hillingdon Supplementary Planning Document - Air Quality
Hillingdon Supplementary Planning Guidance - Land Contamination
Hillingdon Supplementary Planning Document - Planning Obligations

Contact Officer: Tiago Jorge

Telephone No: 01895 250230



Notes

 Site boundary

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Site Address

**Land To The West Of Laurel Lane
 West Drayton**

Planning Application Ref:
70019/APP/2014/1807

Planning Committee
Major Page 52

Scale
1:2,000

Date
August 2014

**LONDON BOROUGH
 OF HILLINGDON**

**Residents Services
 Planning Section**
 Civic Centre, Uxbridge, Middx. UB8 1UW
 Telephone No.: Uxbridge 250111



HILLINGDON
 LONDON

Report of the Head of Planning, Sport and Green Spaces

Address FORMER RAF EASTCOTE LIME GROVE RUISLIP

Development: Section 73 Amendment Application to vary the internal layout of the Community Centre, to retain pillars and subdivide the approved open plan layout (varying condition 11 of permission ref: 10189/APP/2007/3383 which approved the re-development of the former RAF Eastcote site).

LBH Ref Nos: 10189/APP/2014/1842

Drawing Nos: 789006/01
letter dated 18/10/2007
letter dated 28 May 2014
5585-WIM-WL-LOC100-PRELIM-17 06 14
5585-WIM-WL-M-P1 H - 13 05 2014

Date Plans Received: 28/05/2014 **Date(s) of Amendment(s):** 27/06/2014
Date Application Valid: 02/06/2014 28/05/2014

1. SUMMARY

This is a section 73 application which seeks to vary condition 11 of outline planning permission ref: 10189/APP/2007/3383, relating to the provision of a community facility on the former RAF Eastcote site.

The community facility with a gross floorspace of 188m² was approved as an open plan space. However, the facility has been built with 9 columns that subdivide the space. The application therefore seeks to retain the the community facility 'as built' with columns, which would allow for the space to be opened up with sliding folding partitions.

3 letters, together with a petition bearing 29 signatures have been received objecting to the proposal.

Objections are raised proposed amendment to retain the community facility as built, as it is not considered fit for purpose. The as built facility (with columns) would limit the choice of end user and severley curtail the intended multi functional use of the space, which was approved as open plan. As such, the amendment to vary condition 11 of the outline consent cannot be supported.

Refusal is recommended accordingly.

2. RECOMMENDATION

REFUSAL for the following reasons:

1 NON2 Non Standard reason for refusal

The as built facility (with columns) would limit the choice of end user and severley curtail the intended multi functional use of the space, which was approved as open plan. The proposed facility would fail to ensure that a suitable range of uses appropriate for a small local community facility could be satisfactorily accommodated. As such the amendment to vary condition 11 of the outline consent cannot be supported and conflicts with Policies R11 and R17 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November

2012) and the adopted Supplementary Planning Document 'Planning Obligations.

2 NON2 Non Standard reason for refusal

The applicant has failed to secure or provide improvements of services and facilities as a consequence of demands created by the proposed development (in respect of a community building, pedestrian networks, affordable housing, public open space, children's play space and protection of trees.. The scheme therefore conflicts with Policy R17 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and the adopted Supplementary Planning Document 'Planning Obligations.

INFORMATIVES

1 I52 Compulsory Informative (1)

The decision to REFUSE planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

2 I53 Compulsory Informative (2)

The decision to REFUSE planning permission has been taken having regard to the policies and proposals in the Hillingdon Unitary Development Plan Saved Policies (September 2007) as incorporated into the Hillingdon Local Plan (2012) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including the London Plan (July 2011) and national guidance.

AM13	AM13 Increasing the ease of movement for frail and elderly people and people with disabilities in development schemes through (where appropriate): - (i) Dial-a-ride and mobility bus services (ii) Shopmobility schemes (iii) Convenient parking spaces (iv) Design of road, footway, parking and pedestrian and street furniture schemes
AM14	New development and car parking standards.
AM15	Provision of reserved parking spaces for disabled persons
AM2	Development proposals - assessment of traffic generation, impact on congestion and public transport availability and capacity
AM7	Consideration of traffic generated by proposed developments.
AM9	Provision of cycle routes, consideration of cyclists' needs in design of highway improvement schemes, provision of cycle parking facilities
BE13	New development must harmonise with the existing street scene.
BE19	New development must improve or complement the character of the area.
BE21	Siting, bulk and proximity of new buildings/extensions.
BE23	Requires the provision of adequate amenity space.
BE24	Requires new development to ensure adequate levels of privacy to neighbours.
BE38	Retention of topographical and landscape features and provision of new planting and landscaping in development proposals.
BE4	New development within or on the fringes of conservation areas
EC2	Nature conservation considerations and ecological assessments

EC3	Potential effects of development on sites of nature conservation importance
EC5	Retention of ecological features and creation of new habitats
H4	Mix of housing units
H5	Dwellings suitable for large families
LE4	Loss of existing industrial floorspace or land outside designated Industrial and Business Areas
OE1	Protection of the character and amenities of surrounding properties and the local area
OE5	Siting of noise-sensitive developments
OE7	Development in areas likely to flooding - requirement for flood protection measures
OE8	Development likely to result in increased flood risk due to additional surface water run-off - requirement for attenuation measures
R10	Proposals for new meeting halls and buildings for education, social, community and health services
R11	Proposals that involve the loss of land or buildings used for education, social, community and health services
R17	Use of planning obligations to supplement the provision of recreation, leisure and community facilities
HDAS-LAY	Residential Layouts, Hillingdon Design & Access Statement, Supplementary Planning Document, adopted July 2006
LDF-AH	Accessible Hillingdon , Local Development Framework, Supplementary Planning Document, adopted January 2010
SPD-NO	Noise Supplementary Planning Document, adopted April 2006
SPD-PO	Planning Obligations Supplementary Planning Document, adopted July 2008
SPG-AQ	Air Quality Supplementary Planning Guidance, adopted May 2002
LPP 5.1	(2011) Climate Change Mitigation
LPP 5.12	(2011) Flood risk management
LPP 5.13	(2011) Sustainable drainage
LPP 5.2	(2011) Minimising Carbon Dioxide Emissions
LPP 5.7	(2011) Renewable energy
LPP 6.13	(2011) Parking
LPP 7.14	(2011) Improving air quality
LPP 7.3	(2011) Designing out crime
LPP 7.8	(2011) Heritage assets and archaeology
LPP 8.2	(2011) Planning obligations
NPPF	National Planning Policy Framework

3

In this case the Local Planning Authority has worked proactively with the applicant, involving meetings and correspondence, to try and secure a community facility that is fit for purpose.

3. CONSIDERATIONS

3.1 Site and Locality

The former RAF Eastcote site is 7.7 hectares in area and is dissected into a northern and southern area by an existing public footpath. An internal private road links the northern

and southern areas. The northern portion is 4.2 hectares and was formally used as a US Navy facility. The land in this area is undulating, and becomes lower towards the north western boundaries. The southern portion of the site is 3.5 hectares, is generally flat, and formally comprised a number of vacant buildings, previously used by the Ministry of Defence, which have now been demolished. The site has now been substantially built out for residential purposes, in accordance with outline permission (ref: 10189/APP/2004/1781) for the redevelopment of the site for residential purposes at a density of up to 50 dwellings per hectare and the subsequent reserved matters approval ref: 10189/APP/2007/3046.

The site has an average PTAL score of 1b, which is a low score within a possible range of 1 to 6. A number of trees and hedges of varying size and value surround the site boundary and the edge of the public footpath. The site is bounded to the west by Eastcote Road and on all remaining sides by residential properties. To the north the residential character is predominantly 1960/70s in style, with a large number of three storey town houses and flats, many of which have communal garage courts. To the southeast, the area has a larger number of semi-detached two storey dwellings dating to the 1930s. Highgrove Nature Reserve which is of Borough Grade II importance is situated to the south of the site, adjacent to which is Highgrove House. The northwest corner of the site lies adjacent to Eastcote Village Conservation Area, which includes a number of listed buildings.

This application relates to a ground floor community facility located in Block M, adjacent to the L.E.A.P., within the southern portion of the larger site. The consented scheme for this part of the site contains a number of 3 storey apartment blocks and 2 and 3 storey terraced dwellings.

3.2 Proposed Scheme

This S73 application seeks to vary Condition 11 of outline planning permission ref: 10189/APP/2007/3383 relating to the provision of a community facility on the former RAF Eastcote site. It should be noted that most of the site has been built out in accordance with the outline and reserved matters approvals. As such, considerations in this report are mainly limited to the proposed internal amendments to the community facility.

The community facility was approved as an open plan space. The applicant has explained that the community hall was built with 9 columns that subdivide the space, because if it had been built as per the approved plan, the necessary use of a concrete transfer slab as part of the building works would have raised the building ridge height. To avoid this raised ridge height, the engineers built the building with the internal structural columns.

Officers had previously raised concerns about this amendment. To agree a way forward, the applicant has had ongoing discussions and meetings with Council officers. The applicant has now submitted a plan which shows the community facility as built with columns, which allows for the space to be opened up with sliding folding partitions. The application therefore seeks to vary Condition 11 of permission 10189/APP/2007/3383 so that it reads:

"The community building shall be completed in accordance with approved planref: 5585/WIM.W.L/M/P1 Rev H and provide a minimum of 170m of gross floorspace falling within class D1 of the Town and Country Planning Act. The community building shall be completed prior to the occupation of the last dwelling of the development and thereafter be permanently retained on site."

The wording of the condition is proposed to be changed so that it would read as an

informative condition, as opposed to a condition which needs to be discharged. As with the approved plan, a gross floorspace of 188m² would be provided, exceeding the amount required by the relevant planning condition and the S299a agreement.

3.3 Relevant Planning History

Comment on Relevant Planning History

Outline permission (with detailed access) was granted in March 2006 (Council ref: 10189/APP/2004/1781) for the redevelopment of the site for residential purposes at a density of up to 50 no. dwellings per hectare, including affordable housing, live-work units, a community facility and open space. Condition 11 of this consent states:

"Prior to the commencement of development, details of the community facility are to be submitted to and approved in writing by the Local Planning Authority. The community facility is to comprise a minimum of 170m of gross floor space, falling within class D1 of the Town and Country Planning Act. The community facility is to serve local needs. The community facility shall be provided in accordance with the approved details prior to the occupation of the last dwelling of the development and after full consultation with local residents and relevant stakeholders, and thereafter be permanently retained on site."

In February 2008, Condition 40 of the outline permission was amended to remove the requirement for traffic signals on Eastcote Road and on the intersection of Eastcote Road and Fore Street (ref: 10189/APP/2007/3383). Permission 10189/APP/2007/3383 was implemented and is therefore the live permission. Condition 11 of this outline permission reads the same as Condition 11 of the original outline consent. The Decision Notice for permission 10189/APP/2007/3383 states that the reason for this condition is to specify a suitable range of uses appropriate for the small local community facility envisaged by the outline application, in compliance with Policy R17 of the Hillingdon Unitary Development Plan Saved Policies (September 2007).

Further, the provision of a community building (no less than 170m) is provided for by Schedule 3 of the S299a legal agreement, which was signed prior to the issuing of the original outline consent.

Reserved matters approval (ref:10189/APP/2007/3046) for the siting, design, external appearance and landscaping pursuant to the outline consent was granted in March 2008. As part of this consent, the community building floorplan 5585/WIM.W.L/M/P1 Rev B was approved which shows an open plan floor area for the community space. However, the reserved matters permission does not contain a condition relating to the approved plans, hence this S73A application is seeking to vary Condition 11 of the outline consent.

4. Planning Policies and Standards

UDP / LDF Designation and London Plan

The following UDP Policies are considered relevant to the application:-

Part 1 Policies:

- PT1.EM1 (2012) Climate Change Adaptation and Mitigation
- PT1.EM6 (2012) Flood Risk Management
- PT1.EM7 (2012) Biodiversity and Geological Conservation

- PT1.H1 (2012) Housing Growth
- PT1.H2 (2012) Affordable Housing
- PT1.HE1 (2012) Heritage

Part 2 Policies:

- AM13 AM13 Increasing the ease of movement for frail and elderly people and people with disabilities in development schemes through (where appropriate): -
 - (i) Dial-a-ride and mobility bus services
 - (ii) Shopmobility schemes
 - (iii) Convenient parking spaces
 - (iv) Design of road, footway, parking and pedestrian and street furniture schemes
- AM14 New development and car parking standards.
- AM15 Provision of reserved parking spaces for disabled persons
- AM2 Development proposals - assessment of traffic generation, impact on congestion and public transport availability and capacity
- AM7 Consideration of traffic generated by proposed developments.
- AM9 Provision of cycle routes, consideration of cyclists' needs in design of highway improvement schemes, provision of cycle parking facilities
- BE13 New development must harmonise with the existing street scene.
- BE19 New development must improve or complement the character of the area.
- BE21 Siting, bulk and proximity of new buildings/extensions.
- BE23 Requires the provision of adequate amenity space.
- BE24 Requires new development to ensure adequate levels of privacy to neighbours.
- BE38 Retention of topographical and landscape features and provision of new planting and landscaping in development proposals.
- BE4 New development within or on the fringes of conservation areas
- EC2 Nature conservation considerations and ecological assessments
- EC3 Potential effects of development on sites of nature conservation importance
- EC5 Retention of ecological features and creation of new habitats
- H4 Mix of housing units
- H5 Dwellings suitable for large families
- LE4 Loss of existing industrial floorspace or land outside designated Industrial and Business Areas
- OE1 Protection of the character and amenities of surrounding properties and the local area
- OE5 Siting of noise-sensitive developments
- OE7 Development in areas likely to flooding - requirement for flood protection measures
- OE8 Development likely to result in increased flood risk due to additional surface water

	run-off - requirement for attenuation measures
R10	Proposals for new meeting halls and buildings for education, social, community and health services
R11	Proposals that involve the loss of land or buildings used for education, social, community and health services
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HDAS-LAY	Residential Layouts, Hillingdon Design & Access Statement, Supplementary Planning Document, adopted July 2006
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LPP 5.7	(2011) Renewable energy
LPP 6.13	(2011) Parking
LPP 7.14	(2011) Improving air quality
LPP 7.3	(2011) Designing out crime
LPP 7.8	(2011) Heritage assets and archaeology
LPP 8.2	(2011) Planning obligations
NPPF	National Planning Policy Framework

5. Advertisement and Site Notice

5.1 Advertisement Expiry Date:- **27th June 2014**

5.2 Site Notice Expiry Date:- Not applicable

6. Consultations

External Consultees

The application has been advertised under Article 13 of the Town and Country Planning General Development Procedure Order 1995 as a Major Development. Site notices were posted on the site. 540 surrounding property owners/occupiers, and local amenity groups have been consulted on this application. At the time of writing the report, 3 letters have been received objecting on the following grounds:

1. With the changes proposed by Taylor Wimpey, the building will no longer be suitable for its purpose as Community Centre. This is mere attempt to reduce costs at the expense of people who could use the centre.

2. There is already severe congestion on the site and limited parking spaces. Already other people

continue to park in spaces designated for residents. Further housing plots and constant construction have the site severely restricted in terms of road access and free greenland space.

3. Taylor Wimpey have been reapplying and changing their plans for the site continually and we were not told about any of this when we purchased our properties. If we were aware of these then would have serious reservations about moving to this site.

4. Continuous changes on the site lead to disruption of our schedules and cause a lot of disturbance. There are also not enough waste disposal units in place and further congestion on the site creates an environmental impact dilemma.

In addition, a petition bearing 29 signatures has been received objecting to the application for the following reason:

The application goes against the original commitment by the developer to create a fully functional community centre.

Desired outcome: That the community facility be built as originally envisaged, without pillars.

GLREATER LONDON AUTHORITY (GLA)

Since the original outline planning permission was approved prior to April 2008, the application is not referable to the Mayor.

Internal Consultees

7. MAIN PLANNING ISSUES

7.01 The principle of the development

The principle of residential development on this site has already been established by virtue of the outline planning permission. The general layout, design and landscaping of the development has been established by virtue of the reserved matters approvals. As such, no objections are raised in principle to the principle of residential development on this site.

It is considered that this application to vary the outline approval to provide a modified community facility would have only limited local impact on the immediate environment and would not raise fundamental issues in relation to flooding and contamination, ecology, waste disposal and archaeology.

The applicant has argued that there are no provisions in the S299a agreement which require the community hall space to be open plan. In addition, Policy R17 specified in the reason for the condition simply requires community facilities through planning obligations. There is no planning policy requirement for the community hall to be open plan, nor is this required by the condition. The applicant further argues that if the community hall space had been built out in accordance with the approved plans, the condition does not prevent subsequent sub-division, or internal changes to the hall. Such internal works would not require planning permission.

Notwithstanding the above mentioned arguments, details of the community hall were approved under the reserved matters consent ref: 10189/APP/2007/2463 as an open plan facility, which was considered acceptable by Committee at the time. Objections are raised to the proposed amendment to the community facility, as the introduction of 9 columns subdividing the hall would render the space unfit for its intended purpose. The applicant

has stated that the community space could be fitted out with sliding folding partitions which allow the space to be opened up. However, this would not deliver a flexible, functional and practical use of the space, which could be used for a variety of purposes, including a meeting hall.

It is noted that the reason for imposing this condition was to ensure a suitable range of uses appropriate for a small local community facility could be accommodated. Although no end users or operators of the space have been identified to date and no specific community use has been identified, the as built facility (with columns) would limit the choice of end user and severely curtail the intended multi functional use of the space, which was approved as open plan. As such the amendment to vary condition 11 of the outline consent cannot be supported.

7.02 Density of the proposed development

An outline planning permission has already been granted. That application considered the matter of the acceptable density of development for the site and defined this as up to 50 units a hectare. This was stipulated by way of a planning condition on the outline permission. This is a material consideration, which guided the determination of the subsequent reserved matters applications. Both reserved matters applications for the 'approved' and 'alternative' access schemes were approved for the development of 385 residential units, at an average density of 50 dwellings per hectare (dph).

This application seeks only to vary condition 11 of the outline approval, but will not materially increase the density of housing or unit mix on the wider development at the former RAF Eastcote site.

7.03 Impact on archaeology/CAs/LBs or Areas of Special Character

Saved Policy BE4 requires any new development within or on the fringes of a Conservation Area to preserve or enhance those features that contribute to its special architectural and visual qualities, and to make a positive contribution to the character or appearance of the conservation area. Saved Policy BE10 seeks to protect the setting of listed buildings.

There are no archaeological or historic issues associated with this application. It is not considered that the proposed changes would have a direct impact on the character of the Eastcote Village Conservation Area, which is located some distance away to the north of the site, in compliance with Saved Policy BE4 of the Hillingdon Unitary Development Plan.

7.04 Airport safeguarding

There are no airport safeguarding issues related to this development.

7.05 Impact on the green belt

There are no Green Belt issues associated with this site.

7.06 Environmental Impact

Issues relating to land contamination have already been dealt with for the former RAF Eastcote site as a whole. It is not considered that the proposal would raise any further issues in this regard.

7.07 Impact on the character & appearance of the area

Saved Policy BE13 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) states that development will not be permitted if the layout and appearance fail to harmonise with the existing street scene or other features of the area which the local planning authority considers it desirable to retain or enhance. Saved Policy BE19 seeks to ensure that new development within residential areas complements or improves the

amenity and character of the area. Saved Policy BE38 stresses the need to retain and enhance landscape features and provide for appropriate (hard and soft) landscaping in new developments.

The proposed amendments relate only to the internal arrangement of the community hall. As such, there are no changes to the external appearance of Block M. The proposal would therefore not conflict with any of the above mentioned policies.

7.08 Impact on neighbours

Saved Policy BE20 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) states that the Local Planning Authority will seek to ensure that buildings are laid out so that adequate daylight, sunlight and amenities of existing houses are safeguarded. Policy BE21 states that planning permission will not be granted for new development, which by reason of its siting, bulk and proximity, would result in a significant loss of residential amenity of established residential areas. Saved Policy BE24 seeks to ensure that the design of new buildings protects the privacy of the occupiers and their neighbours.

There are no changes to fenestration or layout. As such the proposal would not result in any adverse impacts on the amenity of neighbouring occupiers, in compliance with the above mentioned policies.

7.09 Living conditions for future occupiers

It is considered that the development as proposed would maintain an appropriate environment for future occupiers of the wider estate.

7.10 Traffic impact, car/cycle parking, pedestrian safety

Although car parking and secure cycle parking has not been identified in the application submission, the scheme will not alter the level of car parking provision or traffic generation to the site.

7.11 Urban design, access and security

In respect of security, it is not considered that the proposal would have any significant adverse impacts in terms of security.

7.12 Disabled access

The proposals would not alter the level of accessibility achieved within the development from that previously agreed within the original grant of planning permission.

7.13 Provision of affordable & special needs housing

The proposed amendments would not affect the viability of the development and accordingly the level of affordable housing would not be affected.

7.14 Trees, Landscaping and Ecology

This application relates to internal amendments to the community facility and seeks only to vary condition 11 of the outline approval. The proposal would therefore not alter the development in terms of landscaping, trees and ecology from the scheme previously agreed within the original grant of planning permission on the wider development at the RAF Eastcote site.

7.15 Sustainable waste management

The proposal would not alter the development in terms of waste management or storage from the scheme previously agreed within the original grant of planning permission.

7.16 Renewable energy / Sustainability

The proposal would not alter the development in terms of renewable energy/sustainability from the scheme previously agreed within the original grant of planning permission.

7.17 Flooding or Drainage Issues

There are no specific flooding or drainage issues associated with this application, and the proposal would not alter the development in terms of drainage or flood risk.

7.18 Noise or Air Quality Issues

The proposal would result in no additional impacts on noise or air quality over those considered within the original grant of planning permission.

7.19 Comments on Public Consultations

The comments in the petition are noted and are reflected in the reason for refusal.

The individual comments received do not directly relate to the community facility, but complain about continuing disruption and changes to the originally approved residential scheme.

7.20 Planning Obligations

The original Grant of planning permission secured the following planning obligations by way of a legal agreement:

A financial contribution towards nursery; primary and secondary school places in Ruislip and Eastcote; primary health care facilities; provision of a community facility; improvements in leisure, youth and cultural services; one equipped children's play space on-site; improving pitch sport facilities off-site; off site highway works; improvements to the public right of way; improvements to the London Cycle network; installation of a pedestrian crossing over Elm Avenue; protection of trees; improvements to the habitat of Highgrove Nature Reserve, including upgrading the path network; public consultation; construction management and affordable housing.

Most of the above mentioned obligations have been discharged. In terms of this S73 application, the provision of a community building (no less than 170m) is secured by Schedule 3 of the above mentioned S299a legal agreement. This should be provided prior to the occupation of the last dwelling of the development, which has not yet occurred. The applicants are therefore not in breach of this obligation at this time. Although there are no provisions in the S299A agreement which require the community hall space to be open plan, details of the community hall were approved as such, under the reserved matters consent ref: 10189/APP/2007/2463.

Notwithstanding the above, there are ongoing obligations which need to be secured in order that the development is acceptable in planning terms. This is reflected in the reasons for refusal.

7.21 Expediency of enforcement action

Condition 11 of the outline consent requires the community facility to be provided prior to the occupation of the last dwelling of the development and after full consultation with local residents and relevant stakeholders, and thereafter be permanently retained on site. Since the development is still under construction and the last dwelling has not yet been occupied, the applicants are not technically in breach of this condition. There are therefore no enforcement issues relating to the community facility at this time.

7.22 Other Issues

None.

8. Observations of the Borough Solicitor

General

Members must determine planning applications having due regard to the provisions of the development plan so far as material to the application, any local finance considerations so

far as material to the application, and to any other material considerations (including regional and national policy and guidance). Members must also determine applications in accordance with all relevant primary and secondary legislation.

Material considerations are those which are relevant to regulating the development and use of land in the public interest. The considerations must fairly and reasonably relate to the application concerned.

Members should also ensure that their involvement in the determination of planning applications adheres to the Members Code of Conduct as adopted by Full Council and also the guidance contained in Probity in Planning, 2009.

Planning Conditions

Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal. Planning conditions should only be imposed where Members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

Planning Obligations

Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act 1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

Equalities and Human Rights

Section 149 of the Equalities Act 2010, requires the Council, in considering planning applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The requirement to have due regard to the above goals means that members should consider whether persons with particular protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic. Where equalities issues arise, members should weigh up the equalities impact of the proposals against the other material considerations relating to the planning application. Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be taken into account in weighing up the merits of an application. The weight to be given to any equalities issues is a matter for the decision maker to determine in all of the circumstances.

Members should also consider whether a planning decision would affect human rights, in particular the right to a fair hearing, the right to respect for private and family life, the protection of property and the prohibition of discrimination. Any decision must be proportionate and achieve a fair balance between private interests and the public interest.

9. Observations of the Director of Finance

Not Applicable.

10. CONCLUSION

Details of the community hall were approved under the reserved matters consent ref: 10189/APP/2007/2463 as an open plan facility, which was considered acceptable by Committee. It is considered that the introduction of 9 columns subdividing the hall would render the space unfit for its intended purpose. The community hall 'as built' would fail to deliver a flexible, functional and practical use of the space which could be utilised for a variety of purposes, including a meeting hall. The 'as built' facility (with columns) would limit the choice of end user and severely curtail the intended multi functional use of the space, which was approved as open plan. As such the application to vary condition 11 of the outline consent is not supported.

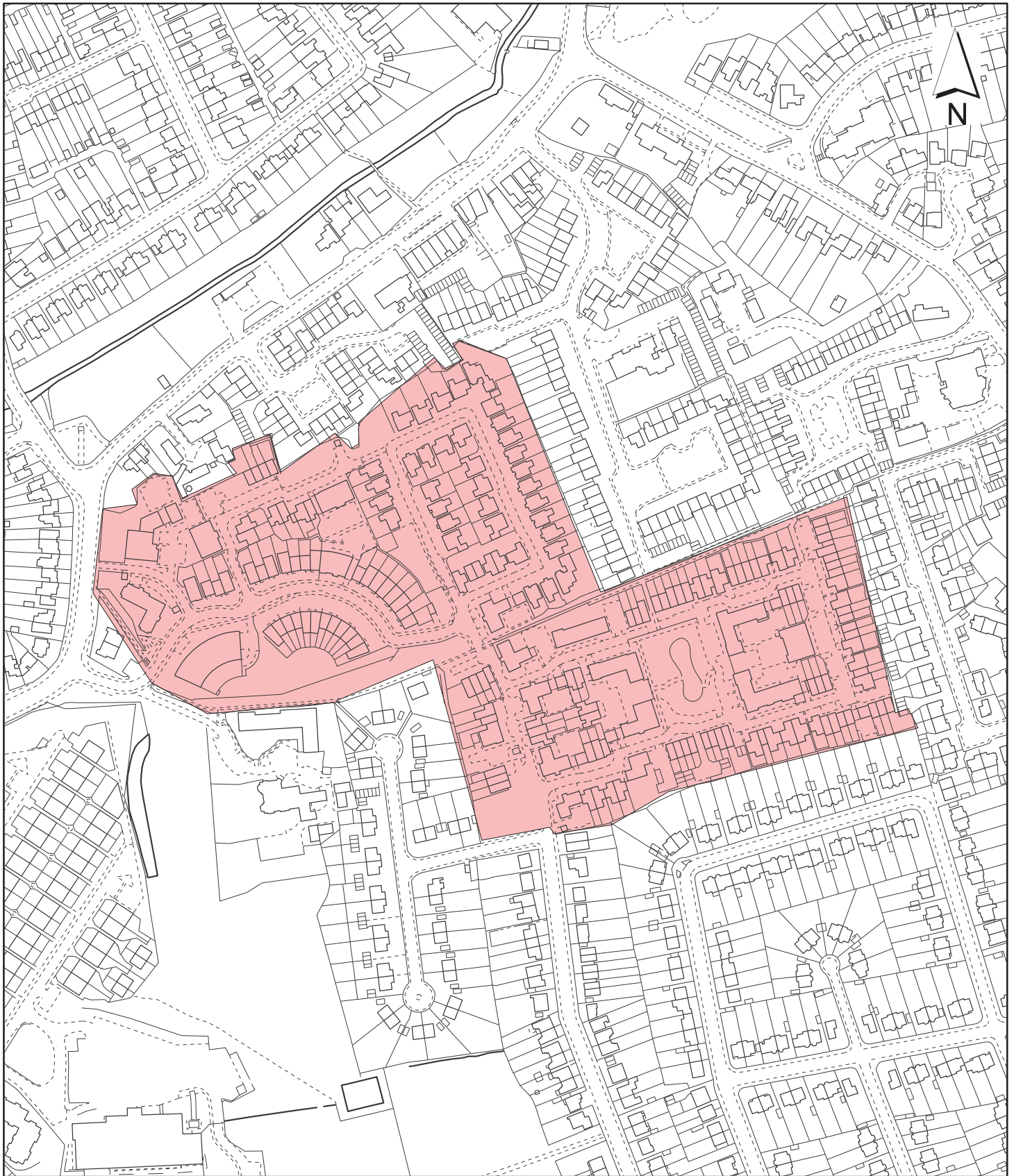
Refusal is recommended accordingly.

11. Reference Documents

The Hillingdon Local Plan: Part 1- Strategic Policies (8th November 21012)
Hillingdon Local Plan: Part 2 - Saved UDP Policies (November 2012)
London Plan 2011
National Planning Policy Framework (NPPF)
Supplementary Planning Document: Accessible Hillingdon
Supplementary Planning Document: Residential Layouts
Supplementary Planning Guidance: Community Safety by Design
Supplementary Planning Document: Affordable Housing
Supplementary Planning Guidance: Planning Obligations Strategy
Letters of objection and petition against the development

Contact Officer: Karl Dafe

Telephone No: 01895 250230



Notes

 Site boundary

For identification purposes only.
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Site Address

**Former RAF Eastcote
 Lime Grove
 Ruislip**

Planning Application Ref:

10189/APP/2014/1842

Planning Committee

Major Page 66

Scale

1:3,200

Date

August 2014

**LONDON BOROUGH
 OF HILLINGDON**

**Residents Services
 Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW
 Telephone No.: Uxbridge 250111



HILLINGDON
 LONDON

Report of the Head of Planning, Sport and Green Spaces

Address	ENTERPRISE HOUSE, 133 BLYTH ROAD HAYES
Development:	Erection of extensions at roof level, erection of external bridge links on the rear elevation and internal works associated with the change of use of part of enterprise house to create 96 residential units (class C3) and associated car parking, retention of approximately 4,500 sqm (GIA) of employment use (Use classes B1 (a-c) and B2 with ancillary cafe) at ground and first floor levels and cafe.
LBH Ref Nos:	11623/APP/2013/3606
Drawing Nos:	524 L (--) 302 Rev. P3 Air Quality Assessment Air Quality Assessment - Owen Coyle (Anodising) Ltd Flue Modelling 524 L (--) 104 Rev. P5 2013-1419-AT-109 2013-1419-AT-115 524 L (--) 100 Rev. P7 524 L (--) 101 Rev. P10 524 L (--) 102 Rev. P5 524 L (--) 107 Rev. P7 Building Services Report, Rev. 03 Energy Statement, Rev. D 3733-S-109 Rev. P2 3733-S-110 Rev. P2 3733-S-111 Rev. P2 3733-S-112 Rev. P2 3733-S-113 Rev. P2 3733-S-114 Rev. P2 3733-S-120 Rev. P2 3733-S-121 Rev. P2 3733-S-122 Rev. P2 3733-S-123 Rev. P2 Daylight and Sunlight Assessment Phase 1 Ecological Appraisal Economic Benefits Assessment Heritage Impact Assessment Planning Statement Sustainability Statement, Rev. F 3733-D-01 Rev. P1 3733-D-02 Rev. P1 524 L (38) 100 Rev. P0 3733-S-106 Rev. P2 Design and Access Statement Noise Survey and Assessment Transport Statement Travel Plan Utility Report Agent's email dated 9 4 14 Assessment of Residential Amenity 524 A (31) 103 Rev. P0

524 A (31) 104 Rev. P0
524 A (31) 105 Rev. P0
524 A (31) 106 Rev. P0
524 C (31) 101 Rev. P0
524 C (31) 102 Rev. P0
524 C (31) 103 Rev. P0
524 C (31) 104 Rev. P0
524 C (31) 105 Rev. P0
524 C (31) 106 Rev. P0
524 L (--) 310 Rev. P2
524 L (--) 312 Rev. P2
3733-S-101 Rev. P2
3733-S-102 Rev. P2
3733-S-103 Rev. P2
3733-S-104 Rev. P2
3733-S-105 Rev. P2
3733-S-107 Rev. P2
3733-S-108 Rev. P2
Waste Management Strategy
Stage (C) Report
Report - Below Ground Drainage and Rainwater Harvesting,
Supplementary Information for Planning, January 2014, Rev. A
SK10.1
SK10.2
524 L (--) 313 Rev. P3
524 L (--) 301 Rev. P4
524 L (--) 303 Rev. P2
524 L (--) 311 Rev. P3
524 L (--) 210 Rev. P3
524 L (--) 211 Rev. P3
524 L (--) 300 Rev. P4
524 L (--) 201 Rev. P4
524 L (--) 600 Rev. P2
524 L (--) 106 Rev. P6
524 L (--) 200 Rev. P4
524 L (31) 300 Rev. P0
524 L (--) 601 Rev. P1
524 C (22) 102 Rev. P0
524 L (24) 100 Rev. P0
524 L (24) 200 Rev. P0
524 C (22) 101 Rev. P0
524 L (24) 201 Rev. P0
524 A (31) 102 Rev. P0
524 A (31) 101 Rev. P0
524 L (--) 108 Rev. P3
524 L (--) 10 Rev. P2
Agent's email dated 9/4/14
Agent's letter dated 25/4/14
524 L (0-) 203 Rev. P0
524 L (0-) 011 Rev. P1
524 L (0-) 012 Rev. P1

524 L (0-) 013 Rev. P1
 524 L (0-) 014 Rev. P1
 524 L (0-) 015 Rev. P1
 524 L (0-) 016 Rev. P1
 524 L (0-) 017 Rev. P1
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 524 L (0-) 031 Rev. P1
 524 L (0-) 032 Rev. P1
 524 L (0-) 033 Rev. P1
 524 L (0-) 034 Rev. P1
 524 A (0-) 100 Rev. P0
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 524 C (22) 100 Rev. P0
 524 C (31) 110 Rev. P0
 524 C (31) 200 Rev. P0
 524 L (32) 100 Rev. P0
 524 L (--) 202 Rev. P2
 524 L (--) 212 Rev. P2

Date Plans Received:	16/11/2013	Date(s) of Amendment(s):	25/06/2014
Date Application Valid:	21/01/2014		22/01/2014
			09/04/2014
			25/04/2014
			03/04/2014
			08/05/2014
			05/12/2013

1. SUMMARY

Enterprise House is a large, distinctive six-storey, Grade II listed industrial building which is located within the Blyth Road/ Printing House Lane Industrial and Business Area and forms part of the Botwell: Thorn EMI Conservation Area. The building is in a poor state of repair and has been included on English Heritage's Heritage at Risk register, with many parts of the building being unsafe to use and its elevations having to be clad in safety netting. The site is opposite the Vinyl Factory site which is seen by many people as the flagship Hayes regeneration scheme.

The proposal is for the restoration, refurbishment, conversion and extension of Enterprise House to provide a mixed use scheme comprising approximately 4,500sqm GIA of employment space (Use classes B1 (A-C) and B2 with ancillary cafe) on the ground and first floors and 96 residential units (Use Class C3) on the upper floors. The building would be extended at roof top level. The scheme has formed the subject of much discussion with officers.

Although residential use is not appropriate within an IBA, strategic planning policies do suggest that the limited release from industrial to other uses of parts of the borough, may be appropriate and one such site that has been identified within the emerging Hillingdon Local Plan is Enterprise House.

A Financial Viability Assessment has been submitted and its findings have been verified by the Council's independent financial consultant. This considered the viability of three development options for the building to generate the necessary funds for its repair and refurbishment. The first two options considered full commercial use of the building, with and without extension. Both the commercial options make substantial losses and are completely unviable. The residential option also makes a loss but a relatively small one and only a small uplift in property prices would be required for the scheme to become viable.

Although the residential scheme is not ideal in terms of the open character of the listed industrial building, requiring extensive subdivision on the upper floors, the ground and first floors of the building would remain in commercial use and be more open. The roof top extension is also not ideal, but it has been sensitively designed. On this basis, English Heritage and the Council's Urban Design/Conservation Officer do not object to the proposed works.

The scheme would secure the renovation of this important Grade II listed building, bringing it back into full use. The commercial element on the ground and first floors would focus on the music/media industries, with the ground floor in the middle of the building being occupied and anchored by The Vinyl Factory (the last remaining vinyl manufacturing company in the UK) and also include recording and broadcasting studios and a publically accessible cafe. The uses will bring vitality and visual interest to this part of Blyth Road and create employment and assist in the regeneration of the area.

The constraints of the site and the limitations of the layout of the listed building, combined with the overriding requirement for the scheme to generate funds for the building's restoration and remain financially viable has resulted in some compromises being made in terms of the residential accommodation proposed. However, all the units would satisfy the Mayor's floor space standards and the vast majority of habitable rooms would have a reasonable outlook and natural lighting. Although little amenity space is provided, public open space is located nearby.

Furthermore, adjoining residents would not be adversely affected by the proposals, traffic impacts upon the adjoining highway network are acceptable. The scheme has been conditioned to ensure that it makes an appropriate contribution towards energy efficiency and carbon reductions and flood risk would be appropriately mitigated.

It is therefore considered that on balance, this scheme is acceptable and is recommended accordingly.

2. RECOMMENDATION

That delegated powers be given to the Head of Planning, Green Spaces and

Culture to grant planning permission, subject to the following:

A) That the Council enter into a legal agreement with the applicants under Section 106/Unilateral Undertaking of the Town and Country Planning Act 1990 (as amended) or other appropriate legislation to secure:

1. Affordable Housing: A review mechanism allowing the council to review the value of the scheme in the future to secure affordable housing.

2. Highways: Highways works together with a S278/S38 Agreement will need to be secured to include details of the operation and management of the car club; residential visitors parking in the commercial car parking spaces during evenings and weekends, a permit free agreement to exclude the future residents of the development from eligibility for the parking permits for the adjacent CPZ and Travel Plan.

3. Conservation/Management Plan to be agreed for the future maintenance of the building

4. Construction Training

- Training Costs: £2500 per £1m build cost

- Coordinator costs: $96/160 \times £71,675 + 4500/7500 \times £71,675 = £86,010$

5. Air Quality monitoring fee: £25,000

6. Project Management & Monitoring Fee: 5% of total cash contributions.

B) That in respect of the application for planning permission, the applicant meets the Council's reasonable costs in the preparation of the S106 Agreement and any abortive work as a result of the agreement not being completed.

C) That officers be authorised to negotiate and agree the detailed terms of the proposed agreement and conditions of approval.

D) That if any of the heads of terms set out above have not been agreed and the S106 legal agreement has not been finalised before the 31st August 2014, or any other period deemed appropriate that delegated authority be given to the Head of Planning, Green Spaces and Culture to refuse the application for the following reason:

'The applicant has failed to provide a commensurate package of planning benefits to maximise the environmental and social benefits, namely the planned maintenance of the building, review mechanism to re-assess affordable housing, highway improvements, construction training and project management of the scheme to the community. The proposal therefore conflicts with Policy R17 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).'

E) That subject to the above, the application be deferred for determination by the Head of Planning, Green Spaces and Culture under delegated powers, subject to the completion of the legal agreement under Section 106 of the Town and Country Planning Act 1990 and other appropriate powers with the applicant.

F) That should the application be approved following the Council's Community Infrastructure Levy coming into force, the applicant pay the required levy on the additional floorspace actually created.

G) That if the application is approved, the following conditions be attached:-

1 COM3 Time Limit

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

REASON

To comply with Section 91 of the Town and Country Planning Act 1990.

2 NONSC Non Standard Condition

Prior to the commencement of any works on site, the building shall be recorded to English Heritage level 1 and discs/ copies of the document shall be submitted to the Council, local library and Uxbridge Local History Library.

REASON

To ensure that a record of the listed building is provided, in accordance with Policy BE9 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

3 COM4 Accordance with Approved Plans

The development hereby permitted shall not be carried out except in complete accordance with the details shown on the submitted plans, numbers 524 L (--) 100 Rev. P7, 524 L (--) 101 Rev. P10, 524 L (--) 102 Rev. P5, 524 L (--) 104 Rev. P5, 524 L (--) 106 Rev. P6, 524 L (--) 107 Rev. P7, 524 L (--) 108 Rev. P3, 524 L (--) 200 Rev. P4, 524 L (--) 201 Rev. P4, 524 L (--) 202 Rev. P2, 524 L (--) 210 Rev. P3, 524 L (--) 211 Rev. P3, 524 L (--) 212 Rev. P2, 524 L (--) 300 Rev. P4, 524 L (--) 301 Rev. P4, 524 L (--) 302 Rev. P3, 524 L (--) 303 Rev. P2, 524 L (--) 310 Rev. P2, 524 L (--) 311 Rev. P3, 524 L (--) 312 Rev. P2, 524 L (--) 313 Rev. P3, 524 L (--) 600 Rev. P2 and 524 L (--) 601 Rev. P1, 524 L (0-) 011 Rev. P1, 524 L (0-) 012 Rev. P1, 524 L (0-) 013 Rev. P1, 524 L (0-) 014 Rev. P1, 524 L (0-) 015 Rev. P1, 524 L (0-) 016 Rev. P1, 524 L (0-) 017 Rev. P1, 524 L (0-) 030 Rev. P1, 524 L (0-) 031 Rev. P1, 524 L (0-) 032 Rev. P1, 524 L (0-) 033 Rev. P1, 524 L (0-) 034 Rev. P1, 524 L (38) 100 Rev. P0, 524 A (31) 101 Rev. P0, 524 A (31) 102 Rev. P0, 524 A (31) 103 Rev. P0, 524 A (31) 104 Rev. P0, 524 A (31) 105 Rev. P0, 524 A (31) 106 Rev. P0, 524 C (22) 100 Rev. P0, 524 C (22) 101 Rev. P0, 524 C (22) 102 Rev. P0, 524 C (31) 101 Rev. P0, 524 C (31) 102 Rev. P0, 524 C (31) 103 Rev. P0, 524 C (31) 104 Rev. P0, 524 C (31) 105 Rev. P0, 524 C (31) 106 Rev. P0, 524 C (31) 110 Rev. P0, 524 C (31) 200 Rev. P0, 524 L (24) 100 Rev. P0, 524 L (24) 201 Rev. P0, 3733-D-02 Rev. P1, 3733-S-101 Rev. P2, 3733-S-102 Rev. P2, 3733-S-103 Rev. P2, 3733-S-104 Rev. P2, 3733-S-105 Rev. P2, 3733-S-106 Rev. P2, 3733-S-107 Rev. P2, 3733-S-108 Rev. P2, 3733-S-109 Rev. P2, 3733-S-110 Rev. P2, 3733-S-111 Rev. P2, 3733-S-112 Rev. P2, 3733-S-113 Rev. P2, 3733-S-114 Rev. P2, 3733-S-120 Rev. P2, 3733-S-121 Rev. P2, 3733-S-122 Rev. P2 and 3733-S-123 Rev. P2 and shall thereafter be retained/maintained for as long as the development remains in existence.

REASON

To ensure the development complies with the provisions of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) and the London Plan (July 2011).

4 COM5 General compliance with supporting documentation

The development hereby permitted shall not be occupied until the following has been completed in accordance with the specified supporting plans and/or documents:

Encouragement of more sustainable/ active means of travel [Travel Plan]

Thereafter the development shall be retained/maintained in accordance with these details for as long as the development remains in existence

REASON

To ensure that the development complies with the objectives of Policies . Specify Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012)

5 COM7 Materials (Submission)

No development shall take place until details of all materials and external surfaces, have been submitted to and approved in writing by the Local Planning Authority, to include the following:-

- A schedule and full specification for building repairs/renovation to be agreed prior to the start of relevant works on site
- A methodology for cleaning works and samples areas of brickwork and concrete cleaning to be agreed on site
- External paint colours for the elevations, to include brickwork, concrete and windows/downpipes,
- Details of the works to windows, including details of the repair and relocation of existing windows; construction and glazing of new windows (including casements),
- Design details, finishes and materials of all external and internal doors, including roller shutters/boxes and also canopies over,
- Details of materials, appearance and location of new downpipes, hopper heads and external pipe work,
- Details of the design, materials and construction of new and refurbished glazed roofs and the new atrium roof; with existing cowls and vents to be reinstated,
- Schedule and details of repairs to original lifts, lift enclosures and related machinery - works to be completed before the building is occupied
- Details of design and construction of 6th floor altered and new roof top additions, including glazing, cladding/roofing materials and brise soleil
- Details of the repair of existing and the design and construction of new hand rails, balustrades, screens and balcony/roof top enclosures
- Details of roof top plant and machinery
- Location, type and finish of flues, louvers and vents, including low level car park vents
- Details of the design, construction, materials, finishes and method of installation of external walk ways
- Details of the design, construction, materials and finishes of the new eastern stair case enclosure
- Full details of works to existing eastern low level additions
- Fire and sound proofing works
- Details of construction and installation of internal partitions and glazed screens- these are to be reversible
- Details of external works to steps, railings and forecourt area to be agreed
- Design and content of interpretation boards to be agreed

Thereafter the development shall be constructed in accordance with the approved details and be retained as such.

Details should include information relating to make, product/type, colour and photographs/images.

REASON

To ensure that the development presents a satisfactory appearance in accordance with Policy BE13 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012).

6 RES13 Obscure Glazing

The kitchen/living room window(s) facing Jupiter House on the second, third, fourth and fifth floors shall be glazed with permanently obscured glass and non-opening below a height of 1.8 metres taken from internal finished floor level for so long as the development remains in existence.

REASON

To prevent overlooking to adjoining properties in accordance with policy BE24 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012)

7 NONSC Privacy

Details of the obscure /louvered glazing to protect the privacy of windows adjoining the external walkway bridges shall be submitted to and approved by the Local Planning Authority and thereafter, the glazing shall be permanently retained.

REASON

To safeguard the amenity of the residential occupiers, in accordance with Policy BE24 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012).

8 COM9 Landscaping (car parking & refuse/cycle storage)

No development shall take place until a landscape scheme has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include: -

1. Details of Soft Landscaping
 - 1.a Planting plans (at not less than a scale of 1:100),
 - 1.b Written specification of planting and cultivation works to be undertaken,
 - 1.c Schedule of plants giving species, plant sizes, and proposed numbers/densities where appropriate

2. Details of Hard Landscaping
 - 2.a Cycle Storage
 - 2.b Means of enclosure/boundary treatments
 - 2.c Car Parking Layouts (including 20% passive and 10% active electric charging spaces for the commercial use and 20% active and 20% passive electric charging spaces for the residential use.
 - 2.d Hard Surfacing Materials
 - 2.e External Lighting

3. Details of Landscape Maintenance
 - 3.a Landscape Maintenance Schedule for a minimum period of 5 years.
 - 3.b Proposals for the replacement of any tree, shrub, or area of surfing/seeding within the landscaping scheme which dies or in the opinion of the Local Planning Authority becomes seriously damaged or diseased.

4. Schedule for Implementation

5. Other
 - 5.a Existing and proposed functional services above and below ground
 - 5.b Proposed finishing levels or contours

Thereafter the development shall be carried out and maintained in full accordance with the approved details.

REASON

To ensure that the proposed development will preserve and enhance the visual amenities of the locality and provide adequate facilities in compliance with policies BE13, BE38 and AM14 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and Policies 5.11 (living walls and roofs) and 5.17 (refuse storage) of the London Plan (July 2011).

9 NONSC Revised Parking

Notwithstanding the details on Drw. No. 524 L (--) 101 Rev. P10, revised a revised car parking scheme to include 5 motorcycle parking spaces shall be submitted to and approved in writing by the Local Planning Authority.

REASON

In order to ensure that the proposal accords with the Council's adopted car parking standards, in accordance with Policy AM14 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2013).

10 NONSC Parking Allocation

The development shall not be occupied until a parking allocation and management plan to include visitor parking and electric charging spaces and the main allocation of the resident's parking to the 2 and 3 bedroom flats has been submitted to, and approved in writing by, the Local Planning Authority. Thereafter the parking shall remain allocated for the use of the units in accordance with the approved scheme and remain under this allocation for the life of the development.

REASON

To ensure that an appropriate level of car parking provision is provided on site in accordance with Policy AM14 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and Chapter 6 of the London Plan (July 2011).

11 NONSC Refuse

Details of a revised refuse management plan to include on-site management staff bringing bins to a temporary storage area at the back of the Blyth Road footway on collection days shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall be implemented in accordance with the revised refuse management plan which shall be retained for so long as the development remains in existence.

REASON

To ensure that the arrangements would not prejudice highway safety, in accordance with Policy AM7(ii) of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and Chapter 6 of the London Plan (July 2011).

12 NONSC Antenna

The existing telecommunications antenna and satellite dishes at roof level and fixed to the water tanks shall be removed prior to the occupation of the building.

REASON

In order to accord with the terms of the application and in order to safeguard the historical and architectural interest of the listed building, maintain and enhance the appearance and character of the conservation area and to improve the visual amenity of the street scene in accordance with Policies BE4, BE8 and BE13 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

13 NONSC Sound Insulation

The development shall not begin until a sound insulation scheme for the control of noise transmission from commercial to residential and from road/rail noise to residential premises has been submitted to and approved in writing by the Local Planning Authority. The scheme shall be fully implemented before the development is occupied/use commences and thereafter shall be retained and maintained in good working order for so long as the building remains in use.

REASON

To safeguard the amenity of the occupants of surrounding properties in accordance with Policy OE1 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) and Policy 7.15 of the London Plan (July 2011).

14 NONSC Air

Before the development is commenced a scheme designed to minimise the ingress of polluted air and ventilate the car park shall be submitted for approval in writing by the Local planning Authority. The design must take into account climate change pollutants. Any suitable ventilation systems will need to address the following:

- Take air from a clean location or treat the air and remove pollutants;
- Be designed to minimise energy usage;
- Be sufficient to prevent summer overheating;
- Have robust arrangements for maintenance.

Thereafter and prior to occupation, the scheme shall be completed in strict accordance with the approved details and thereafter maintained for the life of the development.

REASON

In order to safeguard the the character and heritage value of the listed building, in accordance with Policies BE8 and BE9 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) and Policy 7.14 of the London Plan (July 2011).

15 NONSC Emissions

Before the development is commenced details of any plant, machinery and fuel burnt, as part of the energy provision and the location of the flue for the development shall be submitted to the LPA for approval. This shall include pollutant emission rates at the flues with or without mitigation technologies. The use of ultra low NOx emission gas CHPs and boilers is recommended. The development should as a minimum be 'air quality neutral' and demonstrably below the building emissions benchmark.

Reason

To safeguard the amenity of neighbouring properties in accordance with policy OE1 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

16 NONSC Water management

Prior to commencement, a scheme for the provision of sustainable water management shall be submitted to, and approved in writing by the Local Planning Authority. The scheme shall clearly demonstrate how it follows the strategy set out in the Below Ground Information by the Furness Partnership dated Jan 2014 and incorporates sustainable urban drainage in accordance with the hierarchy set out in Policy 5.15 of the London Plan and will:

- i. provide information on all Suds features including the method employed to delay and

control the surface water discharged from the site and:

- a. calculations showing storm period and intensity and volume of storage required to control surface water and size of features to control that volume.
 - b. any overland flooding should be shown, with flow paths depths and velocities identified as well as any hazards, (safe access and egress must be demonstrated).
 - c. measures taken to prevent pollution of the receiving groundwater and/or surface waters;
 - d. how they or temporary measures will be implemented to ensure no increase in flood risk from commencement of construction.
- ii. provide a management and maintenance plan for the lifetime of the development of arrangements to secure the operation of the scheme throughout its lifetime. Including appropriate details of Inspection regimes, appropriate performance specification, remediation and timescales for the resolving of issues.
 - iii. provide details of the body legally responsible for the implementation of the management and maintenance plan.

The scheme shall also demonstrate the use of methods to minimise the use of potable water through water collection, reuse and recycling and will:

- iii. incorporate water saving measures and equipment.
- iv. provide details of water collection facilities to capture excess rainwater;
- v. provide details of how rain and grey water will be recycled and reused in the development.

Thereafter the development shall be implemented and retained/maintained in accordance with these details for as long as the development remains in existence.

Reason

To ensure that surface water run off is controlled to ensure the development does not increase the risk of flooding contrary to Policy EM6 Flood Risk Management in Hillingdon Local Plan: Part 1- Strategic Policies (Nov 2012), Policy 5.12 Flood Risk Management of the London Plan (July 2011) and Planning Policy Statement 25. To be handled as close to its source as possible in compliance with Policy 5.13 Sustainable Drainage of the London Plan (July 2011), and conserve water supplies in accordance with Policy 5.15 Water use and supplies of the London Plan (July 2011).

17 NONSC Contamination 1

(i) The development hereby permitted shall not commence until a scheme to deal with contamination has been submitted in accordance with the Supplementary Planning Guidance Document on Land Contamination and approved by the Local Planning Authority (LPA). The scheme shall include all of the following measures unless the LPA dispenses with any such requirement specifically and in writing:

(a) A desk-top study carried out by a competent person to characterise the site and provide information on the history of the site/surrounding area and to identify and evaluate all potential sources of contamination and impacts on land and water and all other identified receptors relevant to the site;

(b) A site investigation, including where relevant soil, soil gas, surface and groundwater sampling, together with the results of analysis and risk assessment shall be carried out by a suitably qualified and accredited consultant/contractor. The report should also clearly identify all risks, limitations and recommendations for remedial measures to make the site suitable for the proposed use; and

(c) A written method statement providing details of the remediation scheme and how the completion of the remedial works will be verified shall be agreed in writing with the LPA prior to commencement, along with details of a watching brief to address undiscovered

contamination.

(ii) If during development works contamination not addressed in the submitted remediation scheme is identified, the updated watching brief shall be submitted and an addendum to the remediation scheme shall be agreed with the LPA prior to implementation; and

(iii) All works which form part of the remediation scheme shall be completed and a comprehensive verification report shall be submitted to the Council's Environmental Protection Unit before any part of the development is occupied or brought into use unless the LPA dispenses with any such requirement specifically and in writing.

REASON: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems and the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors in accordance with policy OE11 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

18 NONSC Contamination 2

Before any part of the development is occupied, site derived soils and imported soils shall be independently tested for chemical contamination, and the results of this testing shall be submitted and approved in writing by the Local Planning Authority. All soils used for gardens and/or landscaping purposes shall be clean and free of contamination.

Note: The Environmental Protection Unit (EPU) must be consulted for their advice when using this condition.

Reason

To ensure that the occupants of the development are not subject to any risks from soil contamination in accordance with policy OE11 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

19 NONSC Secure by Design

The development shall achieve 'Secured by Design' accreditation awarded by the Hillingdon Metropolitan Police Crime Prevention Design Adviser (CPDA) on behalf of the Association of Chief Police Officers (ACPO). No unit shall be occupied until accreditation has been achieved.

REASON:

In pursuance of the Council's duty under section 17 of the Crime and Disorder Act 1998 to consider crime and disorder implications in excising its planning functions; to promote the well being of the area in pursuance of the Council's powers under section 2 of the Local Government Act 2000, to reflect the guidance contained in the Council's SPG on Community Safety By Design and to ensure the development provides a safe and secure environment in accordance with London Plan (July 2011) Policies 7.1 and 7.3.

20 NONSC Engery

Prior to commencement of development a scheme for the connection to the Old Vinyl Factory Site (outline planning reference 59872/APP/2012/1838) energy centre and/or district heating network shall be investigated. If feasible, details of the connection (including timetable) must be presented to and approved in writing by the Local Planning Authority prior to commencement of development. If a connection is considered

unfeasible, full justification must be provided to the Local Planning Authority. In any event, the site must be constructed in a manner that allows a future connection to the Old Vinyl Factory district heating network. The development must proceed in accordance with the approved scheme and details.

REASON

To ensure the development can be supplied with efficient energy in accordance with Policy 5.6 of the London Plan.

21 NONSC Ecology

No development shall take place until a scheme, based upon the recommendations of the Phase 1 Ecological Appraisal to enhance the nature conservation interest of the site has been submitted to and approved by the Local Planning Authority. The agreed works shall be undertaken prior to occupation of the development.

REASON

In order to encourage a wide diversity of wildlife on the existing semi-natural habitat of the site in accordance with Policy EC5 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and London Plan (July 2011) Policy 7.19.

22 NONSC Amenity space

The roof top amenity space shall be provided prior to the occupation of the residential units.

To ensure that an element of amenity space is provided, in accordance with Policy BE23 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

23 NONSC Non Standard Condition

Notwithstanding the details shown on Drw. No. 524 L (--) 101 Rev. P10, revised plans showing a soft play area of at least 60sqm within the cafe shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall be implemented in accordance with the revised plans and thereafter the play area shall be permanently retained.

REASON

To ensure that a satisfactory play area is provided, in accordance with Policy 3.6 of the London Plan (July 2011).

INFORMATIVES

1 152 Compulsory Informative (1)

The decision to GRANT planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

2 153 Compulsory Informative (2)

The decision to GRANT planning permission has been taken having regard to the policies and proposals in the Hillingdon Unitary Development Plan Saved Policies (September 2007) as incorporated into the Hillingdon Local Plan (2012) set out below, including Supplementary Planning Guidance, and to all relevant material considerations,

including the London Plan (July 2011) and national guidance.

NPPF1	NPPF - Delivering sustainable development
NPPF4	NPPF - Promoting sustainable transport
NPPF6	NPPF - Delivering a wide choice of high quality homes
NPPF10	NPPF - Meeting challenge of climate change flooding coastal
NPPF12	NPPF - Conserving & enhancing the historic environment
LPP 2.17	(2011) Strategic Industrial Locations
LPP 3.2	(2011) Improving health and addressing health inequalities
LPP 3.3	(2011) Increasing housing supply
LPP 3.4	(2011) Optimising housing potential
LPP 3.5	(2011) Quality and design of housing developments
LPP 3.6	(2011) Children and young people's play and informal recreation (strategies) facilities
LPP 3.8	(2011) Housing Choice
LPP 3.12	(2011) Negotiating affordable housing (in) on individual private residential and mixed-use schemes
LPP 3.13	(2011) Affordable housing thresholds
LPP 4.12	(2011) Improving opportunities for all
LPP 5.2	(2011) Minimising Carbon Dioxide Emissions
LPP 5.3	(2011) Sustainable design and construction
LPP 5.6	(2011) Decentralised Energy in Development Proposals
LPP 5.7	(2011) Renewable energy
LPP 5.9	(2011) Overheating and cooling
LPP 5.11	(2011) Green roofs and development site environs
LPP 5.12	(2011) Flood risk management
LPP 5.13	(2011) Sustainable drainage
LPP 5.15	(2011) Water use and supplies
LPP 5.21	(2011) Contaminated land
LPP 6.3	(2011) Assessing effects of development on transport capacity
LPP 6.5	(2011) Funding Crossrail and other strategically important transport infrastructure
LPP 6.9	(2011) Cycling
LPP 6.10	(2011) Walking
LPP 6.13	(2011) Parking
LPP 7.1	(2011) Building London's neighbourhoods and communities
LPP 7.2	(2011) An inclusive environment
LPP 7.3	(2011) Designing out crime
LPP 7.4	(2011) Local character
LPP 7.5	(2011) Public realm
LPP 7.6	(2011) Architecture
LPP 7.8	(2011) Heritage assets and archaeology
LPP 7.9	(2011) Heritage-led regeneration
LPP 7.13	(2011) Safety, security and resilience to emergency
LPP 7.14	(2011) Improving air quality
LPP 7.15	(2011) Reducing noise and enhancing soundscapes
LPP 7.21	(2011) Trees and woodland
BE4	New development within or on the fringes of conservation areas
BE8	Planning applications for alteration or extension of listed buildings
BE9	Listed building consent applications for alterations or extensions
BE12	Proposals for alternative use (to original historic use) of statutorily

	listed buildings
BE13	New development must harmonise with the existing street scene.
BE15	Alterations and extensions to existing buildings
BE20	Daylight and sunlight considerations.
BE21	Siting, bulk and proximity of new buildings/extensions.
BE23	Requires the provision of adequate amenity space.
BE24	Requires new development to ensure adequate levels of privacy to neighbours.
BE25	Modernisation and improvement of industrial and business areas
BE38	Retention of topographical and landscape features and provision of new planting and landscaping in development proposals.
OE1	Protection of the character and amenities of surrounding properties and the local area
OE3	Buildings or uses likely to cause noise annoyance - mitigation measures
OE5	Siting of noise-sensitive developments
OE8	Development likely to result in increased flood risk due to additional surface water run-off - requirement for attenuation measures
H4	Mix of housing units
H5	Dwellings suitable for large families
H8	Change of use from non-residential to residential
R17	Use of planning obligations to supplement the provision of recreation, leisure and community facilities
LE1	Proposals for industry, warehousing and business development
LE2	Development in designated Industrial and Business Areas
LE3	Provision of small units in designated Industrial and Business Areas
LE7	Provision of planning benefits from industry, warehousing and business development
AM2	Development proposals - assessment of traffic generation, impact on congestion and public transport availability and capacity
AM7	Consideration of traffic generated by proposed developments.
AM9	Provision of cycle routes, consideration of cyclists' needs in design of highway improvement schemes, provision of cycle parking facilities
AM14	New development and car parking standards.
AM15	Provision of reserved parking spaces for disabled persons
HDAS-LAY	Residential Layouts, Hillingdon Design & Access Statement, Supplementary Planning Document, adopted July 2006
LDF-AH	Accessible Hillingdon , Local Development Framework, Supplementary Planning Document, adopted January 2010
SPD-NO	Noise Supplementary Planning Document, adopted April 2006
SPG-AQ	Air Quality Supplementary Planning Guidance, adopted May 2002
SPG-CS	Community Safety by Design, Supplementary Planning Guidance, adopted July 2004
SPD-PO	Planning Obligations Supplementary Planning Document, adopted July 2008

3 13 Building Regulations - Demolition and Building Works

Your attention is drawn to the need to comply with the relevant provisions of the Building Regulations, the Building Acts and other related legislation. These cover such works as - the demolition of existing buildings, the erection of a new building or structure, the

extension or alteration to a building, change of use of buildings, installation of services, underpinning works, and fire safety/means of escape works. Notice of intention to demolish existing buildings must be given to the Council's Building Control Service at least 6 weeks before work starts. A completed application form together with detailed plans must be submitted for approval before any building work is commenced. For further information and advice, contact - Residents Services, Building Control, 3N/01 Civic Centre, Uxbridge (Telephone 01895 250804 / 805 / 808).

4 16 Property Rights/Rights of Light

Your attention is drawn to the fact that the planning permission does not override property rights and any ancient rights of light that may exist. This permission does not empower you to enter onto land not in your ownership without the specific consent of the owner. If you require further information or advice, you should consult a solicitor.

5 113 Asbestos Removal

Demolition and removal of any material containing asbestos must be carried out in accordance with guidance from the Health and Safety Executive and the Council's Environmental Services. For advice and information contact: - Environmental Protection Unit, 3S/02, Civic Centre, High Street, Uxbridge, UB8 1UW (Tel. 01895 277401) or the Health and Safety Executive, Rose Court, 2 Southwark Bridge Road, London, SE1 9HS (Tel. 020 7556 2100).

6 115 Control of Environmental Nuisance from Construction Work

Nuisance from demolition and construction works is subject to control under The Control of Pollution Act 1974, the Clean Air Acts and other related legislation. In particular, you should ensure that the following are complied with:-

A. Demolition and construction works which are audible at the site boundary shall only be carried out between the hours of 08.00 and 18.00 hours Monday to Friday and between the hours of 08.00 hours and 13.00 hours on Saturday. No works shall be carried out on Sundays, Bank or Public Holidays.

B. All noise generated during such works shall be controlled in compliance with British Standard Code of Practice BS 5228:2009.

C. Dust emissions shall be controlled in compliance with the Mayor of London's Best Practice Guidance 'The Control of dust and emissions from construction and demolition.

D. No bonfires that create dark smoke or nuisance to local residents.

You are advised to consult the Council's Environmental Protection Unit (www.hillingdon.gov.uk/noise Tel. 01895 250155) or to seek prior approval under Section 61 of the Control of Pollution Act if you anticipate any difficulty in carrying out construction other than within the normal working hours set out in (A) above, and by means that would minimise disturbance to adjoining premises.

7 121 Street Naming and Numbering

All proposed new street names must be notified to and approved by the Council. Building names and numbers, and proposed changes of street names must also be notified to the Council. For further information and advice, contact - The Street Naming and Numbering Officer, Planning & Community Services, 3 North Civic Centre, High Street, Uxbridge, UB8 1UW (Tel. 01895 250557).

8 125 Consent for the Display of Adverts and Illuminated Signs

This permission does not authorise the display of advertisements or signs, separate consent for which may be required under the Town and Country Planning (Control of Advertisements) Regulations 1992. [To display an advertisement without the necessary consent is an offence that can lead to prosecution]. For further information and advice, contact - Residents Services, 3N/04, Civic Centre, High Street, Uxbridge, UB8 1UW (Tel. 01895 250574).

9 147 Damage to Verge - For Council Roads:

The Council will recover from the applicant the cost of highway and footway repairs, including damage to grass verges.

Care should be taken during the building works hereby approved to ensure no damage occurs to the verge or footpaths during construction. Vehicles delivering materials to this development shall not override or cause damage to the public footway. Any damage will require to be made good to the satisfaction of the Council and at the applicant's expense.

For further information and advice contact - Highways Maintenance Operations, Central Depot - Block K, Harlington Road Depot, 128 Harlington Road, Hillingdon, Middlesex, UB3 3EU (Tel: 01895 277524).

3. CONSIDERATIONS

3.1 Site and Locality

Enterprise House is located on the northern side of Blyth Road, some 50m to the east of its junction with Trevor Road and is a Grade II listed, six storey building with projecting wings at the rear and a prominent raised roof top water tower sited at the front of the building. The 0.64 hectare application site also includes the accesses/ servicing areas on each side of the building and a surface car park at the rear of the building which has a 28m frontage onto and is accessed from Clayton Road.

Enterprise House originally formed part of the 50 acre HMV (subsequently EMI) development site, originally purchased in 1907 and used as the 'machine shop' of the former gramophone factory. The building was built around 1912 and designed on an irregular 'E' shaped plan by Arthur Blomfield and Sir E Owen Williams (the earliest known example of his work) using the Kahn system of reinforced concrete with an exposed frame. It is believed to be the only remaining building using this construction technique in England. It was extended on its eastern end between the wars (circa 1927), probably designed by Wallis Gilbert and Partners, who were working on other buildings for The Gramophone Company at that time to create an enclosed atrium at the rear, with the central and western projecting wings retaining their open aspect to the rear with only the ground floor being covered with pitched skylights. The main roof of the building is primarily flat, with a pitched skylight along the original spine of the building and a second smaller water tower at roof level to the rear of the main tower.

Enterprise House is managed by Workspace Group and provides office, studio, workshop and storage space in units that range from 193sqm to 356sqm. On part of the ground floor level is The Vinyl Factory which has recently relocated from the south east of Blyth Road.

The building's concrete frame is suffering from concrete spalling and reinforcement

corrosion and its steel framed windows are also deteriorating from corrosion, requiring many of its facades to be covered in safety netting. A significant amount of the building is unfit for health and safety grounds due to its poor condition. Of the total floorspace (15,275sqm), 3,890sqm or 25.5% is let, 4,255sqm or 28% is lettable but vacant and 7,130sqm or 46.5% is unsafe for use.

Adjoining the site to the north and east are industrial/warehouse premises, some of which share use of the side accesses and in the case of Owen Coyle (Anodising) Ltd. to the north, use Enterprise House to site some of the factory's plant, including a flue which runs up the rear elevation of the building. Beyond the industrial uses to the east is mainly terraced housing which fronts Blyth and Clayton Roads. To the west is a mainly three storey Grade II listed Jupiter House which formerly provided the headquarter building for the Gramophone Company and is now in use as an hostel. On the opposite side of Byth Road is the site of the former Old Vinyl Factory which has been granted planning permission for its redevelopment.

Blyth Road at this point forms part of a one-way system. The site has a Public Transport Accessibility Level (PTAL) of 3, on a scale of 1 to 6 where 1 denotes the lowest level of accessibility and 6 the highest.

The site is located within the Blyth Road/ Printing House Lane Industrial and Business Area and also forms part of the Botwell: Thorn EMI Conservation Area. It is also located within the Hayes/West Drayton Corridor which also forms part of the Heathrow Opportunity Area as identified in the Hillindon Local Plan (November 2012).

3.2 Proposed Scheme

The proposal is for the restoration, refurbishment, conversion and extension of Enterprise House to provide a mixed use scheme comprising approximately 4,500sqm GIA of employment space (Use classes B1 (a-c) and B2 with ancillary cafe) on the ground and first floors and 96 residential units (Use Class C3) on the upper floors. The building would be extended at roof top level.

External Works:-

The proposed external works include the removal of most of the pipes, ducts and wires to allow the grit blast cleaning of the external facades and loose/degraded concrete to be removed to reveal reinforcement. Repairs and preservation treatment to reinforcement would be applied, missing concrete replaced with proprietary repair to match original and all concrete painted with specialist waterproof protective solution. Existing brick spandrel panels and cills would be grit blasted to remove later paint covering, repaired, brickwork to have clear Silicon protection, cills painted to match concrete and damp protection and insulation added. All windows will be removed to effect concrete repairs behind, and the windows, following further investigation after their removal, will be re-used wherever possible in areas of the building where insulation is not required (such as the car parking areas), although the majority of the windows are likely to have deteriorated beyond repair. These will be replaced with steel framed double glazed windows to match the appearance of the existing windows.

As far as possible, the original entrance doors would be retained. Two new entrances are proposed to the east of the main central entrance, one as the principal access to the Business Centre and one for the cafe, allowing the main access to be used for residential access. The first existing entrance (single doorway) to the west of the main central entrance would be closed and restored to the traditional window/glazing pattern.

Rendered walls surrounding the existing double doorways would be removed and replaced with the traditional glazing pattern. Existing meter cupboards at ground floor on the southern front elevation will be removed and substation moved to rear of the building.

At roof level, the existing pitched roof to the west of the main water tower would be demolished and replaced with a flat roof extension running along the length of the roof. The frontage would comprise full height glazed doors and windows with plain panels at the rear. On the north elevation, the existing patent glazing will be replaced with new glazing to match the original, although some would be opening and one full height panel of fixed louvres concealing service outlets. The useable parts of the roof space would be concealed behind a new steel balustrade with wooden handrails, infilled with opaque glass.

The existing metal clad fire escape on the south-eastern corner would be removed to allow the the original window openings to be re-instated and a new stair constructed alongside the existing lift shaft.

Conversion works also involve the installation of external bridge links within the rear enclosed elevations to allow access to the flats on floors two to five.

Internal Works:-

Internal works include repairing and repainting ceilings and walls, with the original wooden floorboards lifted and re-used as far as possible on the first floor of the building and within corridors and residential halls.

The Otis lift will be restored to working order. This would involve removing the grill door which will be restored as a feature on the stair well and replaced with a glass door. The metal mesh enclosure will be restored at ground level and fixed in front of removable glass panels and on the upper floors the mesh will be removed and replaced with fixed glass panels with sliding panel doors on each floor.

The central passenger lift is a modern addition and will be replaced with a new lift that will access the sixth floor and the eastern goods lift will be restored with a new lobby in front linking to the external stair. The remaining lift shafts have been sealed off and the lifts removed. These shafts would be used as floor space and the original metal lift doors retained as features.

Two of the three single storey sections in the lightwells to the rear (the central and western courtyards) will be restored. The western section would be restored as originally constructed and the glazing in the central section would be replaced with a modern double glazed system. The atrium within the more modern eastern extension would be replaced by a new structure but raised to be installed at second floor level.

Internal partition walls would then be inserted along with the necessary servicing pipe and ducts.

The ground and first floors would be retained in employment use as a business centre, focusing on the music/media industries, with the ground floor in the middle of the building being occupied and anchored by The Vinyl Factory (the last remaining vinyl manufacturing company in the UK), which has recently relocated to the building from their previous site to the south east of Blyth Road. Use of the ground floor would also include a small Vinyl Heritage Centre at the western end of the Vinyl factory's Blyth Road frontage. The

reception/office area for the business centre and communal space would be provided within the double storey height atrium at the eastern end of the building, from which the Vinyl factory, a recording studio, a broadcasting studio and an ancillary cafe adjoining the space would be accessed, together with a first floor gallery connecting to first floor units via a new glass lift and stair. The cafe would also have a frontage onto Blyth Road with a narrow area of external seating along the front. The Vinyl factory would have a glass frontage, together with the two studios (which would be soundproof) and cafe, allowing activities within to be viewed from the centre. The ground floor at the western end of the building would provide 28 internal car parking spaces, 76 cycle parking and additional refuse storage facilities. The first floor would provide 15 commercial units, varying in size from 30sqm to 280sqm.

Floors two to six would comprise residential accommodation with a total of 96 units, comprising 35 x one-bedroom units, 42 x two-bedroom units and 19 x three bedroom units. The units on the sixth floor form part of a duplex split between the fifth and sixth floors. 10% of the units would be wheelchair accessible, or easily adaptable for wheelchair users.

The proposals now include a total of 91 car parking spaces (including 13 disabled spaces), 27 of which would be accommodated within the western end of the building, 3 spaces would adjoin the western end of the building, 7 spaces would adjoin the eastern end of the building and the remaining 54 spaces would be provided within the surface car park at the rear of Enterprise House. 72 of the spaces would be allocated to the residential element (representing a unit to parking space ratio of 1:0.75) and the remaining 19 spaces would serve the commercial use. Also, 134 cycle parking spaces are proposed in the form of single and double tier cycle stands within 3 areas located at the eastern and western ends of the building.

The application is supported by the following documents:-

Design and Access Statement:-

This provides the background to the statement and describes the site context. Design principles are discussed, with the report emphasising that the scheme needs to generate maximum value from the residential element to cover the cost of the building's restoration and acknowledges the challenges faced of converting an historic industrial building to residential use. The design concept is outlined for both the residential and employment elements and details the various options assessed in terms of the residential layout. The report goes on to provide a detailed assessment of the considerations involved and the design process that has led to the proposed scheme in terms of the internal layout, distribution of uses, external appearance of the building and access requirements. The treatment of the historic fabric is described, and the report then considers secure by design, refuse strategy, landscaping and external spaces, overlooking strategy and the integration of services. The report concludes that without refurbishment and repair that would be funded by the building's part residential conversion, Enterprise House will decline further and lose additional employment floorspace. The proposed scheme has been sensitively designed and will benefit the area.

Planning Statement:-

This provides an introduction to the statement and outlines the report's structure. The site and its surroundings are described, together with a detailed description of the development. Details of various pre-application consultations, both with the LPA (in

November 2012 and August 2013) and local residents and existing commercial tenants are presented and business continuity plans outlined. The planning policy context is then provided and accompanying documents are summarised. The key assessments are considered, including principle of development, designhousing, heritage and conservation, economic and regeneration benefits, transport, sustainability, and conclusions reached.

Heritage Impact Assessment:-

This provides an introduction to the assessment, outlines its structure and relevant planning policy. The site and its surrounds are described, with a particular focus upon surrounding heritage assets. The development history of the site is outlined and the assessment then provides a statement of significance which assesses the impact of the proposed development on heritage assets both within and adjacent to the application site, against relevant policy in the NPPF and guidance published by English Heritage, notably that on enabling development. The report concludes by stating that Enterprise House is deteriorating and due to this condition, its refurbishment as a factory is not a viable option and therefore enabling development is required. The HIA contends that the proposed development meets the tests of enabling development policy. Most importantly, the development would maintain the Grade II listed building and its repair and reuse would significantly enhance the character and appearance of the wider conservation area, as well as the setting of adjacent locally listed buildings.

Economic Benefits Assessment:-

This sets out the potential economic and regeneration benefits of the scheme and identifies the key quantifiable impacts during the construction and operational phases before considering the wider fiscal and qualitative effects of the development in supporting economic growth generally.

Transport Statement:-

This provides an introduction to the study and outlines the content of the statement. The transportation characteristics of the local area are described and the development proposals considered in relation to access, car parking, cycle parking and refuse/servicing. Accessibility of the site is described, together with relevant planning policy. The effect of the development is assessed in terms of trip generation and servicing arrangements and finds that peak period trip generation would be less with the part residential conversion, even if only that part of the building that is still capable of use is considered and that with servicing, use of the accesses would be reduced as compared with the industrial use of the building. The report concludes that the development is acceptable in highway terms.

Travel Plan:-

This provides an introduction and background to the Travel Plan and advises of the targets, measures and initiatives that will be employed to encourage employees, residents and visitors to use more sustainable and active ways of travelling to and from the site.

Sustainability Statement:-

This describes the proposals and the site and outlines the policy context for sustainability. The report then goes on to assess specific issues, including BREEAM standards (domestic and offices), water efficiency, CO2 reduction, ecology, waste recycling,

transport, materials and adaptability. The report concludes by stating that the proposals for Enterprise House include a number of exemplar energy and sustainability measures which demonstrate that sustainable design standards have been considered at the very beginning of the design process.

Energy Statement:-

This describes relevant policy and guidance and identifies Enterprise House as being very energy inefficient, reflecting the time of its construction. Energy efficiency improvements are considered and renewable energy technologies are assessed. An energy strategy is presented, and energy improvement targets and measures are recommended.

Phase 1 Ecological Appraisal:-

This provides an introduction to the report and describes the methodology which is based upon a desk-top study and a site survey. A preliminary protected species assessment identifies possible suitable habitat for bats and breeding birds only, but advises that no evidence of bats was found on site and the only use by birds of the building was by feral pigeons on the top floors, entering the building through broken windows. Recommendations are made for protecting, managing and enhancing the site's wildlife interest, including seeking the services of a licensed pest controller to make an inspection of the building, and to determine a suitable course of action for the removal of feral pigeon and any other pest species.

Daylight and Sunlight Assessment:-

This provides the background to the assessment and describes the site, its surroundings and the proposals. The scope of the assessment is described and the report advises that due to the modest extension on the roof, there would only be a negligible effect on the levels of natural light experienced by neighbouring residential properties. The planning policy context and daylight and sunlight methodologies using BRE guidance are then described and results are presented. The report concludes that although certain proposed residential windows and rooms would experience daylight and sunlight deficiencies in relation to the BRE guide levels, there are various mitigation factors that offset the overall significance of these deficiencies, including the incidence of these deficiencies being low and therefore localised and marginal, compared to the overall levels being good for an urban development. Also, the listed status of the building limits the opportunities that could be taken to mitigate these impacts when the BRE guidance itself advises that results need to be interpreted flexibly. The report concludes that the development would not result in any materially unacceptable daylight and sunlight effects in relation to the proposed accommodation and therefore the development is consistent with the objectives and requirements of the BRE guidance and relevant planning policy.

Noise Survey and Assessment, September 2013:-

This provides the background to the study and describes its methodology. Details of a noise survey undertaken on site from Friday 30th August to Tuesday 3rd September 2013 are presented and assessed to determine the Noise Exposure Categories of the site. Relevant standards are discussed and mitigation proposed. The report concludes with a Noise Exposure Category B on all facades, appropriate noise criteria are achievable using conventional construction.

Assessment of Residential Amenity, March 2014:-

This essentially up-dates the Noise Survey and Assessment with the results from a noise survey undertaken from Monday 17th to Thursday 20th February 2014. The Noise Exposure Category of a first floor monitoring point on the eastern end of the Blyth Road frontage of the building was found to be higher, at C, but the report concludes that the building would be suitable for residential use, providing a suitable facade design and ventilation strategy was adopted.

Air Quality Assessment:-

This provides an introduction to the study and assesses relevant legislation and planning policy as regards air quality. The report goes on to describe the assessment methodology and establishes baseline conditions. Demolition and construction phase effects and then operational phase effects are assessed and mitigation measures are recommended and residual effects described.

Air Quality Assessment - Owen Coyle (Anodising) Ltd Flue Modelling:-

This assesses the impact on local air quality of a flue from the adjoining Owen Coyle (Anodising) Ltd Factory which is to be retained and runs up the rear of Enterprise House. Relevant legislation and planning policy is described and the modelling technique described. Results are presented.

Stage (C) Report:-

This report (together with its appendices) describes the building and its structural arrangement. It considers the existing condition of the building and sets out the repair strategy. It describes proposed structural alterations, below ground drainage and the extent of the surveys and investigation works undertaken and those still required.

Waste Management Strategy:-

This describes the site and relevant policy, guidance and the documents used to formulate the strategy. It identifies the waste management approach that will be adopted for the residential and commercial uses within the building, the storage provision required and other operational considerations, together with the plan for construction waste.

Building Services Report:-

This provides an introduction to the report and describes the existing plant and equipment installed on site, including buried services. The need for new supplies are identified and the report sets out the design approach for the mechanical and electrical services for the development, using efficient plant and equipment with low energy usage for the residential and commercial elements.

Utility Report:-

This presents the findings of a utility survey which assesses the site's infrastructure and its ability to support the new loads likely to be imposed by the development.

Below Ground Drainage & Rainwater Harvesting Supplementary Information for Planning Report:-

This report assesses existing rainwater flows on site and assesses the potential, mechanisms and constraints on site to reduce these flows.

Marketing Statement:-

This outlines the recent marketing history of Enterprise House.

Financial Viability Appraisal

This assesses the financial viability of various development options for Enterprise House.

3.3 Relevant Planning History

Comment on Relevant Planning History

The associated application for listed building consent is also being presented to this committee meeting (App. No. 11623/APP/2013/3592 refers).

Outline planning permission has also been granted on the adjoining Old Vinyl Factory site for major mixed use redevelopment through a phased refurbishment and alterations to a number of existing buildings and new build including new commercial and leisure floor space and 510 residential units (App. No. 59872/APP/2012/1838 refers).

4. Planning Policies and Standards

UDP / LDF Designation and London Plan

The following UDP Policies are considered relevant to the application:-

Part 1 Policies:

PT1.E1	(2012) Managing the Supply of Employment Land
PT1.E7	(2012) Raising Skills
PT1.H1	(2012) Housing Growth
PT1.H2	(2012) Affordable Housing
PT1.HE1	(2012) Heritage
PT1.BE1	(2012) Built Environment
PT1.EM1	(2012) Climate Change Adaptation and Mitigation
PT1.EM5	(2012) Sport and Leisure
PT1.EM6	(2012) Flood Risk Management
PT1.EM8	(2012) Land, Water, Air and Noise
PT1.T1	(2012) Accessible Local Destinations
PT1.CI1	(2012) Community Infrastructure Provision
PT1.CI2	(2012) Leisure and Recreation

Part 2 Policies:

NPPF1	NPPF - Delivering sustainable development
NPPF4	NPPF - Promoting sustainable transport
NPPF6	NPPF - Delivering a wide choice of high quality homes
NPPF10	NPPF - Meeting challenge of climate change flooding coastal
NPPF12	NPPF - Conserving & enhancing the historic environment
LPP 2.17	(2011) Strategic Industrial Locations
LPP 3.2	(2011) Improving health and addressing health inequalities
LPP 3.3	(2011) Increasing housing supply
LPP 3.4	(2011) Optimising housing potential
LPP 3.5	(2011) Quality and design of housing developments
LPP 3.6	(2011) Children and young people's play and informal recreation (strategies) facilities
LPP 3.8	(2011) Housing Choice
LPP 3.12	(2011) Negotiating affordable housing (in) on individual private residential and mixed-use schemes
LPP 3.13	(2011) Affordable housing thresholds
LPP 4.12	(2011) Improving opportunities for all
LPP 5.2	(2011) Minimising Carbon Dioxide Emissions
LPP 5.3	(2011) Sustainable design and construction
LPP 5.6	(2011) Decentralised Energy in Development Proposals
LPP 5.7	(2011) Renewable energy
LPP 5.9	(2011) Overheating and cooling
LPP 5.11	(2011) Green roofs and development site environs
LPP 5.12	(2011) Flood risk management
LPP 5.13	(2011) Sustainable drainage
LPP 5.15	(2011) Water use and supplies
LPP 5.21	(2011) Contaminated land
LPP 6.3	(2011) Assessing effects of development on transport capacity
LPP 6.5	(2011) Funding Crossrail and other strategically important transport infrastructure
LPP 6.9	(2011) Cycling
LPP 6.10	(2011) Walking
LPP 6.13	(2011) Parking
LPP 7.1	(2011) Building London's neighbourhoods and communities
LPP 7.2	(2011) An inclusive environment
LPP 7.3	(2011) Designing out crime
LPP 7.4	(2011) Local character
LPP 7.5	(2011) Public realm
LPP 7.6	(2011) Architecture
LPP 7.8	(2011) Heritage assets and archaeology
LPP 7.9	(2011) Heritage-led regeneration

LPP 7.13	(2011) Safety, security and resilience to emergency
LPP 7.14	(2011) Improving air quality
LPP 7.15	(2011) Reducing noise and enhancing soundscapes
LPP 7.21	(2011) Trees and woodland
BE4	New development within or on the fringes of conservation areas
BE8	Planning applications for alteration or extension of listed buildings
BE9	Listed building consent applications for alterations or extensions
BE12	Proposals for alternative use (to original historic use) of statutorily listed buildings
BE13	New development must harmonise with the existing street scene.
BE15	Alterations and extensions to existing buildings
BE20	Daylight and sunlight considerations.
BE21	Siting, bulk and proximity of new buildings/extensions.
BE23	Requires the provision of adequate amenity space.
BE24	Requires new development to ensure adequate levels of privacy to neighbours.
BE25	Modernisation and improvement of industrial and business areas
BE38	Retention of topographical and landscape features and provision of new planting and landscaping in development proposals.
OE1	Protection of the character and amenities of surrounding properties and the local area
OE3	Buildings or uses likely to cause noise annoyance - mitigation measures
OE5	Siting of noise-sensitive developments
OE8	Development likely to result in increased flood risk due to additional surface water run-off - requirement for attenuation measures
H4	Mix of housing units
H5	Dwellings suitable for large families
H8	Change of use from non-residential to residential
R17	Use of planning obligations to supplement the provision of recreation, leisure and community facilities
LE1	Proposals for industry, warehousing and business development
LE2	Development in designated Industrial and Business Areas
LE3	Provision of small units in designated Industrial and Business Areas
LE7	Provision of planning benefits from industry, warehousing and business development
AM2	Development proposals - assessment of traffic generation, impact on congestion and public transport availability and capacity
AM7	Consideration of traffic generated by proposed developments.
AM9	Provision of cycle routes, consideration of cyclists' needs in design of highway improvement schemes, provision of cycle parking facilities
AM14	New development and car parking standards.
AM15	Provision of reserved parking spaces for disabled persons
HDAS-LAY	Residential Layouts, Hillingdon Design & Access Statement, Supplementary

	Planning Document, adopted July 2006
LDF-AH	Accessible Hillingdon , Local Development Framework, Supplementary Planning Document, adopted January 2010
SPD-NO	Noise Supplementary Planning Document, adopted April 2006
SPG-AQ	Air Quality Supplementary Planning Guidance, adopted May 2002
SPG-CS	Community Safety by Design, Supplementary Planning Guidance, adopted July 2004
SPD-PO	Planning Obligations Supplementary Planning Document, adopted July 2008

5. Advertisement and Site Notice

5.1 Advertisement Expiry Date:- **4th March 2014**

5.2 Site Notice Expiry Date:- Not applicable

6. Consultations

External Consultees

74 neighbouring properties have been consulted, 2 notices have been displayed on site and the application advertised in local press on 12/2/14. 1 response has been received, making the following points:-

- (i) The current proposals, especially to the west end, would have a major impact on the operational ability of the adjoining factory building. The proposals introduce additional parking around the perimeter of Enterprise House, particularly at its west end, which is the main industrial access, and would now include the main entrance and exit to the ground floor parking, which are immediately adjacent to the main entrance of Owen Coyle (Anodising) Ltd. from which all goods and finished products are accessed. Photographs are attached showing 40ft articulated lorries delivering and receiving finished goods with the ancillary manouevring that is required. Current proposals would totally prevent these activities taking place and have severe financial implications. Rights of access and manouevring off Blyth roiad to main factory entrance have been established for over 40 years,
- (ii) There is a main exhaust flue from Owen Coyle (Anodising) Ltd. which is attached to Enterprise and there is no indication in the proposals for this to be accommodated which has been established for over 40 years,
- (iii) The introduction of 96 residential apartments adjacent to a working factory complex (3.5m) with fire escape staircases leading directly onto the common passageway between the two buildings together with commercial traffic movements and manouevring all have major environmental and health and safety issues,
- (iv) Lack of adequate amenity space will exacerbate health and safety issues,
- (v) Concentration of residential apartments and associated parking will put enormous pressure on vehicular movements around existing one-way system on Blyth Road, which is already overcrowded.

ENGLISH HERITAGE:

Thank you for your letter of 10 February 2013 seeking English Heritage views on an application at Enterprise House, Hillingdon. I have assessed the proposals and visited the site, in the company of your own specialist conservation staff.

English Heritage Advice

Enterprise House was originally a gramophone factory for His Master's Voice (HMV), constructed in 1912 to the designs of the noted architect Owen E Williams. Williams was a pioneer in the field of concrete architecture who went on to design a number of notable buildings, but Enterprise House is his first known work. As noted by the statutory list description, the building is unusual for the era in that its construction is expressed externally, with slender concrete pilasters flanking large metal-framed windows with brick infills below cill level. The building has undergone little alteration internally other than removal of all manufacturing equipment, and retains a remarkable original lift car finished in mahogany and cut glass.

The building is now only partially occupied, and in a semi-derelict condition of slow decay with evident issues of spalling concrete. It has recently been included on English Heritage's Heritage at Risk register, and it is clear that urgent action is needed to prevent further decay in the structure.

In addition to its evident aesthetic quality, Enterprise House has much historic value, not just in its pioneering form of construction, but also as a tangible reminder of a major industrial concern that dominated the locality for generations and provided employment to thousands of local people. Its place in the history of the borough gives the building some communal value.

The proposed development envisages the conversion of the upper four levels of the building into residential use, with a single storey rooftop extension above to house further residential accommodation. Commercial uses would be provided for at ground and first floor, and a portion of ground floor is also given over to car parking. We understand that the commercial uses will include retention of an existing vinyl record manufacturing business that currently operates from the site, in addition to other music-related businesses. The impressive internal is to be restored and made available to the general public.

The proposed commercial units at ground and first floor levels are of a relatively large size, which provides the benefit of negating the need for extensive subdivision in the impressive internal spaces of the building at lower floors. Part of the former factory floor at ground floor level will be turned over to car parking, which may be acceptable in principle, but will require sensitive handling in terms of surface finishes, maintenance and servicing. It would be useful if further information could be supplied demonstrating that the car parking area will not require unsightly mechanical ventilation equipment, for example.

At the floors on which residential conversion is proposed, it is planned to construct a series of external bridges to provide independent access to the flats. This unusual design approach removes the need to create extensive internal corridors and thus allows the majority of the proposed apartments to have a dual aspect, ensuring that the full width of the building is expressed in the residential conversion. Provided the external bridges are not enclosed and of an appropriately industrial character and appearance we would support this proposal, but should it prove necessary for the bridges to be enclosed internal spaces, we feel there may be a resultant negative impact on the aesthetic value of the building. We therefore would require some confirmation that open external bridges will be acceptable in terms of building regulations.

In terms of the level of internal subdivision, we note that many of the proposed apartments are one or two bedroom units. It may be possible to reduce the visual impact of subdivision within the flats through the use of bathroom and kitchen "pods" rather than straightforward stud partition walls. However, there would be less need for subdivision if larger units were created, and we would be interested in reviewing financial information justifying the creation of smaller units.

We understand there are a number of potential wider planning policy conflicts associated with the proposal such as lack of amenity space and the provision of residential accommodation in land zoned for employment use. However, in regards to the impact of the proposals on the significance of the listed building, provided that the external open bridges are feasible and the car parking is

handled sensitively, we are of the view that the proposals are potentially beneficial.

There is some minor harm in the subdivision of the upper floors for residential use, but this must be weighed against the benefits associated with the restoration of the building and the consequent likely removal from the Heritage at Risk register.

Paragraph 131 of the NPPF states that "local planning authorities should take account of the desirability of preserving and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation." If the additional information I have requested in this letter can be supplied by the applicant, we suggest that aim could be achieved by these plans.

Recommendation

These proposals potentially have significant heritage benefits which should be weighed against conflicts with wider planning policy. However, we encourage your authority to request the additional information detailed above, to ensure that the proposed development is the optimum viable use of the heritage asset.

It may be advantageous to arrange a meeting involving myself and our heritage at risk team, alongside yourselves, your in-house specialist conservation team, and the applicant, to discuss the proposals further. This could enable us to fully understand the conflicts with wider planning policy. Please do contact me at your earliest convenience, to arrange a suitable time for us to all meet at your offices.

HAYES CONSERVATION AREA ADVISORY PANEL:

We welcome the proposal to bring this building back into full and beneficial use, and applaud the proposals to make the most of its surviving historic features. We do have some concerns about the access bridges on the uppermost floors as their effect on the northern facade is not apparent from the elevation drawings supplied. We also hope that the occupants who have to use them will have a good head for heights!

These comments also apply to application 11623/APP/2013/3592.

Internal Consultees

URBAN DESIGN/CONSERVATION OFFICER:

Background: Enterprise House is a grade II listed building located in the Botwell Thorn EMI Conservation Area. The building sits within an industrial area, and lies adjacent to mainly modern, shed like buildings to the north, east and west. To the south is The Old Vinyl Factory, a major redevelopment site where works on the flag ship building are about to commence.

Enterprise House dates from 1912 and was constructed for The Gramophone Company. It was originally known as The Machine Factory and was where gramophones were designed and built. It was designed by Arthur Blomfield and A Henderson, and built by E Owen Jones, the most significant engineer turned architect of the twentieth century. It is also the earliest known example of his work. The structure is of reinforced concrete, with its construction clearly expressed both externally and internally. The building utilises the 'Kahn Daylight System' of construction manufactured by the American firm of TRUSCON - the Trussed Concrete Steel Company. Most buildings of this use and period are more traditional in appearance and usually clad in brick. It is believed that Enterprise House is the only remaining building of its type in England. The building comprises the original 1912 structure and an addition of similar design to the east. This dates from circa 1927 and was probably designed by Wallis Gilbert and Partners, who were working on other buildings for The Gramophone Company at that time.

Enterprise House has a striking and unusual appearance. It has largely symmetrical principal elevations, which include a significant amount of original, steel framed windows. These give the building quite a light weight appearance, are set in a grid like pattern and have a distinctive pattern of openers. The main building has north lights at roof level, set over the original design studios and full height rear wings, above glazed atria at ground floor. The later addition is slightly different in design, with a fully enclosed light well and larger areas of glazing to its rear elevation. This part of the building has a flat roof.

The perimeter of the roof is enclosed with a simple metal railing, there is no parapet and the most distinctive features are the original high level concrete water tanks. The larger of these is supported above the building on reinforced arches with simple classical detailing. The water tanks and distinctive roof line of the building are significant local landmarks, which are highly visible from the surrounding streets and the wider area.

Internally, the rugged construction of the building is clearly expressed, with a grid of simple concrete faceted columns and high level spandrels. Whilst some of the spaces have been subdivided over time, most of the floors retain large, open and well lit spaces with impressively high ceilings. It is not clear how many of the original lifts remain, but one original timber panelled lined lift remains in the front section of the building. A number of metal vaults, originally designed to protect/store tools during fire, remain on the upper floors.

The building has been neglected in recent years, there are sections of damaged render and exposed areas of reinforcement on many of the elevations and numerous unsightly telecommunication antennas at roof level. The building was recently made wind and weather tight, and was included in the EH BAR in 2013.

Comments: The proposals have been subject to extensive discussion with the applicants and also English Heritage, who have been supportive of the works. The current drawings address many of the concerns raised by the Conservation Team, and whilst not ideal in listed building terms, as the level of subdivision is considered to be damaging to the building, overall the proposals would bring a vacant listed building on the BAR back into a viable new use. In these circumstances, and on balance, no objection is raised by the Conservation and Design Team to the scheme as now proposed.

The following items need to be covered by suitable conditions to be discharged prior to the start of the relevant part of the work unless specified:

- Recording of the building to EH level 1 (prior to the start of any works on site) - discs/ copies of the document to be lodged with the Council, local library and Uxbridge Local History Library
- A schedule and full specification for building repairs/renovation/alteration to be agreed prior to the start of relevant works on site
- A methodology for cleaning works and samples areas of brickwork and concrete cleaning to be agreed on site
- External paint colours for the elevations, to include brickwork, concrete and windows/ downpipes to be agreed
- A Conservation/Management Plan to be agreed for the future maintenance of the building - this might be better covered in a S106 Agreement
- Details of the works to windows, including details of the repair and relocation of existing windows; construction and glazing of new windows (including casements)
- Design details, finishes and materials of all external and internal doors, including roller shutters/boxes and also canopies over
- Details of materials, appearance and location of new downpipes, hopper heads and external pipe work
- Details of the design, materials and construction of new and refurbished glazed roofs and the new

atrium roof; existing cowls and vents to be reinstated

- Schedule and details of repairs to original lifts, lift enclosures and related machinery- works to be completed before the building is occupied
- Details of design and construction of 6th floor altered and new roof top additions, including glazing, cladding/roofing materials and brise soleil
- Details of the repair of existing and the design and construction of new hand rails, balustrades, screens and balcony/roof top enclosures
- Details of roof top plant and machinery
- Location, type and finish of flues, louvers and vents, including low level car park vents
- Details of the design, construction, materials, finishes and method of installation of external walk ways
- Details of the design, construction, materials and finishes of the new eastern stair case enclosure
- Full details of works to existing eastern low level additions
- Fire and sound proofing works
- Details of construction and installation of internal partitions and glazed screens- these are to be reversible
- Existing antenna and satellite dishes at roof level and fixed to the water tanks to be removed prior to the occupation of the building
- Details of external works to steps, railings and forecourt area to be agreed
- Design and content of interpretation boards to be agreed
- Details of external signage to be agreed.

HIGHWAY ENGINEER:

Having considered the servicing and parking requirements for the B8 uses, the applicant has agreed to remove it in its entirety from Enterprise House. The proposals therefore seek consent for residential use, and employment uses in use classes B1 (a-c) and B2 (Vinyl Factory).

The proposals include a total of 91 car parking spaces (including 13 disabled spaces), of which, 72 spaces will be allocated to the residential use and the remaining 19 spaces will be allocated to the commercial use.

134 cycle parking spaces are proposed in the form of single and double tier cycle stands over 3 areas.

Parking for electric vehicles should be provided in the order of 20% passive and 10% active for the commercial use and 20% active and 20% passive for the residential use. As per the Council's standards, 5 parking spaces should be provided for two-wheelers in addition to those for cars and bicycles.

A car club will be provided, for which 2 parking spaces will be reserved. Details of the operation and management of the car club should be secured within the S106 agreement.

It is proposed to utilise the commercial car parking spaces for visitors of the future residents during evenings and weekends and a permit free agreement to exclude the future residents of the development from eligibility for the parking permits for the adjacent CPZ. These two elements should be covered within the S106 Agreement.

A parking allocation and management plan should be secured through a planning condition, including but not limited to, allocating residential car parking to the 2 and 3 bed flats, visitor parking and electric charging spaces.

Consideration has been given to the servicing of the Owen Coyle building. Spaces 29, 30 and 31 on the previously proposed car parking layout were considered to have the potential to conflict with

the servicing of the Owen Coyle building. These spaces have been relocated in the revised car parking layout.

The location of the refuse store is not within the maximum trundle distance of 10m for collection from the carriage in Blyth Road. It is proposed that the on-site management staff will bring the bins to the back of the Blyth Road footway on collection days. Whilst this arrangement is not entirely reliable for the life of the development, there is space available on private land adjacent to the back of the footway in Blyth Road to temporarily store bins for collection without obstructing the footway. This arrangement will alleviate the need for refuse vehicles to access the site; instead refuse will be collected from Blyth Road. The proposed refuse management arrangement should be secured by way of a condition.

Table 6.1 of the Transport Statement is somewhat misleading. The existing trip generation should be based on the existing occupation of the site. Instead, the trip generation has been estimated on the total floor space available and also on the total floor space excluding space that is unsafe for use, both of which are permitted scenarios and cannot be considered as existing scenarios. Moreover, if the total available commercial floor space had demand for it, the site would unlikely to have come forward for redevelopment in its current form. Notwithstanding this, the proposed development is not considered to have a significant and/or unacceptable vehicle traffic impact on the surrounding road network.

Subject to the issues discussed in the comments above being covered through suitable planning conditions/S106 agreement, no objection is raised on the highways aspect of the application.

TRANSPORT AND AVIATION:

No objections are raised on the Travel Plan.

TREE/LANDSCAPE OFFICER:

Landscape/ Context:

The site is a Grade II listed building situated on the north side of Blyth Road. It is a part of the original Thorn EMI industrial complex of buildings which lie within the designated Botwell / Thorn EMI Conservation Area.

There is a narrow strip of forecourt between the building and the Blyth Road footway, which is defined by metal railings. The only other open space, associated with this industrial building is an access /service road which links to the rear of the building and serves neighbouring industrial sites. There are also rooftop walkways / access points.

There is no associated open space for amenity and no areas of soft landscape on, or close to, the site.

Proposal:

The proposal is to erect extensions at roof level, the erection of external bridge links on the rear elevation and internal works associated with the change of use of part of enterprise house to create 96 residential units (class C3) and associated car parking, retention of approximately 4,500 sqm (GIA) of employment use (Class B1 and B8) at ground and first floor levels and cafe.

Landscape Considerations:

Saved policy BE38 seeks the retention and utilisation of topographical and landscape features of merit and the provision of new planting and landscaping wherever it is appropriate.

- As noted there are no existing trees or other landscape features associated with this building.
- The Design & Access Statement confirms that there is very limited space or opportunity for landscape enhancement on the site.

- At 5.2 (D&AS) reference is made to the Blyth Road site frontage, where the railings are to be removed and 429sqm of space will be available for landscape enhancement to include seating and planting in containers.
- Landscape and external spaces are also described in section 11.00, where a 126sqm communal rooftop garden is proposed.
- A Phase 1 Ecological Appraisal (section 5.0) recommends the use of plants of known value to wildlife for inclusion in the planting scheme, utilising 'a high percentage of native species'. This recommendation should be treated with caution as several of the plants listed will be unsuitable for the limited space available and of little amenity value. Plants will require careful selection to ensure that they satisfy both visual amenity and biodiversity objectives.
- If the application is recommended for approval, landscape conditions should be imposed to ensure that the proposals preserve and enhance the character and appearance of the surrounding built environment and contribute to wildlife.

Recommendations:

No objection subject to the above observations and COM9 (parts 1, 2, 4, 5 and 6).

EPU (Noise):

Having read through the plans, please can the following conditions be added:

1. The commercial premises shall not be used for deliveries and collections, including waste collections other than between the hours of 08:00 and 20:00, Mondays to Fridays, 08:00 to 20:00 Saturdays and not at all on Sundays and Bank and Public Holidays.

REASON

To safeguard the amenity of surrounding areas, in accordance with Policies OE1, OE3 and S6 of the Hillingdon Unitary Development Plan Saved Policies (September 2007) and Policy 4A.20 of the London Plan (February 2008).

2. The development shall not begin until a sound insulation scheme for the control of noise transmission from commercial to residential and from road/rail noise to residential premises has been submitted to and approved in writing by the Local Planning Authority. The scheme shall be fully implemented before the development is occupied/use commences and thereafter shall be retained and maintained in good working order for so long as the building remains in use.

REASON

To safeguard the amenity of the occupants of surrounding properties in accordance with Policy OE1 of the Hillingdon Unitary Development Plan Saved Policies (September 2007) and Policy 4A.20 of the London Plan (February 2008).

Please add the standard construction informative on:

Nuisance from demolition and construction works is subject to control under the Control of Pollution Act 1974, the Clean Air acts and other related legislation. In particular, you should ensure that the following are complied with:

- a) demolition and construction works should only be carried out between the hours of 08:00 and 18:00 Monday to Friday and between the hours of 08:00 and 13:00 on Saturday. No works shall be carried out on Sundays or Bank Holidays.
- b) All noise generated during such works should be controlled in compliance with British standard BS 5228-1:2009
- c) The elimination of the release of dust or odours that could create a public health nuisance
- d) No bonfires that create dark smoke or nuisance to local residents

You are advised to contact the Council's Environmental Protection Unit, 3S/02 Civic Centre, High Street, Uxbridge, UB8 1UW if you anticipate any difficulty in carrying out construction other than within the normal working hours set out above and by means that would minimise disturbance to adjoining premises.

EPU (Air Quality and Contamination):

Comments regarding air quality and land contamination are provided below. No information was submitted with regard to land contamination to date.

The following documents were submitted with regard to air quality:

- Enterprise House, Hayes Air Quality Assessment by Waterman for Enterprise House LLP, dated February 2014

Enterprise House, Hayes Air Quality Assessment - Owen Coyle (Adonising) Ltd Flue Modelling by Waterman for Enterprise House LLP, dated February 2014

Air Quality

The proposed development is within the declared AQMA and in an area currently exceeding the European Union limit value for annual mean nitrogen dioxide (40.0 mg/m³). Modelling carried out or Hillingdon by CERC in 2011 indicates the whole site is above the EU limit value, with the eastern most part of the site at slightly over 49 mg/m³ and the western boundary slightly over 40 mg/m³. This is assumed to be a slight overestimate. NO_x tube monitoring to the east of Blyth Road undertaken by Hillingdon indicates NO₂ levels at 43 mg/m³ in 2013. There was a monitored exceedance in 2012 (less than a full years monitoring) as well, which was closer to the worst modelled data.

The June 2014 Air Quality Assessment was submitted as the February 2014 assessment assumed the Owen Coyle flue (attached to Enterprise House), was to be relocated as part of the development. However, it seems this will no longer be the case. The approach of the second assessment has been somewhat conservative with regard to the emissions from the Owen Coyle flue, however it should be noted the DEFRA background for NO₂ at the site may be an underestimate for current year 2012 in all assessments.

Both assessments indicate some exceedances at the modelled second floor residential receptors on site only. The assessment also indicates an existing exceedance of the EU limit value for NO₂ at existing ground floor receptors considered on Blyth Road. To one of these, the proposed development is indicated to add 0.1 mg/m³ which is considered to be 'negligible'. A similar increase is noted at a Dawley Road receptor with the development. This may be an underestimate as the impact from traffic for this development considered alongside other developments in the area may be greater than modelled. However, the report does indicate the proposed development 'would generate less demand for travel than the currently proposed use of the site', which is consistent with the trip generation information in the Transport Statement.

As the development is in and will cause increases in an area already suffering poor air quality the following are requested:

Section 106

Section 106 obligation for £25,000 should be sought for contribution to the air quality monitoring network in the area.

Neither air quality assessments made reference to the transport emissions benchmark being met for the development to demonstrate the development is at least 'air quality neutral'. If they can demonstrate this, a reduction in the s106 request for air quality could be made. The submission of

the Travel Plan is noted.

The Owen Coyle flue emissions are indicated to impact the most on the receptors on the fifth and sixth floors. No exceedances are indicated at these receptors. It is possible there may be still be exceedances in this locality as the 2012 background used may be an underestimated and some of the modelled NO₂ levels are high. The February 2014 Air Quality Assessment indicates the gas-fired CHP proposed as part of the development has been modelled, however the information is presented in a way that the NO₂ contribution from the CHP at each receptor location is not plain to see.

The assessment also makes a reference to the gas-fired CHP and boilers at the Old Vinyl Factory site, to the south of Blyth Road, however the modelled data provided seems to indicate it may have not been modelled as part of the assessment, in a manner consistent with the Vinyl Factory assessment as the expected increases at higher floor levels and NO₂ exceedances are not apparent. Again this may be due to the underestimate in the background NO₂ value used. As the predominant wind direction is indicated to be south westerly's, there is likely to be air quality impacts from the CHP at the Old Vinyl Factory. It is also unclear if all air quality impacts from the site have been considered, as operations on site are indicated to continue. Could they please clarify all emissions to air from the development, including for pollutants not considered under LAQM?

As monitoring indicates exceedances on Blyth Road and exceedances are likely at a number of receptor locations for the proposed development, the ingress of polluted air condition is recommended for any permission that may be given.

Condition 1: Ingress of Polluted Air

Before the development is commenced a scheme designed to minimise the ingress of polluted air shall be submitted for approval in writing by the Local planning Authority. The design must take into account climate change pollutants. Any suitable ventilation systems will need to address the following:

- Take air from a clean location or treat the air and remove pollutants;
- Be designed to minimise energy usage;
- Be sufficient to prevent summer overheating;
- Have robust arrangements for maintenance.

Thereafter and prior to occupation, the scheme shall be completed in strict accordance with the approved details and thereafter maintained for the life of the development.

REASON: In order to safeguard the amenities of the area, in accordance with Policy OE1 of the Hillingdon Unitary Development Plan Saved Policies (September 2007) and London Plan (July 2011) Policy 7.14.

Information provided about the gas-fired CHP in the air quality assessment suggests the flue is shorter than the building at 25.5 metres above ground level with NO_x emission rates of 0.00719 g/s (surprisingly low). The location of the flue is not entirely clear based on drawings. No specific details about the CHP engine has been provided, other than the revised Energy Statement indicating the use of 100 kWe CHP system, which is needed to cover the hot water demand. It goes on to say the electricity generated by the CHP would significantly exceed the electrical base load of the building so that a large proportion of the electricity would have to be sold to the grid. However, it indicates a CHP is not proposed for the site. The Sustainability Statement indicates high efficiency NO_x gas boilers emitting less than 40 mg/kWh will be used for all dwellings. It is not clear, if it will also be provided for the other uses on site. It does not look like consideration has been given to connecting to the energy centre at the Old Vinyl Factory site. The following condition is recommended to ensure relevant information regarding the energy sources at the site is

provided, including details of if the building emissions benchmark has been met and the development is at least 'air quality neutral'.

Air Quality Condition 2 - Details of Energy Provision

Before the development is commenced details of any plant, machinery and fuel burnt, as part of the energy provision and the location of the flue for the development shall be submitted to the LPA for approval. This shall include pollutant emission rates at the flues with or without mitigation technologies. The use of ultra low NOx emission gas CHPs and boilers is recommended. The development should as a minimum be 'air quality neutral' and demonstrably below the building emissions benchmark.

Reason: To safeguard the amenity of neighbouring properties in accordance with policy OE1 of the Hillingdon Unitary Development Plan.

Notes: This condition relates to the operational phase of residential and commercial development and is intended for the protection of future residents in a designated AQMA and Smoke Control Area. Advice on the assessment of CHPs is available from EPUK at: http://www.iaqm.co.uk/text/guidance/epuk/chp_guidance.pdf. An area up to a distance of 10 times the appropriate stack height needs to be assessed. Guidance on air quality neutral is available at: <https://www.london.gov.uk/priorities/planning/consultations/draft-sustainable-design-and-construction>. They should contact Planning Specialists if they have any queries.

Land Contamination

No information has been submitted with regard to land contamination. Policy EM8 is referred to in the Planning Statement, although there is no mention of land contamination. The application form indicates the proposed development would be particularly vulnerable to the presence of contamination. I would also add that contamination is suspected for all or part of the site given the onsite use and the industrial history of the site and surrounding sites. The standard contaminated land condition is advised. Some ground investigation is recommended especially with regard to ground gas and vapour issues.

Contaminated Land Condition

(i) The development hereby permitted shall not commence until a scheme to deal with contamination has been submitted in accordance with the Supplementary Planning Guidance Document on Land Contamination and approved by the Local Planning Authority (LPA). The scheme shall include all of the following measures unless the LPA dispenses with any such requirement specifically and in writing:

(a) A desk-top study carried out by a competent person to characterise the site and provide information on the history of the site/surrounding area and to identify and evaluate all potential sources of contamination and impacts on land and water and all other identified receptors relevant to the site;

(b) A site investigation, including where relevant soil, soil gas, surface and groundwater sampling, together with the results of analysis and risk assessment shall be carried out by a suitably qualified and accredited consultant/contractor. The report should also clearly identify all risks, limitations and recommendations for remedial measures to make the site suitable for the proposed use; and

(c) A written method statement providing details of the remediation scheme and how the completion of the remedial works will be verified shall be agreed in writing with the LPA prior to commencement, along with details of a watching brief to address undiscovered contamination.

(ii) If during development works contamination not addressed in the submitted remediation scheme is identified, the updated watching brief shall be submitted and an addendum to the remediation scheme shall be agreed with the LPA prior to implementation; and

(iii) All works which form part of the remediation scheme shall be completed and a comprehensive verification report shall be submitted to the Council's Environmental Protection Unit before any part of the development is occupied or brought into use unless the LPA dispenses with any such requirement specifically and in writing.

REASON: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems and the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors in accordance with policy OE11 of the Hillingdon Unitary Development Plan Saved Policies (September 2007).

The following condition needs to be included in any permission given to ensure the soils are suitable for use, if any areas of soft landscaping or planting are created as part of the development.

Condition to minimise risk of contamination from garden and landscaped areas

Before any part of the development is occupied, site derived soils and imported soils shall be independently tested for chemical contamination, and the results of this testing shall be submitted and approved in writing by the Local Planning Authority. All soils used for gardens and/or landscaping purposes shall be clean and free of contamination.

Note: The Environmental Protection Unit (EPU) must be consulted for their advice when using this condition.

Reason

To ensure that the occupants of the development are not subject to any risks from soil contamination in accordance with policy OE11 of the Hillingdon Unitary Development Plan Saved Policies (September 2007).

ACCESS OFFICER:

The latest revised plan are acceptable from an accessibility standpoint.

SUSTAINABILITY OFFICER:

The Enterprise House development does not meet an equivalent building regulation standard development in terms of energy, let alone the 40% reduction target of the London Plan.

I accept this is a significant refurbishment proposal on building with heritage value. As a consequence, achieving the energy reduction through improvements to the fabric of the building are complicated at best and impossible in some instances.

Nonetheless, an opportunity to link into the Old Vinyl Factory district heating network has considerable potential. I had asked this to be considered at part of the planning stages, but little progress was made. Therefore the plans as presented set out a development that would have sizeable energy loads (compared to a building regulations compliant development) and this would have implications for energy bills of residents.

The potential link to the Old Vinyl Factory site needs to be properly analysed and considered to ensure energy reductions measures for Enterprise House have been exhausted.

The following condition is therefore essential:

Condition:

Prior to commencement of development a scheme for the connection to the Old Vinyl Factory Site (outline planning reference 59872/APP/2012/1838) energy centre and/or district heating network shall be investigated. If feasible, details of the connection (including timetable) must be presented to and approved in writing by the Local Planning Authority prior to commencement of development. If a connection is considered unfeasible, full justification must be provided to the Local Planning Authority. In any event, the site must be constructed in a manner that allows a future connection to the Old Vinyl Factory district heating network. The development must proceed in accordance with the approved scheme and details.

Reason:

To ensure the development can be supplied with efficient energy in accordance with Policy 5.6 of the London Plan.

WATER AND FLOOD MANAGEMENT OFFICER:

Following the submission of the additional Below Ground information by the Furness Partnership, which highlights the issues facing the site in reducing the surface water run off from the site. However as the site is reducing surface water run off by 15% and commits to further work to explore opportunities to control water further at the detailed design stage. I am therefore happy to recommend the Sustainable Drainage condition:

Prior to commencement, a scheme for the provision of sustainable water management shall be submitted to, and approved in writing by the Local Planning Authority. The scheme shall clearly demonstrate how it follows the strategy set out in the Below Ground Information by the Furness Partnership dated Jan 2014 and incorporates sustainable urban drainage in accordance with the hierarchy set out in Policy 5.15 of the London Plan and will:

- i. provide information on all Suds features including the method employed to delay and control the surface water discharged from the site and:
 - a. calculations showing storm period and intensity and volume of storage required to control surface water and size of features to control that volume.
 - b. any overland flooding should be shown, with flow paths depths and velocities identified as well as any hazards, (safe access and egress must be demonstrated).
 - c. measures taken to prevent pollution of the receiving groundwater and/or surface waters;
 - d. how they or temporary measures will be implemented to ensure no increase in flood risk from commencement of construction.
- ii. provide a management and maintenance plan for the lifetime of the development of arrangements to secure the operation of the scheme throughout its lifetime. Including appropriate details of Inspection regimes, appropriate performance specification, remediation and timescales for the resolving of issues.
- iii. provide details of the body legally responsible for the implementation of the management and maintenance plan.

The scheme shall also demonstrate the use of methods to minimise the use of potable water through water collection, reuse and recycling and will:

- iii. incorporate water saving measures and equipment.
- iv. provide details of water collection facilities to capture excess rainwater;
- v. provide details of how rain and grey water will be recycled and reused in the development.

Thereafter the development shall be implemented and retained/maintained in accordance with these details for as long as the development remains in existence.

Reason

To ensure that surface water run off is controlled to ensure the development does not increase the

risk of flooding contrary to Policy EM6 Flood Risk Management in Hillingdon Local Plan: Part 1- Strategic Policies (Nov 2012) Policy 5.12 Flood Risk Management of the London Plan (July 2011) and Planning Policy Statement 25. To be handled as close to its source as possible in compliance with Policy 5.13 Sustainable Drainage of the London Plan (July 2011), and conserve water supplies in accordance with Policy 5.15 Water use and supplies of the London Plan (July 2011).

S106 OFFICER:

I have reviewed the proposed development:

Erection of extensions at roof level, erection of external bridge links on the rear elevation and internal works associated with the change of use of part of enterprise house to create 96 residential units (class C3) and associated car parking, retention of approximately 4,500 sqm (GIA) of employment use (Class B1 and B8) at ground and first floor levels and cafe.

35 x 1 bed
42 x 2 bed
19 x 3 bed
100% market housing

The following S106 Planning Obligations need to be secured:

1. Highways: Highways works together with a S278/S38 Agreement will need to be secured subject to comments from the Highways officer.
2. Affordable Housing: A review mechanism allowing the council to review the value of the scheme in the future to secure affordable housing.
3. Construction Training
 - Training Costs: £2500 per £1m build cost
 - Coordinator costs: $96/160 \times £71,675 + 4500/7500 \times £71,675 = £86,010$
4. Air Quality monitoring fee: £25,000
5. Project Management & Monitoring Fee: 5% of total cash contributions.

7. MAIN PLANNING ISSUES

7.01 The principle of the development

The National Planning Policy Framework, March 2012 (NPPF) at paragraph 2 states that "Planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise".

The application site is identified as a Strategic Industrial Location (SIL): Preferred Industrial Location (PIL) within the London Plan (July 2011), as a Locally Significant Industrial Site (LSIS) by the Hillingdon Local Plan: Part One - Strategic Policies (November 2012) and an Industrial and Business Area (IBA) within the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

Policy 2.17 of the London Plan (July 2011) states that SILs are London's main reservoirs of industrial and related capacity and that:-

"...Development proposals in SILs should be refused unless:

- a) they fall within the broad industrial type activities outlined in paragraph 2.79; or
- b) they are part of a strategically co-ordinated process of SIL consolidation through an opportunity area planning framework or borough development plan document; or
- c) the proposal is for employment workspace to meet identified needs for small and medium sized enterprises (SMEs) or new emerging industrial sectors; or

d) the proposal is for small scale 'walk to' services for industrial occupiers such as workplace creches or cafes."

However, Policy 4.4 of the London Plan also advises that LDFs should protect locally significant industrial sites where justified by evidence of demand but also take into account the need to transfer industrial land to other uses, having regard to strategic and local criteria and strategic monitoring benchmarks (Hillingdon has been identified as an intermediate area, where the limited transfer of land would be appropriate).

Policy LE2 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) essentially re-iterates London Plan policy by stating that IBAs are designated for business, industrial and warehouse purposes (Use Classes B1 - B8) and for sui generis uses appropriate to an industrial area. The policy goes on to advise that alternative uses will not be permitted unless (i) there is no realistic prospect of the land being used for industrial or warehousing purposes in the future, (ii) the alternative use does not conflict with other policies and objectives of the plan and the proposal better meets the plan's objectives, particularly in relation to affordable housing and economic regeneration.

The NPPF does advise at paragraph 22 of the need for LPAs to review employment use designations:-

"Planning policies should avoid the long term protection of sites allocated for employment use where there is no reasonable prospect of a site being used for that purpose. Land allocations should be regularly reviewed. Where there is no reasonable prospect of a site being used for the allocated employment use, applications for alternative uses of land or buildings should be treated on their merits having regard to market signals and the relative need for different land uses to support sustainable local communities."

As part of the review process, the Council's Hillingdon Local Plan: Part One - Strategic Policies (November 2012) did advise at paragraph 5.10 that:-

"There is more employment land than currently needed, and any release of surplus industrial land will be carefully managed to support Hillingdon's employment generation whilst creating opportunities for regeneration and release to other uses including much needed housing. The London Plan requires Hillingdon to adopt a 'Limited Transfer' approach to the transfer of industrial sites to other uses."

Paragraph 5.12 of this document goes on to advise of the locations considered to be most suitable for the managed release of industrial land which includes part of the Blyth Road area and advises that these areas will form the starting point for a more detailed review of such land, to be undertaken as part of the production of the Hillingdon Local Plan: Part 2 - Site Specific Allocations Local Development Document (LDD).

Specific site designation is being progressed with a draft of the LDD (September 2013) having been prepared. The public have not been consulted on this document and therefore it may be subject to change and little weight should be attached to it at this stage. However, it should be noted that Policy SA 1: Enterprise House, Hayes states:-

"The Council is of the view that Enterprise House is suitable to accommodate residential led mixed use development with commercial uses at ground floor level. Redevelopment should respect the Listed status of the building and meet the following criteria:

- Parking should not be accommodated within the original building envelope;

- Any subdivision to create residential units should protect the original open quality of the spaces within the building;
- Shared circulation spaces and access cores should be contained within the building;
- Particular attention should be paid to the retention of historic features that are important to the listed status of the building. These include the north lights at roof level and the original window frames;
- Particular care should be paid to providing active ground floor uses;
- Proposals will need the criteria of relevant policies in the Development Management Policies Document."

Therefore, this document does consider that this site, together with The Old Vinyl Factory and Gatefold Building site opposite as being suitable for redevelopment to include residential use, and in the case of the latter, where planning permission has now been granted.

The applicant also advises that the site has been extensively marketed for business/industrial uses, but there has been little interest and take up rates have been low. Since the applicant (Workspace Group) purchased the building in 2007, up-to date marketing details have been maintained on their website as this is where 90% of their property portfolio is marketed and has proven effective. Of the 15,275sqm of existing floorspace at Enterprise House, 7,130sqm has been 'moth-balled' on safety grounds and the 4,255sqm of unlet space has been marketed for between 18 months and 4 years, with no new lettings to new tenants for approximately 12 months.

In addition, a financial viability assessment has been submitted with the application which has been independently assessed by a specialist third party contractor. They advise that the appraisal considered 3 options, a commercial renovation with and without an extension, and a residential conversion and an amount of retained commercial use and no affordable housing. The specialist contractor advises that of these, both the commercial options make substantial losses and on this basis are completely unviable. The residential scheme also makes a loss but a relatively small one so that it would only require a small increase in capital growth for the scheme to become viable.

Therefore, in policy terms, the loss of industrial floorspace can be justified where there is sufficient evidence to suggest a sustained lack of demand from potential new occupiers. In this instance, the submitted marketing material suggests that this is the case, although this could be attributable to the poor quality of the floorspace than an overall lack of demand. However, emerging policy does point to an oversupply of industrial floor space and this site has been identified as being potentially suitable for release. Having regard to the individual site circumstances and the need for a residential conversion on the scale proposed to generate the funds needed for the repair and refurbishment of this important listed building, it is considered that there can be no objection to the loss of employment land in principle.

A further issue applicable to the application site is its location opposite the Vinyl Factory site. The Council sees the regeneration of the Vinyl Factory site as pivotal to the wider investment in Hayes. A run down Enterprise House would be seen as a negative factor by investors into the Vinyl Factory site and Hayes.

7.02 Density of the proposed development

As this scheme is for the part residential conversion of an existing building, the Mayor's residential density guidelines are not considered to be applicable in this instance.

7.03 Impact on archaeology/CAs/LBs or Areas of Special Character

The impact of this development upon the Grade II listed Enterprise House and adjoining Grade II listed Jupiter House, together with its impact upon the Botwell: Thorn EMI Conservation Area is considered under the remit of the application for listed building consent (App. No. 11623/APP/2013/3592) which is also being presented to this committee.

7.04 Airport safeguarding

There are no safeguarding issues raised by this application. An informative has been added as regards the use of cranes.

7.05 Impact on the green belt

This site does not form part of the Green Belt, nor is it located close to the Green Belt boundary. As such, no Green Belt issues are raised by the proposal.

7.07 Impact on the character & appearance of the area

Policy BE13 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) requires new development to harmonise with the existing street scene or other features of the area which it is desirable to retain or enhance. Policy BE25 seeks the modernisation and improvement of IBAs through careful design and landscaping and environmental improvements.

Enterprise House forms a prominent and distinctive building within the Blyth Road street scene and surrounding Botwell: Thorn EMI Conservation Area. The proposed works will assist with the restoration and refurbishment of the building, allowing it to be brought back into a good state of repair. As the proposals include a fairly modest extension to the building that would be located at roof top level and set back from the roof edges, it is considered that it would not appear as an unduly conspicuous addition to the building, particularly at street level, and would only be visible in much longer views. The extension's detailed design and use of materials which would be controlled by a recommended condition would also ensure that the extension would harmonise with the existing historic building. Furthermore, other physical alterations to the building such as the additional of open metal walkways between the rear wings have been discretely located to minimise their impact and carefully designed to mimic the industrial character of the building. Other alterations such as the removal of corrugated iron clad external staircases and telecommunications equipment from the water tower will enhance the overall appearance of the building.

The proposed works also seek to add activity and visual interest, particularly at street level, with the addition of a publically accessible cafe with external seating and internal atrium, a small Vinyl Factory museum and interpretation panels. Although car parking on the ground floor on the western side of the building is less successful, this would at least be concealed within the building. Also, there is scope to introduce some boundary planting at the front of the building, whereas currently there is no planting on site.

It is therefore concluded that overall, the proposals would allow for a significant improvement to the existing street scene, in terms of Policies BE13 and BE25 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

7.08 Impact on neighbours

Policies BE20, BE21 and BE24 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) seek to protect the amenities of surrounding residential properties from new development in relation to loss of sunlight, dominance and loss of privacy respectively. The Council's Supplementary Planning Document HDAS: Residential Layouts provides further clarification relevant to this proposal in that it advises that a 21m distance between facing habitable room windows and private amenity space should be

maintained to safeguard privacy.

The nearest existing residential properties to Enterprise House are those fronting Blyth Road to the east of the application site and the quasi-residential uses within Jupiter House sited immediately to the west of the application site. The nearest residential property is No. 101 Blyth Road which is sited some 38m to the east of the application site and separated from Enterprise House by other commercial uses and buildings. The proposed development including the roof top extension would not have a material impact upon the amenities of this property.

As regards Jupiter House, this provides hostel accommodation and the building does contain side bedroom windows on three floors which face onto the shared access running between the two buildings. It is considered that the proposed roof top extension would not have any significant impact upon the outlook and the daylight and sunlight levels from these windows, given that it would be set back some 6.5m from the edge of the roof. This has been confirmed by the submitted Daylight and Sunlight Assessment. These windows are sited some 8.8m from the projecting stairwell on this corner of Enterprise House, which would in part be converted to provide kitchen/ living rooms on the second to fifth floors with side windows directly facing Jupiter House. Although to some extent this is an existing situation, the change of use of the space served by the windows on the stairwell from commercial access to private residence would afford greater opportunities for overlooking and a consequent reduction in privacy. However, as the kitchen/ living rooms also have large rear facing windows, the side windows can be fixed shut and obscure glazed, controlled by condition.

The re-development of the Old Vinyl Factory opposite involves proposed residential units, with those closest to Enterprise House being within the Boiler House on the opposite side of Blyth Road. This would be a part six, part seven storey building with residential use on the upper floors with a roof top amenity area. Although this building and its residential units would be sited some 17m from the front elevation of Enterprise House, and therefore less than the 21m separation distance normally required by Council guidance to afford adequate privacy, this is an across the street relationship where habitable rooms often afford a reduced standard of privacy.

It is therefore considered that the scheme would not result in any significant adverse impact upon the amenities of existing and proposed surrounding residential occupiers, in accordance with Policies BE20, BE21 and BE24 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

7.09 Living conditions for future occupiers

Internal Floor Space

The proposed flats would have internal floor areas ranging from 50 to 64sqm for the 1 bedroom, 2 person units; 72 to 120sqm in the case of the two bedroom, 4 person units; 101 to 122sqm in the case of the 3 bedroom, 5 person units and 133 to 160sqm in the case of the 3 bedroom, 6 person units. All of the flats would provide a sufficient quantum of internal floor space to satisfy the Mayor's 50, 70, 86 and 95sqm minimum floor space standards for each respective type of property.

In any residential conversion scheme, the layout of the units is going to be influenced by the original layout of the building. In this instance, the historic importance of the building further constrains the conversion options available whilst there is a requirement to provide the overall number of residential units proposed if the scheme is to have a chance of

being financially viable which has been demonstrated within the submitted Financial Viability Appraisal.

The submitted Design and Access Statement advises of the design options considered and the factors that have influenced the preferred layout of the submitted scheme. At 10.5m wide, the floor plates of Enterprise House are not of an ideal size and this has led to the inclusion of external bridge walkways providing access to the flats, which avoids the need for internal corridors that would result in the majority of the flats within the main spine of Enterprise House being single aspect and of an awkward depth relative to the outlook afforded by its windows. At the rear of the building, the closeness of the wings of the building at 15m also raises significant overlooking issues. The length of these wings also brings means of escape and staircase issues since a dead-end access corridor can only be 7.5m long before a means of escape has to be provided from 2 directions.

The bridge walkways would be set back from the rear elevation of the main spine of the building by some 3.8m. They have been designed as open and light weight structures and therefore the impact upon the outlook from rear facing windows would be minimised. Furthermore, although the walkways would be sited close to habitable room windows in this elevation, this relationship would be similar to that experienced by ground floor units within more traditional flatted blocks. Although the opportunity exists for overlooking of habitable room windows at closer distances as the bridge walkways enter the wings, this could be mitigated with obscure/ louvred glazing within those parts of the multi-paned windows closest to the walkways.

A Daylight and Sunlight Assessment has also been submitted with the application. This uses the revised Building Research Establishment (BRE) report 'Site Layout Planning for Daylight and Sunlight: A guide to Good Practice' (October 2011). The assessment advises that overall, the proposed accommodation would experience good levels of interior daylight and sunlight availability in the context of the guidance. Although a proportion of the proposed windows and rooms would experience levels of interior daylight and sunlight below the respective BRE guide, these would mainly be bedrooms which the BRE guidance considers to be of less importance than main habitable rooms and levels are low and the breaches localised and marginal. The report advises that in mitigation, the BRE guidance itself states that it is advisory and does not contain mandatory standards and therefore needs to be interpreted flexibly. Furthermore, the standards are based on a typical two storey suburban model and it is reasonable to assume that expectations of levels of daylight and sunlight would be different in a more urban setting. Therefore, given the limited nature of the breaches, the constraints imposed by the listed status of the building to adopt mitigation measures to alleviate lower light levels, and the weight that needs to be attached to making efficient use of developed land, overall, the quality of the residential accommodation in terms of daylight and sunlight levels is acceptable and the scheme is considered to be consistent with the objectives and requirements of the BRE guidance.

As mentioned in Section 7.08 above, the re-development of the Old Vinyl Factory site opposite would result in habitable rooms facing habitable rooms within Enterprise House within a 17m distance, but this is an across the street relationship where reduced standard of privacy can be expected.

External Amenity Space

The Council's amenity space standards require a minimum total of 2,320sqm of amenity space to be provided for the 96 units. This scheme would provide the 14 no. maisonettes

split between the fifth and sixth floors with private rooftop terraces, ranging in size from 5 to 19sqm, totalling some 120sqm. A communal residents' terrace is also proposed on the roof, which would provide 129sqm of external amenity space. This equates to a total 249sqm of external amenity space.

The amount of external amenity space provided is deficient in terms of satisfying the Council's standards. It is also noted that no provision has been made for a children's play area. Due to the constraints of the site and the listed building, additional amenity space is not available on site (the agent's were asked to explore the possibility of providing a decked amenity space above the surface car park, but they advise that whilst they have sole rights to use the surface car park, these rights do not extend to building over it). The FVA also demonstrates that the scheme cannot generate sufficient funds to contribute to the provision/ improvement of open space elsewhere.

However, the technical requirement is only 108sqm. of playspace for children as assessed under the Mayor's London Plan, 60sqm of which would be for under 5s. The cafe is 226sqm and officers are aware of child friendly cafes incorporating designated soft play areas. It is considered that a 226sqm would be large enough to incorporate a 60sqm soft play area. A condition is therefore imposed to this effect.

The nearest area of public open space is Lake Farm Country Park to the north west of Enterprise House, sited within a walking distance of some 575m. The most direct route to Lake Farm uses the towpath of the Grand Union Canal, sited some 270m from Enterprise House which itself forms a green link and part of the blue ribbon network, and therefore forms part of a green corridor which itself is valued and recognized for its contribution towards recreation and wildlife. Therefore, public open space is available nearby for residents of Enterprise House within a reasonable walking distance. There are also a number of publicly accessible play spaces for children which are to be provided as part of the Old Vinyl Factory redevelopment directly opposite the site.

Conclusion

This scheme needs to be considered as currently submitted. On this basis, it has been demonstrated that the overall amount and type of residential accommodation proposed is required to secure the funds that will enable this important historic building to be renovated and refurbished and that given the quantum of residential space required, the proposed layout does make best use of the space available. On balance, although the residential accommodation proposed is not ideal, particularly in terms of the lack of external amenity space and children's play space, the quality of the accommodation is not so substandard that a refusal of permission is justified given the significant associated benefits of bringing this dilapidated historic building back into use and the availability of facilities in the vicinity of the site.

7.10 Traffic impact, Car/cycle parking, pedestrian safety

Policies AM2 and AM7 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) advise respectively that proposals for development will be assessed against their contribution to traffic generation and impact on congestion, having regard to the present and potential capacity of public transport and that the traffic generated by proposed developments would need to be accommodated on principal roads without increasing demand along roads or at junctions already used to capacity, not prejudice the free flow of traffic, nor diminish environmental benefits brought about by other road improvement schemes or infiltrate local roads. Policy AM9 supports cycle provision, including the need for cycle storage provision within development schemes and Policy

AM14 advises that development should accord with adopted car parking standards.

The application is supported by a Transport Statement and a Travel Plan. The Council's Highway Engineer has reviewed the former and advises that despite part of the Transport Statement regarding existing trip generation being misrepresentative, the overall impact of the proposed development on the capacity of the surrounding road network would not be significant as compared to the existing use and is acceptable. The Council's Transport and Aviation Team have also reviewed the Travel Plan and consider its measures to reduce reliance on the private car are acceptable.

Following concerns raised by the Council's Highway Engineer regarding the more onerous servicing requirements for storage uses (Class B8), the applicant has now agreed to remove this use class element from the proposals. The Highway Engineer also advises that further consideration has now been given to the servicing of the adjacent Owen Coyle building. Previously, 3 proposed spaces on the shared access were considered to have the potential to conflict with the servicing of the Owen Coyle building which have now been relocated in the revised car parking layout.

A car club would be provided, for which 2 parking spaces would be reserved. Also, it is proposed to utilise the commercial car parking spaces for visitors of the residents during evenings and weekends and a permit free agreement to exclude the future residents of the development from eligibility for the parking permits for the adjacent Controlled Parking Zone (CPZ).

The Council's Highway Engineer advises that no objections are raised to the overall parking arrangements, although 5 no. parking spaces for motorbikes etc. should be provided in addition to those for cars and bicycles. Also, provision for electric vehicles would need to be provided in the order of 20% passive and 10% active for the commercial use and 20% active and 20% passive for the residential use and a parking allocation and management plan should be secured, including but not limited to, allocating residential car parking to the 2 and 3 bed flats, visitor parking and electric charging spaces. As the location of the refuse store is not within the maximum trundle distance of 10m for collection from the carriageway in Blyth Road, it is proposed that on-site management staff will bring the bins to the back of the Blyth Road footway on collection days where appropriate space is available. Although not ideal, the Highway Engineer advises that this arrangement is workable. Details of the provisions to be made for motorbike parking, electric charging, the parking allocation and management plan and the refuse management plan can be controlled by recommended conditions. Details of the operation and management of the car club, residential use of commercial parking during evenings and weekends and restriction of resident's eligibility to use the parking spaces within the SPZ would need to be secured within the S106 agreement.

The Highway Engineer concludes that subject to the issues raised in his comments being covered through suitable planning conditions/S106 agreement, no objection is raised from a highways aspect on this application.

7.11 Urban design, access and security

Security

A secure by design condition has been added.

7.12 Disabled access

Policies 3.8 and 7.2 of the London Plan (July 2011) promote housing choice and require all new development to provide an inclusive environment that achieves the highest

standards of accessibility and inclusive design respectively. The Council's Supplementary Planning Document "Accessible Hillingdon" (May 2013) provides detailed design guidance on accessibility issues.

The Design and Access Statement advises that all the units would be fully accessible, with 3 apartments on every floor being built as fully wheelchair accessible. The entrances would achieve suitable gradients, and wheelchair accessible lifts have been incorporated into the design.

Nine accessible parking spaces are proposed within the internal car park, which would provide ample car parking for the wheelchair standard accommodation.

The Council's Access Officer advises that since the plans have been revised in response to his original comments, the proposal is now satisfactory from an accessibility perspective. As such, the scheme complies with Policies 3.8 and 7.2 of the London Plan (July 2011) and the Council's Supplementary Planning Document "Accessible Hillingdon" (May 2013).

7.13 Provision of affordable & special needs housing

A financial viability assessment has been submitted with the application which has been independently assessed by a specialist third party contractor. He advises that 3 options have been considered, a commercial renovation with and without an extension, and a residential conversion with a small amount of retained commercial and no affordable housing. Both the commercial options make substantial losses and are completely unviable. The residential option also makes a loss but a relatively small one. However, the situation is likely to change if house prices continues to rise (which is quite likely if as expected the re-development of the Old Vinyl Factory site markedly enhances local values). A review mechanism in the planning agreement based on outturn sales values is therefore required, albeit noting that 5% growth would be necessary to achieve viability.

7.14 Trees, landscaping and Ecology

Trees and Landscaping

Policy BE38 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) requires development proposals to retain and utilise topographical and landscape features of merit and to provide for new planting and landscaping wherever it is appropriate.

There are no existing trees or other landscape features of merit associated with this site which apart from a narrow strip of raised beds along the Clayton Road frontage of the surface car park, is entirely covered by buildings and hardstanding.

The submitted Design and Access Statement advises that there is very limited space or opportunity for landscape enhancement on the site, with the only possible areas being the Blyth Road site frontage, where the railings are to be removed and 429sqm of space could potentially be available to include landscape enhancement, with seating and planting in containers and the 126sqm communal rooftop garden.

The Council's Tree and Landscape Officer raises no objections to the proposal, only noting that some of the plant species recommended for use and listed within the Phase 1 Ecological Appraisal (section 5.0) would not be appropriate for the space available but advises that this could be dealt with by way of condition to ensure that the proposals do preserve and enhance the character and appearance of the surrounding built environment and contribute to wildlife. The suggested conditions are recommended and therefore the scheme would make an appropriate contribution to the enhancement of the overall

appearance of the street scene and complies with Policy BE38 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

Ecology

The Phase 1 Ecological Appraisal advises that the site is of low ecological value. In particular, given the site's location and nature, the main focus of the survey was upon bats and nesting birds. The survey concluded that Enterprise House lacks suitable features for roosting bats and no evidence of their presence was found so that the site is considered to have negligible potential to support roosting bats. Numbers of feral pigeon were observed entering the building. The appraisal recommends that a licensed pest controller is commissioned to make an inspection of the building, and to determine a suitable course of action for the removal of feral pigeon and any other pest species. The report also advises that appropriate wildlife enhancement on site should take the form of bird boxes and use native species in any planting. These are controlled by condition.

7.15 Sustainable waste management

The scheme makes adequate provision, integral within the buildings for waste and recycling at two points adjacent to the main entrances to each of the blocks.

7.16 Renewable energy / Sustainability

The Council's Sustainability Officer advises that this is a refurbishment proposal for a building with considerable heritage value and therefore as a consequence, achieving the energy reduction targets through improvements to the fabric of the building are complicated at best and impossible in some instances.

The Officer does advise that the opportunities to link into the Old Vinyl Factory district heating network opposite this site has considerable potential and this had been raised with the applicant, but little progress has been made. Therefore the plans as presented set out a development that would have sizeable energy loads (compared to a building regulations compliant development) and this would have implications for energy bills of residents.

The potential link to the Old Vinyl Factory site needs to be properly analysed and considered to ensure energy reductions measures for Enterprise House have been exhausted and a condition is recommended to ensure this is explored. This is recommended.

7.17 Flooding or Drainage Issues

The Council's Flood and Water Management Officer advises that the site is reducing surface water run off by 15% and commits to further work to explore opportunities to control water further at the detailed design stage and therefore, no objections are raised to the granting of permission on flood risk grounds, subject to the imposition of a suitable Sustainable Drainage condition. This forms part of the officer recommendation.

7.18 Noise or Air Quality Issues

Noise

The Council's Environmental Protection Officer (Noise) does not raise any objections to the proposal on noise grounds, subject to conditions controlling the hours of collection/deliveries to the commercial premises and the submission of a sound insulation scheme for the control of noise transmission from commercial to residential and from road/rail noise to residential premises. Given that this is an IBA, it is considered that controlling collection/delivery hours would not be appropriate. The requirement for a sound insulation scheme forms part of the officer's recommendation.

Air Quality

The application site is located within the Borough's Air Quality Management Area (AQMA). The Council's Environmental Protection Officer has reviewed the Air Quality Assessment submitted with the application and notes that it is now intended to retain a flue belonging to the adjoining Owen Coyle Factory which runs up the rear of Enterprise House. The officer details a number of limitations of the study, but raises no objections to the proposal, subject to a £25,000 contribution towards the air quality monitoring network in the area and two conditions to deal with ingress of Polluted Air and details of energy provision, which form part of the officer's recommendation.

7.19 Comments on Public Consultations

The current proposals, especially to the west end, would have a major impact on the operational ability of the adjoining factory building. The proposals introduce additional parking around the perimeter of Enterprise House, particularly at its west end, which is the main industrial access, and would now include the main entrance and exit to the ground floor parking, which are immediately adjacent to the main entrance of Owen Coyle (Anodising) Ltd. from which all goods and finished products are accessed. Photographs are attached showing 40ft articulated lorries delivering and receiving finished goods with the ancillary manoeuvring that is required. Current proposals would totally prevent these activities taking place and have severe financial implications. Rights of access and manoeuvring off Blyth Road to main factory entrance have been established for over 40 years,

(ii) There is a main exhaust flue from Owen Coyle (Anodising) Ltd. which is attached to Enterprise and there is no indication in the proposals for this to be accommodated which has been established for over 40 years,

(iii) The introduction of 96 residential apartments adjacent to a working factory complex (3.5m) with fire escape staircases leading directly onto the common passageway between the two buildings together with commercial traffic movements and manoeuvring all have major environmental and health and safety issues,

(iv) Lack of adequate amenity space will exacerbate health and safety issues,

(v) Concentration of residential apartments and associated parking will put enormous pressure on vehicular movements around existing one-way system on Blyth Road, which is already overcrowded.

7.20 Planning obligations

Policy R17 of the Council's Unitary Development Plan states that: 'The Local Planning Authority will, where appropriate, seek to supplement the provision of recreation open space, facilities to support arts, cultural and entertainment activities, and other community, social and education facilities through planning obligations in conjunction with other development proposals'.

As of 1st August 2014, the Council's CIL would become effective which replaces a number of S106 requirements. In this instance however, it is only the new floorspace which is CIL liable, which would require a payment of £63,650.

A S106 Agreement would be needed to secure the following:-

1. Affordable Housing: A review mechanism allowing the council to review the value of the scheme in the future to secure affordable housing.
2. Highways: Highways works together with a S278/S38 Agreement will need to be secured to include details of the operation and management of the car club; residential visitors parking in the commercial car parking spaces during evenings and weekends and a permit free agreement to exclude the future residents of the development from eligibility

for the parking permits for the adjacent CPZ.

3. Conservation/Management Plan to be agreed for the future maintenance of the building

4. Construction Training

- Training Costs: £2500 per £1m build cost

- Coordinator costs: $96/160 \times £71,675 + 4500/7500 \times £71,675 = £86,010$

5. Air Quality monitoring fee: £25,000

6. Project Management & Monitoring Fee: 5% of total cash contributions.

The applicant has agreed to the above heads of terms. As such, the scheme complies with Policy R17 of the Hillingdon Local Plan: Part Two - Saved UDP Policies.

The new floorspace would also be liable for the Mayoral CIL.

7.21 Expediency of enforcement action

There are no enforcement issues raised by this application.

7.22 Other Issues

Land Contamination

The proposals are not likely to require significant construction work on the ground floor, which would remain in commercial use. The Council's Environmental Health Officer (Land Contamination) advises that contamination is suspected for all or part of the site given the onsite use and the industrial history of the site and surrounding sites. A contaminated land condition is recommended, together with a soil importation condition. These form part of the officer's recommendation.

8. Observations of the Borough Solicitor

General

Members must determine planning applications having due regard to the provisions of the development plan so far as material to the application, any local finance considerations so far as material to the application, and to any other material considerations (including regional and national policy and guidance). Members must also determine applications in accordance with all relevant primary and secondary legislation.

Material considerations are those which are relevant to regulating the development and use of land in the public interest. The considerations must fairly and reasonably relate to the application concerned.

Members should also ensure that their involvement in the determination of planning applications adheres to the Members Code of Conduct as adopted by Full Council and also the guidance contained in Probit in Planning, 2009.

Planning Conditions

Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal. Planning conditions should only be imposed where Members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

Planning Obligations

Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act

1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

Equalities and Human Rights

Section 149 of the Equalities Act 2010, requires the Council, in considering planning applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The requirement to have due regard to the above goals means that members should consider whether persons with particular protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic. Where equalities issues arise, members should weigh up the equalities impact of the proposals against the other material considerations relating to the planning application. Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be taken into account in weighing up the merits of an application. The weight to be given to any equalities issues is a matter for the decision maker to determine in all of the circumstances.

Members should also consider whether a planning decision would affect human rights, in particular the right to a fair hearing, the right to respect for private and family life, the protection of property and the prohibition of discrimination. Any decision must be proportionate and achieve a fair balance between private interests and the public interest.

9. Observations of the Director of Finance

10. CONCLUSION

Officers consider that substantial weighting should be given to restoration of the listed building and positive benefits of the building being brought back into active use at what is a prominent location in Hayes.

The conversion of any listed building to a new use will involve an element of compromise. In this case, the proposal must also be capable of generating sufficient funds to secure the restoration of the building, whilst remaining viable. It is considered that although the residential accommodation proposed is not ideal and the subdivision of the building is extensive that would adversely impact upon the building's character, this scheme offers an acceptable compromise that offers the most realistic prospect of securing the future of the building whilst assisting with re-generation of the area.

It is recommended for approval.

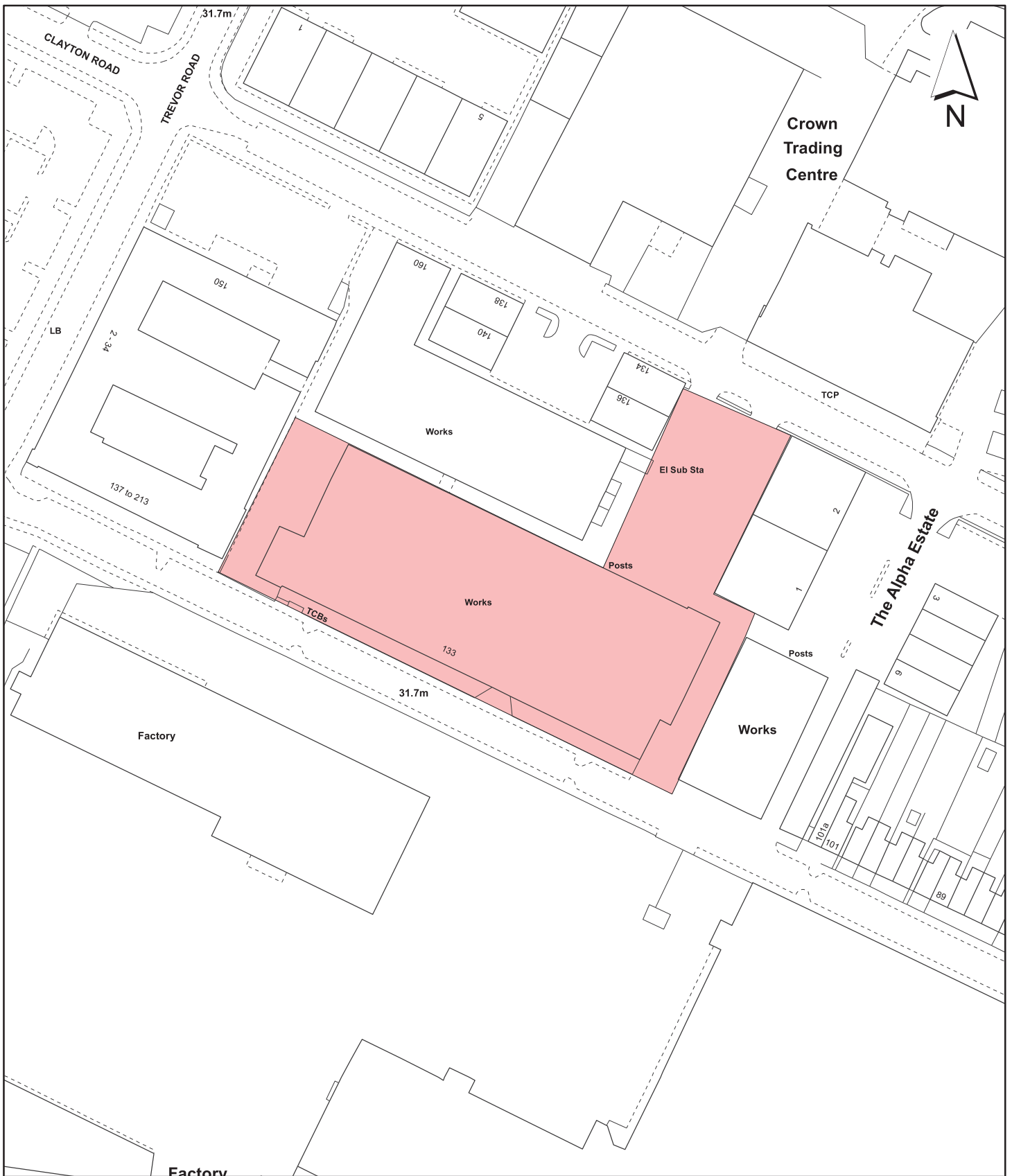
11. Reference Documents



National Planning Policy Framework (March 2012)
Planning Practice Guidance (March 2014)
London Plan (July 2011)
Mayor's Housing Supplementary Planning Guidance, November 2012
Hillingdon Local Plan (November 2012)
Council's Draft Proposed Site Allocations and Designations, September 2013

Consultation Responses

Contact Officer: Richard Phillips

Telephone No: 01895 250230



<p>Notes</p> <p> Site boundary</p> <p>For identification purposes only.</p> <p>This copy has been made by or with the authority of the Head of Committee Services pursuant to section 47 of the Copyright, Designs and Patents Act 1988 (the Act). Unless the Act provides a relevant exception to copyright.</p> <p>© Crown copyright and database rights 2014 Ordnance Survey 100019283</p>	<p>Site Address</p> <p align="center">Enterprise House 133 Blyth Road Hayes</p>		<p align="center">LONDON BOROUGH OF HILLINGDON</p> <p align="center">Residents Services Planning Section</p> <p align="center">Civic Centre, Uxbridge, Middx. UB8 1UW Telephone No.: Uxbridge 250111</p>
	<p>Planning Application Ref:</p> <p align="center">11623/APP/2013/3606</p>	<p>Scale</p> <p align="center">1:1,250</p>	 <p align="center">HILLINGDON LONDON</p>
	<p>Planning Committee</p> <p align="center">Major Page 119</p>	<p>Date</p> <p align="center">August 2014</p>	

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Report of the Head of Planning, Sport and Green Spaces

- Address** ENTERPRISE HOUSE, 133 BLYTH ROAD HAYES
- Development:** Erection of extensions at roof level, erection of external bridge links on the rear elevation and internal works associated with the change of use of part of enterprise house to create 96 residential units (class C3) and associated car parking, retention of approximately 4,500 sqm (GIA) of employment use (Class B1 and B8) at ground and first floor levels and cafe. (Application for Listed Building Consent)
- LBH Ref Nos:** 11623/APP/2013/3592
- Drawing Nos:** Planning Statement
Design and Access Statement
Utility Report
Agent's email dated 9 4 14
524 A (31) 103 Rev. P0
524 A (31) 104 Rev. P0
3733-S-109 Rev. P2
3733-S-110 Rev. P2
3733-S-111 Rev. P2
3733-S-112 Rev. P2
3733-S-113 Rev. P2
3733-S-114 Rev. P2
3733-S-120 Rev. P2
3733-S-121 Rev. P2
3733-S-122 Rev. P2
3733-S-123 Rev. P2
Heritage Impact Assessment
524 A (31) 105 Rev. P0
524 A (31) 106 Rev. P0
524 C (31) 101 Rev. P0
524 C (31) 102 Rev. P0
524 C (31) 103 Rev. P0
524 C (31) 104 Rev. P0
524 L (24) 200 Rev. P0
524 C (22) 101 Rev. P0
524 L (24) 201 Rev. P0
524 A (31) 102 Rev. P0
524 A (31) 101 Rev. P0
Agent's email dated 9/4/14
Agent's email dated 25/4/14
524 L (0-) 203 Rev. P0
524 L (0-) 011 Rev. P1
524 L (0-) 012 Rev. P1
524 L (0-) 013 Rev. P1
524 L (0-) 014 Rev. P1
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524 L (0-) 032 Rev. P1
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524 L (-- 108 Rev. P3
524 L (-- 104 Rev. P5
524 L (-- 601 Rev. P1
524 L (-- 210 Rev. P3
524 L (-- 10 Rev. P2
524 L (-- 107 Rev. P7
524 L (-- 201 Rev. P4
524 L (-- 600 Rev. P2
524 L (-- 106 Rev. P6
524 L (-- 200 Rev. P4
3733-D-01 Rev. P1
3733-D-02 Rev. P1
524 L (38) 100 Rev. P0
3733-S-106 Rev. P2
524 C (31) 105 Rev. P0
524 C (31) 106 Rev. P0
524 L (-- 310 Rev. P2
524 L (-- 312 Rev. P2
Stage (C) Report
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SK10.2
524 L (31) 300 Rev. P0
524 C (22) 102 Rev. P0
524 L (24) 100 Rev. P0
524 L (0-) 201 Rev. P1
524 L (0-) 300 Rev. P1
524 L (0-) 301 Rev. P2
524 L (0-) 302 Rev. P2
524 L (0-) 303 Rev. P2
524 L (0-) 304 Rev. P2
524 C (22) 100 Rev. P0
524 C (31) 110 Rev. P0
524 C (31) 200 Rev. P0
524 L (-- 202 Rev. P2
524 L (-- 212 Rev. P2
524 L (-- 211 Rev. P3

524 L (32) 100 Rev. P0
524 L (--) 300 Rev. P4
524 L (--) 311 Rev. P3
524 L (--) 303 Rev. P2
524 L (--) 301 Rev. P4
524 L (--) 313 Rev. P3
524 L (--) 102 Rev. P5
524 L (--) 101 Rev. P10
Building Services Report, Rev. 03

Date Plans Received:	16/11/2013	Date(s) of Amendment(s):	25/06/2014
Date Application Valid:	21/01/2014		09/04/2014
			25/04/2014
			08/05/2014
			05/12/2013
			22/01/2014

2. RECOMMENDATION

APPROVAL subject to the following:

1 LB1 Time Limit (3 years) - Listd Building Consent

The works hereby permitted shall be begun before the expiration of three years from the date of this consent.

REASON

To comply with Section 18 of the Planning (Listed Buildings and Conservation Areas) Act 1990.

2 NONSC Non Standard Condition

The development hereby consented shall not be carried out except in complete accordance with the details shown on the submitted plans, numbers 524 L (--) 100 Rev. P7, 524 L (--) 101 Rev. P10, 524 L (--) 102 Rev. P5, 524 L (--) 104 Rev. P5, 524 L (--) 106 Rev. P6, 524 L (--) 107 Rev. P7, 524 L (--) 108 Rev. P3, 524 L (--) 200 Rev. P4, 524 L (--) 201 Rev. P4, 524 L (--) 202 Rev. P2, 524 L (--) 210 Rev. P3, 524 L (--) 211 Rev. P3, 524 L (--) 212 Rev. P2, 524 L (--) 300 Rev. P4, 524 L (--) 301 Rev. P4, 524 L (--) 302 Rev. P3, 524 L (--) 303 Rev. P2, 524 L (--) 310 Rev. P2, 524 L (--) 311 Rev. P3, 524 L (--) 312 Rev. P2, 524 L (--) 313 Rev. P3, 524 L (--) 600 Rev. P2 and 524 L (--) 601 Rev. P1, 524 L (0-) 011 Rev. P1, 524 L (0-) 012 Rev. P1, 524 L (0-) 013 Rev. P1, 524 L (0-) 014 Rev. P1, 524 L (0-) 015 Rev. P1, 524 L (0-) 016 Rev. P1, 524 L (0-) 017 Rev. P1, 524 L (0-) 030 Rev. P1, 524 L (0-) 031 Rev. P1, 524 L (0-) 032 Rev. P1, 524 L (0-) 033 Rev. P1, 524 L (0-) 034 Rev. P1, 524 L (38) 100 Rev. P0, 524 A (31) 101 Rev. P0, 524 A (31) 102 Rev. P0, 524 A (31) 103 Rev. P0, 524 A (31) 104 Rev. P0, 524 A (31) 105 Rev. P0, 524 A (31) 106 Rev. P0, 524 C (22) 100 Rev. P0, 524 C (22) 101 Rev. P0, 524 C (22) 102 Rev. P0, 524 C (31) 101 Rev. P0, 524 C (31) 102 Rev. P0, 524 C (31) 103 Rev. P0, 524 C (31) 104 Rev. P0, 524 C (31) 105 Rev. P0, 524 C (31) 106 Rev. P0, 524 C (31) 110 Rev. P0, 524 C (31) 200 Rev. P0, 524 L (24) 100 Rev. P0, 524 L (24) 201 Rev. P0, 3733-D-02 Rev. P1, 3733-S-101 Rev. P2, 3733-S-102 Rev. P2, 3733-S-103 Rev. P2, 3733-S-104 Rev. P2, 3733-S-105 Rev. P2, 3733-S-106 Rev. P2, 3733-S-107 Rev. P2, 3733-S-108 Rev. P2, 3733-S-109 Rev. P2, 3733-S-110 Rev. P2, 3733-S-111 Rev. P2,

3733-S-112 Rev. P2, 3733-S-113 Rev. P2, 3733-S-114 Rev. P2, 3733-S-120 Rev. P2, 3733-S-121 Rev. P2, 3733-S-122 Rev. P2 and 3733-S-123 Rev. P2 and shall thereafter be retained/maintained for as long as the development remains in existence.

REASON

To ensure the development complies with the provisions of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) and the London Plan (July 2011).

3 NONSC Non Standard Condition

No development shall take place until details of all materials and external surfaces, have been submitted to and approved in writing by the Local Planning Authority, to include the following:-

- A schedule and full specification for building repairs/renovation to be agreed prior to the start of relevant works on site
- A methodology for cleaning works and samples areas of brickwork and concrete cleaning to be agreed on site
- External paint colours for the elevations, to include brickwork, concrete and windows/downpipes,
- Details of the works to windows, including details of the repair and relocation of existing windows; construction and glazing of new windows (including casements),
- Design details, finishes and materials of all external and internal doors, including roller shutters/boxes and also canopies over,
- Details of materials, appearance and location of new downpipes, hopper heads and external pipe work,
- Details of the design, materials and construction of new and refurbished glazed roofs and the new atrium roof; with existing cowls and vents to be reinstated,
- Schedule and details of repairs to original lifts, lift enclosures and related machinery - works to be completed before the building is occupied
- Details of design and construction of 6th floor altered and new roof top additions, including glazing, cladding/roofing materials and brise soleil
- Details of the repair of existing and the design and construction of new hand rails, balustrades, screens and balcony/roof top enclosures
- Details of roof top plant and machinery
- Location, type and finish of flues, louvers and vents, including low level car park vents
- Details of the design, construction, materials, finishes and method of installation of external walk ways
- Details of the design, construction, materials and finishes of the new eastern stair case enclosure
- Full details of works to existing eastern low level additions
- Fire and sound proofing works
- Details of construction and installation of internal partitions and glazed screens- these are to be reversible
- Details of external works to steps, railings and forecourt area to be agreed
- Design and content of interpretation boards to be agreed

Thereafter the development shall be constructed in accordance with the approved details and be retained as such.

Details should include information relating to make, product/type, colour and photographs/images.

REASON

To ensure that the development presents a satisfactory appearance in accordance with Policies BE8 and BE9 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012).

4 CA2 Demolition - requirement for development contract

The works of demolition, including partial demolition hereby approved shall not be commenced before contract(s) for the carrying out of the completion of the entire scheme of works approved under planning permission [Ref: 11623/APP/2013/3606], including the works contract, have been made and evidence of such contract(s) has been submitted to and accepted in writing by the Council as local planning authority.

REASON

To ensure that premature demolition does not occur in accordance with Policy BE4 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012)

INFORMATIVES

1 I52 Compulsory Informative (1)

The decision to GRANT listed building consent has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

2 I53 Compulsory Informative (2)

The decision to GRANT listed building consent has been taken having regard to the policies and proposals in the Hillingdon Unitary Development Plan Saved Policies (September 2007) as incorporated into the Hillingdon Local Plan (2012) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including the London Plan (July 2011) and national guidance.

NPPF12	NPPF - Conserving & enhancing the historic environment
LPP 7.8	(2011) Heritage assets and archaeology
BE4	New development within or on the fringes of conservation areas
BE8	Planning applications for alteration or extension of listed buildings
BE9	Listed building consent applications for alterations or extensions
BE12	Proposals for alternative use (to original historic use) of statutorily listed buildings
BE13	New development must harmonise with the existing street scene.
BE15	Alterations and extensions to existing buildings

3. CONSIDERATIONS

3.3 Relevant Planning History

UDP / LDF Designation and London Plan

The following UDP Policies are considered relevant to the application:-

Part 1 Policies:

PT1.HE1 (2012) Heritage

Part 2 Policies:

NPPF12	NPPF - Conserving & enhancing the historic environment
LPP 7.8	(2011) Heritage assets and archaeology
BE4	New development within or on the fringes of conservation areas
BE8	Planning applications for alteration or extension of listed buildings
BE9	Listed building consent applications for alterations or extensions
BE12	Proposals for alternative use (to original historic use) of statutorily listed buildings
BE13	New development must harmonise with the existing street scene.
BE15	Alterations and extensions to existing buildings

5. Advertisement and Site Notice

5.1 Advertisement Expiry Date:- Not applicable

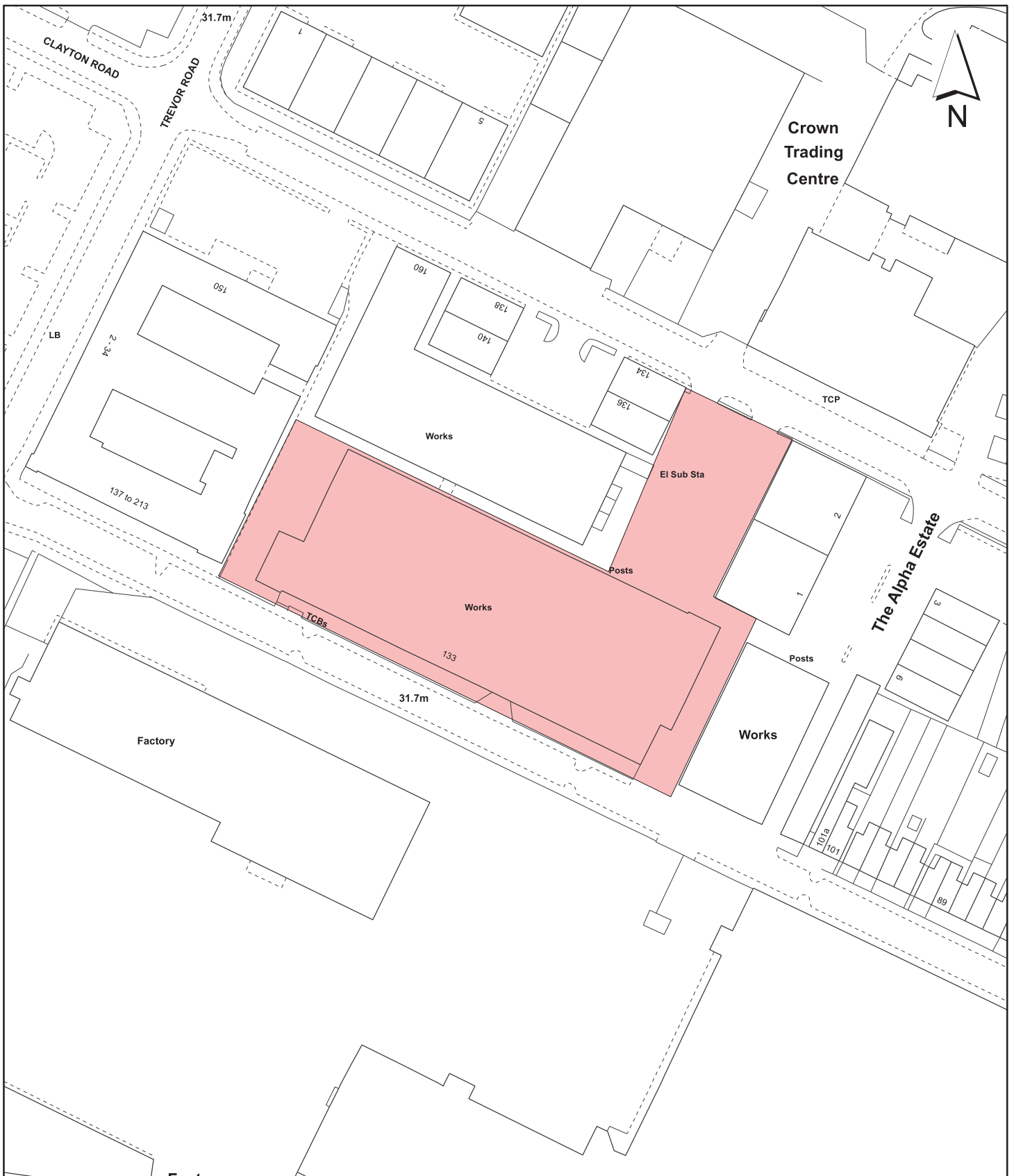
5.2 Site Notice Expiry Date:- Not applicable



6. Consultations

7. MAIN PLANNING ISSUES

Contact Officer: Richard Phillips

Telephone No: 01895 250230



<p>Notes</p> <p> Site boundary</p> <p>For identification purposes only.</p> <p>This copy has been made by or with the authority of the Head of Committee Services pursuant to section 47 of the Copyright, Designs and Patents Act 1988 (the Act). Unless the Act provides a relevant exception to copyright.</p> <p>© Crown copyright and database rights 2014 Ordnance Survey 100019283</p>	<p>Site Address</p> <p align="center">Enterprise House 133 Blyth Road Hayes</p>		<p align="center">LONDON BOROUGH OF HILLINGDON</p> <p align="center">Residents Services Planning Section</p> <p align="center">Civic Centre, Uxbridge, Middx. UB8 1UW Telephone No.: Uxbridge 250111</p>
	<p>Planning Application Ref:</p> <p align="center">11623/APP/2013/3592</p>	<p>Scale</p> <p align="center">1:1,250</p>	 <p align="center">HILLINGDON LONDON</p>
	<p>Planning Committee</p> <p align="center">Major Page 127</p>	<p>Date</p> <p align="center">August 2014</p>	

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Report of the Head of Planning, Sport and Green Spaces

Address 5 STATION ROAD WEST DRAYTON

Development: Demolition of existing public house and erection of 38 flats and 237sqm of retail floorspace with parking, landscaping and amenity space.

LBH Ref Nos: 65480/APP/2014/1018

Drawing Nos: 1720-104 C
1720-103 C
1720-102 C
1720-100 D
1720-114 C
01 Rev. R0
02 Rev. R0
1720-101 L
1720-105 D
1720-106 E
1720-113
1720-107 B
1720-108 B
1720-110
1720-111
Planning, Design and Access Statement, March 2014 (including all appendices)
1720-112
1720-109 C
J-D1132.00/605 Rev. R2

Date Plans Received:	24/03/2014	Date(s) of Amendment(s):	24/07/2014
Date Application Valid:	24/03/2014		03/06/2014
			24/03/2014

1. SUMMARY

This application seeks permission to re-develop this 0.27 hectare town centre site which comprises The Blues Bar Public House fronting Station Road and open yard areas behind in use for parking and open storage. The site is semi-derelict and provides a poor quality environment.

The proposal is for a mixed retail and residential scheme providing 237sqm of retail floorspace fronting Station Road and 38 one and two-bedroomed flats within 2 three to four storey 'L'- shaped blocks. The scheme has been informed by pre-application advice and a previous application which was subsequently withdrawn. The scheme has also undergone a number of revisions following officer advice.

There is no objection to the loss of the public house use or the building, which is of little architectural or historical interest. The retail unit would provide an acceptable use within the primary shopping frontage of the town centre, helping to maintain and enhance the centre's vitality and retail interest.

The scheme has been well designed and would provide two courtyard areas, a landscaped area at the front of the site which would separate the new retail unit and the

adjoining Grade II listed Railway Arms Public House and a central courtyard, which would mainly provide car parking. The Council's Design/ Conservation Officer considers the scheme to be of an appropriate scale within the town centre and one which integrates successfully with the existing built and historic environment.

The proposed residential units would provide an acceptable standard of residential accommodation of which 35% would be affordable. Shared amenity space would mainly be provided in the form of roof top gardens and a children's play area would be provided on Tachbrook Road.

The scheme would not be detrimental to the amenities of surrounding residential occupiers and would not be prejudicial to highway safety.

The scheme also provides a commensurate package of planning benefits. It is recommended accordingly.

2. RECOMMENDATION

That delegated powers be given to the Head of Planning, Green Spaces and Culture to grant planning permission, subject to the following:

A) The Council enters into an agreement with the applicant under Section 106 of the Town and Country Planning Act 1990 (as amended) and/or other appropriate legislation to secure:

- 1. Affordable Housing: 34% Provision as proposed.**
- 2. Highway: S278/S38 for Highways works to include the reconfiguration and extension of the existing loading bay adjacent to the site along Station Road, in accordance with the details first to be submitted and agreed in writing by the LPA and the existing vehicle crossover providing access to the site from Station Road, is reinstated as pedestrian footway, prior to occupation of the development.**
- 3. Construction Training: Training Cost: £2500 per £1m build cost + coordinator costs: 38/160 x £71,675**
- 4. Air Quality: £12,500**
- 5. Project Management & Monitoring Fee: 5% of total cash contributions**

B) That in respect of the application for planning permission, the applicant meets the Council's reasonable costs in the preparation of the S106 Agreement and any abortive work as a result of the agreement not being completed.

C) That officers be authorised to negotiate and agree the detailed terms of the proposed agreement and conditions of approval.

D) That if any of the heads of terms set out above have not been agreed and the S106 legal agreement has not been finalised before the 31st August 2014, or any other period deemed appropriate that delegated authority be given to the Head of Planning, Green Spaces and Culture to refuse the application for the following reason:

'The applicant has failed to provide a commensurate package of planning benefits to maximise the health and social benefits, namely affordable housing, highway improvements, construction training and project management of the scheme to the community. The proposal therefore conflicts with Policy R17 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).'

E) That subject to the above, the application be deferred for determination by the Head of Planning, Green Spaces and Culture under delegated powers, subject to the completion of the legal agreement under Section 106 of the Town and Country Planning Act 1990 and other appropriate powers with the applicant.

F) That should the application be approved following the Council's Community Infrastructure Levy coming into force, the applicant pay the required levy on the additional floorspace actually created.

G) That if the application is approved, the following conditions be attached:-

1 COM3 Time Limit

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

REASON

To comply with Section 91 of the Town and Country Planning Act 1990.

2 COM4 Accordance with Approved Plans

The development hereby permitted shall not be carried out except in complete accordance with the details shown on the submitted plans, numbers 1720-100 D, 1720-101 L, 1720-102 C, 1720-103 C, 1720-104 C, 1720-105 D, 1720-106 E, 1720-107 B, 1720-108 B, 1720-109 C and 1720-114 C and shall thereafter be retained/maintained for as long as the development remains in existence.

REASON

To ensure the development complies with the provisions of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and the London Plan (July 2011).

3 COM6 Levels

No development shall take place until plans of the site showing the existing and proposed ground levels and the proposed finished floor levels of all proposed buildings have been submitted to and approved in writing by the Local Planning Authority. Such levels shall be shown in relation to a fixed and known datum point. Thereafter the development shall not be carried out other than in accordance with the approved details.

REASON

To ensure that the development relates satisfactorily to adjoining properties in accordance with policy BE13 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012)

4 COM7 Materials (Submission)

No development shall take place until details of all materials and external surfaces, including details of balconies have been submitted to and approved in writing by the Local Planning Authority. Thereafter the development shall be constructed in accordance with the approved details and be retained as such.

Details should include information relating to make, product/type, colour and photographs/images.

REASON

To ensure that the development presents a satisfactory appearance in accordance with Policy BE13 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012)

5 RES9 Landscaping (car parking & refuse/cycle storage)

No development shall take place until a landscape scheme has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include: -

1. Details of Soft Landscaping, to include tree planting in the courtyards and the children's play area.
 - 1.a Planting plans (at not less than a scale of 1:100),
 - 1.b Written specification of planting and cultivation works to be undertaken,
 - 1.c Schedule of plants giving species, plant sizes, and proposed numbers/densities where appropriate
2. Details of Hard Landscaping
 - 2.a Covered and Secure Refuse Storage
 - 2.b 38 Covered and Secure Cycle Storage Spaces for the Residential Flats and 12 Spaces for the Retail Unit
 - 2.c Means of enclosure/boundary treatments
 - 2.d Car Parking Layouts (including demonstration that 7 spaces are served by active electric charging points and 7 have passive capacity)
 - 2.e Hard Surfacing Materials
 - 2.f External Lighting
 - 2.g Other structures
3. Living Walls and Roofs
 - 3.a Details of the inclusion of living walls and roofs
4. Details of Landscape Maintenance
 - 4.a Landscape Maintenance Schedule for a minimum period of 5 years.
 - 4.b Proposals for the replacement of any tree, shrub, or area of surfacing/seeding within the landscaping scheme which dies or in the opinion of the Local Planning Authority becomes seriously damaged or diseased.
5. Schedule for Implementation
6. Other
 - 6.a Existing and proposed functional services above and below ground
 - 6.b Proposed finishing levels or contours

Thereafter the development shall be carried out and maintained in full accordance with the approved details.

REASON

To ensure that the proposed development will preserve and enhance the visual amenities of the locality and provide adequate facilities in compliance with policies BE13, BE38 and AM14 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and Policies 5.11 (living walls and roofs) and 5.17 (refuse storage) of the London Plan (July 2011)

6 NONSC Non Standard Condition

Notwithstanding the car parking layout shown on Drw. Nos. 1720-101 L and 1720-114 C, prior to the occupation of the first residential unit, a revised car parking layout and scheme, involving the use of a minimum of 6 car parking stackers shall be submitted to and approved in writing by the Local Planning Authority. This should include details of the management and maintenance regime of the car parking stackers proposed within the

revised car park layout. The strategy and layout shall only be installed in strict accordance with these approved details, unless otherwise agreed in writing with the Local Planning Authority.

REASON

To ensure that accessible off-street car parking is provided, in accordance with Policy AM14 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

7 RES11 Play Area provision of details

No development shall commence until details of play areas for children, including safety and security measures have been submitted to and approved in writing by the Local Planning Authority. Thereafter, the play areas shall be provided prior to the occupation of any unit within the development and maintained for this purpose.

REASON

To ensure that the development makes adequate provision of children's play space in accordance with Policy R1 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and London Plan (July 2011) Policy 3.16 and in pursuance of the Council's duty under section 17 of the Crime and Disorder Act 1998 to consider crime and disorder implications in exercising its planning functions; to promote the well being of the area in pursuance of the Council's powers under section 2 of the Local Government Act 2000, to reflect the guidance contained in the Council's SPG on Community Safety By Design and to ensure the development provides a safe and secure environment in accordance with London Plan (July 2011) Policies 7.1 and 7.3.

8 NONSC Non Standard Condition

None of the development hereby permitted shall be commenced until a method statement has been submitted to, and approved in writing, by the Local Planning Authority to include arrangements to secure that, during any period when concurrent construction is taking place of both the permitted development and of the Crossrail structures and tunnels in or adjacent to the site of the approved development, the construction of the Crossrail structures and tunnels is not impeded.

REASON

To ensure that the development does not prejudice the construction of Crossrail, in accordance with Policy 6.2 of the London Plan (July 2011).

9 RES13 Obscure Glazing

The bedroom windows of Flat 5 on the first floor and Flat 10 on the second floor facing the Railway Arms Public House shall be glazed with permanently obscured glass and non-opening below a height of 1.8 metres taken from internal finished floor level for so long as the development remains in existence.

REASON

To prevent overlooking to adjoining properties in accordance with policy BE24 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012)

10 NONSC Non Standard Condition

Prior to the commencement of development, a detailed energy assessment showing how the development will reduce carbon emissions by 40% from a 2010 Building Regulations compliant development shall be submitted to and approved by the Local Planning Authority. The assessment shall clearly show:

1) The baseline energy demand (kwhr and kgCO₂) for each element of the regulated energy use (e.g. space heating, hot water and electricity) for all the relevant uses (e.g. residential, commercial etc.).

2) The methods to improve the energy efficiency of the development and how this impacts on the baseline emissions and where they will be included within the development.

3) Full details, specification and location of low carbon and renewable energy technology and the impacts they have on the baseline

4) How the technology will be maintained and managed throughout the lifetime of the development.

The development must proceed in accordance with the approved details.

REASON

To ensure appropriate carbon savings are delivered in accordance with London Plan Policy 5.2.

11 NONSC Non Standard Condition

Prior to commencement, a scheme for the provision of sustainable water management shall be submitted to, and approved in writing by the Local Planning Authority. The scheme shall clearly demonstrate how it (follows the strategy set out in Flood Risk Assessment and Surface Water Drainage Strategy, produced by POUS dated October 2013 Ref D1132 R03 incorporates sustainable urban drainage in accordance with the hierarchy set out in Policy 5.15 of the London Plan and will:-

- i. provide information on all Suds features including the method employed to delay and control the surface water discharged from the site and:
 - a. calculations showing storm period and intensity and volume of storage required to control surface water and size of features to control that volume.
 - b. any overland flooding should be shown, with flow paths depths and velocities identified as well as any hazards, (safe access and egress must be demonstrated).
 - c. measures taken to prevent pollution of the receiving groundwater and/or surface waters;
 - d. how they or temporary measures will be implemented to ensure no increase in flood risk from commencement of construction.
- ii. provide a management and maintenance plan for the lifetime of the development of arrangements to secure the operation of the scheme throughout its lifetime. Including appropriate details of Inspection regimes, appropriate performance specification, remediation and timescales for the resolving of issues.
- iii. provide details of the body legally responsible for the implementation of the management and maintenance plan.

The scheme shall also demonstrate the use of methods to minimise the use of potable water through water collection, reuse and recycling and will:-

- iv. incorporate water saving measures and equipment.
- v. provide details of water collection facilities to capture excess rainwater;
- vi. provide details of how rain and grey water will be recycled and reused in the development.

Thereafter the development shall be implemented and retained/maintained in accordance

with these details for as long as the development remains in existence.

REASON

To ensure that surface water run off is controlled to ensure the development does not increase the risk of flooding contrary to Policy EM6 Flood Risk Management in Hillingdon Local Plan: Part 1- Strategic Policies (November 2012) Policy 5.12 Flood Risk Management of the London Plan (July 2011) and National Planning Policy Framework (March 2012) and the Planning Practice Guidance (March 2014). To be handled as close to its source as possible in compliance with Policy 5.13 Sustainable Drainage of the London Plan (July 2011), and conserve water supplies in accordance with Policy 5.15 Water use and supplies of the London Plan (July 2011).

12 NONSC Non Standard Condition

The rating level of noise emitted from the plant and/or machinery hereby approved shall be at least 5 dB below the existing background noise level. The noise levels shall be determined at the nearest residential property. The measurements and assessment shall be made in accordance with British Standard 4142 "Method for rating industrial noise affecting mixed residential and industrial areas".

REASON

To safeguard the amenity of the surrounding area in accordance with Policy OE1 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012).

13 NONSC Non Standard Condition

The development shall not begin until a sound insulation scheme for the control of noise transmission to the adjoining dwellings has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include such combination of sound insulation and other measures as may be approved by the LPA. Thereafter, the scheme shall be implemented and maintained in full compliance with the approved measures.

REASON

To safeguard the amenity of the occupants of surrounding properties in accordance with Policy OE1 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012).

14 NONSC Non Standard Condition

Before the development is commenced, details of any plant, machinery or fuel burnt, as part of the energy provision for the development shall be submitted to the LPA for approval. This shall include pollutant emission rates with or without mitigation technologies. Where a scheme to mitigate emissions is required, this shall be submitted to the LPA for approval. The said scheme should be implemented, and maintained for the life of the development.

REASON:

To safeguard the amenity of neighbouring properties in accordance with Policy OE1 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

15 NONSC Non Standard Condition

The double glazing/ ventilator acoustic specification recommended in the in the Noise and Vibration Assessment Report 31429 shall be implemented in full, including the recommendations regarding balconies in Section 5.8 of the report and shall thereafter be retained.

REASON

To ensure a satisfactory residential environment is achieved, in accordance with Policy OE1 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

16 RES24 Secured by Design

The dwelling(s) shall achieve 'Secured by Design' accreditation awarded by the Hillingdon Metropolitan Police Crime Prevention Design Adviser (CPDA) on behalf of the Association of Chief Police Officers (ACPO). No dwelling shall be occupied until accreditation has been achieved.

REASON:

In pursuance of the Council's duty under section 17 of the Crime and Disorder Act 1998 to consider crime and disorder implications in excising its planning functions; to promote the well being of the area in pursuance of the Council's powers under section 2 of the Local Government Act 2000, to reflect the guidance contained in the Council's SPG on Community Safety By Design and to ensure the development provides a safe and secure environment in accordance with London Plan (July 2011) Policies 7.1 and 7.3.

17 RES26 Contaminated Land

(i) The development hereby permitted shall not commence until a scheme to deal with contamination has been submitted in accordance with the Supplementary Planning Guidance on Land Contamination and approved by the Local Planning Authority (LPA). The scheme shall include all of the following measures unless the LPA dispenses with any such requirement specifically and in writing:

(a) A desk-top study carried out by a competent person to characterise the site and provide information on the history of the site/surrounding area and to identify and evaluate all potential sources of contamination and impacts on land and water and all other identified receptors relevant to the site;

(b) A site investigation, including where relevant soil, soil gas, surface and groundwater sampling, together with the results of analysis and risk assessment shall be carried out by a suitably qualified and accredited consultant/contractor. The report should also clearly identify all risks, limitations and recommendations for remedial measures to make the site suitable for the proposed use.

(c) A written method statement providing details of the remediation scheme and how the completion of the remedial works will be verified shall be agreed in writing with the LPA prior to commencement.

(ii) If during development or works contamination not addressed in the submitted remediation scheme is identified, an addendum to the remediation scheme must be agreed with the LPA prior to implementation; and

(iii) All works which form part of the remediation scheme shall be completed and a verification report submitted to the Council's Environmental Protection Unit before any part of the development is occupied or brought into use unless the LPA dispenses with any such requirement specifically and in writing.

REASON

To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems and the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors in accordance with Policy OE11 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

18 NONSC Non Standard Condition

No contaminated soils or other materials shall be imported to the site. All imported soils for landscaping purposes shall be clean and free of contamination. Before any part of the development is occupied, all imported soils, or reused onsite topsoils shall be independently tested for chemical contamination, and the results of this testing shall be submitted and approved in writing by the Local Planning Authority. All soils used for gardens and/or landscaping purposes shall be clean and free of contamination.

Note: The Environmental Protection Unit (EPU) must be consulted for their advice when using this condition.

REASON

To ensure that the occupants of the development are not subject to any risks from soil contamination in accordance with Policy OE11 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

19 NONSC Non Standard Condition

None of the development hereby permitted shall be commenced until a method statement has been submitted to, and approved in writing, by the Local Planning Authority to include arrangements to secure that, during any period when concurrent construction is taking place of both the permitted development and of the Crossrail structures and tunnels in or adjacent to the site of the approved development, the construction of the Crossrail structures and tunnels is not impeded.

REASON

To ensure that the development does not prejudice the construction of Crossrail, in accordance with Policy 6.2 of the London Plan (July 2011).

INFORMATIVES

1 I52 Compulsory Informative (1)

The decision to GRANT planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

2 I53 Compulsory Informative (2)

The decision to GRANT planning permission has been taken having regard to the policies and proposals in the Hillingdon Unitary Development Plan Saved Policies (September 2007) as incorporated into the Hillingdon Local Plan (2012) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including the London Plan (July 2011) and national guidance.

NPPF1	NPPF - Delivering sustainable development
NPPF2	NPPF - Ensuring the vitality of town centres
NPPF4	NPPF - Promoting sustainable transport
NPPF6	NPPF - Delivering a wide choice of high quality homes
NPPF7	NPPF - Requiring good design
NPPF10	NPPF - Meeting challenge of climate change flooding costal
NPPF12	NPPF - Conserving & enhancing the historic environment
LPP 2.15	(2011) Town Centres
LPP 3.1	(2011) Ensuring equal life chances for all

LPP 3.2	(2011) Improving health and addressing health inequalities
LPP 3.3	(2011) Increasing housing supply
LPP 3.4	(2011) Optimising housing potential
LPP 3.5	(2011) Quality and design of housing developments
LPP 3.6	(2011) Children and young people's play and informal recreation (strategies) facilities
LPP 3.8	(2011) Housing Choice
LPP 3.12	(2011) Negotiating affordable housing (in) on individual private residential and mixed-use schemes
LPP 3.13	(2011) Affordable housing thresholds
LPP 3.14	(2011) Existing Housing - Efficient use of stock
LPP 3.16	(2011) Protection and enhancement of social infrastructure
LPP 4.7	(2011) Retail and town centre development
LPP 4.8	(2011) Supporting a Successful and Diverse Retail Sector
LPP 5.2	(2011) Minimising Carbon Dioxide Emissions
LPP 5.3	(2011) Sustainable design and construction
LPP 5.6	(2011) Decentralised Energy in Development Proposals
LPP 5.7	(2011) Renewable energy
LPP 5.9	(2011) Overheating and cooling
LPP 5.10	(2011) Urban Greening
LPP 5.11	(2011) Green roofs and development site environs
LPP 5.12	(2011) Flood risk management
LPP 5.13	(2011) Sustainable drainage
LPP 5.15	(2011) Water use and supplies
LPP 5.21	(2011) Contaminated land
LPP 6.2	(2011) Providing public transport capacity and safeguarding land for transport
LPP 6.3	(2011) Assessing effects of development on transport capacity
LPP 6.5	(2011) Funding Crossrail and other strategically important transport infrastructure
LPP 6.9	(2011) Cycling
LPP 6.10	(2011) Walking
LPP 6.13	(2011) Parking
LPP 7.1	(2011) Building London's neighbourhoods and communities
LPP 7.2	(2011) An inclusive environment
LPP 7.3	(2011) Designing out crime
LPP 7.4	(2011) Local character
LPP 7.5	(2011) Public realm
LPP 7.6	(2011) Architecture
LPP 7.8	(2011) Heritage assets and archaeology
LPP 7.9	(2011) Heritage-led regeneration
LPP 7.14	(2011) Improving air quality
LPP 7.15	(2011) Reducing noise and enhancing soundscapes
LPP 7.19	(2011) Biodiversity and access to nature
LPP 7.21	(2011) Trees and woodland
LPP 8.2	(2011) Planning obligations
BE10	Proposals detrimental to the setting of a listed building
BE13	New development must harmonise with the existing street scene.
BE18	Design considerations - pedestrian security and safety
BE19	New development must improve or complement the character of the

	area.
BE20	Daylight and sunlight considerations.
BE21	Siting, bulk and proximity of new buildings/extensions.
BE22	Residential extensions/buildings of two or more storeys.
BE23	Requires the provision of adequate amenity space.
BE24	Requires new development to ensure adequate levels of privacy to neighbours.
BE26	Town centres - design, layout and landscaping of new buildings
BE38	Retention of topographical and landscape features and provision of new planting and landscaping in development proposals.
OE1	Protection of the character and amenities of surrounding properties and the local area
OE5	Siting of noise-sensitive developments
OE8	Development likely to result in increased flood risk due to additional surface water run-off - requirement for attenuation measures
H3	Loss and replacement of residential accommodation
H4	Mix of housing units
R2	Provision of recreation, entertainment and leisure facilities in Town Centres
R17	Use of planning obligations to supplement the provision of recreation, leisure and community facilities
AM2	Development proposals - assessment of traffic generation, impact on congestion and public transport availability and capacity
AM7	Consideration of traffic generated by proposed developments.
AM9	Provision of cycle routes, consideration of cyclists' needs in design of highway improvement schemes, provision of cycle parking facilities
AM14	New development and car parking standards.
AM15	Provision of reserved parking spaces for disabled persons
HDAS-LAY	Residential Layouts, Hillingdon Design & Access Statement, Supplementary Planning Document, adopted July 2006
LDF-AH	Accessible Hillingdon , Local Development Framework, Supplementary Planning Document, adopted January 2010
SPD-NO	Noise Supplementary Planning Document, adopted April 2006
SPG-AQ	Air Quality Supplementary Planning Guidance, adopted May 2002
SPG-CS	Community Safety by Design, Supplementary Planning Guidance, adopted July 2004
SPD-PO	Planning Obligations Supplementary Planning Document, adopted July 2008

3 I2 Encroachment

You are advised that if any part of the development hereby permitted encroaches by either its roof, walls, eaves, gutters, or foundations, then a new planning application will have to be submitted. This planning permission is not valid for a development that results in any form of encroachment.

4 I6 Property Rights/Rights of Light

Your attention is drawn to the fact that the planning permission does not override property rights and any ancient rights of light that may exist. This permission does not empower you to enter onto land not in your ownership without the specific consent of the owner. If you require further information or advice, you should consult a solicitor.

5 113 **Asbestos Removal**

Demolition and removal of any material containing asbestos must be carried out in accordance with guidance from the Health and Safety Executive and the Council's Environmental Services. For advice and information contact: - Environmental Protection Unit, 3S/02, Civic Centre, High Street, Uxbridge, UB8 1UW (Tel. 01895 277401) or the Health and Safety Executive, Rose Court, 2 Southwark Bridge Road, London, SE1 9HS (Tel. 020 7556 2100).

6 115 **Control of Environmental Nuisance from Construction Work**

Nuisance from demolition and construction works is subject to control under The Control of Pollution Act 1974, the Clean Air Acts and other related legislation. In particular, you should ensure that the following are complied with:-

A. Demolition and construction works which are audible at the site boundary shall only be carried out between the hours of 08.00 and 18.00 hours Monday to Friday and between the hours of 08.00 hours and 13.00 hours on Saturday. No works shall be carried out on Sundays, Bank or Public Holidays.

B. All noise generated during such works shall be controlled in compliance with British Standard Code of Practice BS 5228:2009.

C. Dust emissions shall be controlled in compliance with the Mayor of London's Best Practice Guidance 'The Control of dust and emissions from construction and demolition.

D. No bonfires that create dark smoke or nuisance to local residents.

You are advised to consult the Council's Environmental Protection Unit (www.hillingdon.gov.uk/noise Tel. 01895 250155) or to seek prior approval under Section 61 of the Control of Pollution Act if you anticipate any difficulty in carrying out construction other than within the normal working hours set out in (A) above, and by means that would minimise disturbance to adjoining premises.

7

Network Rail advise that:-

Fencing

If not already in place, the developer/applicant must provide at their expense a suitable trespass

proof fence (of at least 1.8m in height) adjacent to Network Rail's boundary and make provision for

its future maintenance and renewal without encroachment upon Network Rail land.

Network Rail's

existing fencing / wall must not be removed or damaged and at no point either during construction

or after works are completed on site should the foundations of the fencing or wall or any embankment therein be damaged, undermined or compromised in any way. Any vegetation on Network Rail land and within Network Rail's boundary must also not be disturbed.

Drainage

Network Rail would require details of any additional drainage works (temporary or permanent) to ensure there will be no adverse effect upon Network Rail's earthworks.

Additional or increased flows of surface water should not be discharged onto Network Rail land or into Network Rail's culvert or drains. In the interest of the long-term stability of the railway, it is recommended that soakaways should not be constructed within 20 metres of Network Rail's boundary.

Safety

No work should be carried out on the development site that may endanger the safe operation of the railway or the stability of Network Rail's structures and adjoining land. In particular, the demolition of buildings or other structures must be carried out in accordance with an agreed method statement. Care must be taken to ensure that no debris or other materials can fall onto Network Rail land. In view of the close proximity of these proposed works to the railway boundary the developer must contact Richard Selwood at Network Rail on AssetProtectionWestern@networkrail.co.uk before works begin.

Ground Levels

The developers should be made aware that Network Rail needs to be consulted on any alterations to ground levels. No excavations should be carried out near railway embankments, retaining walls or bridges.

Access to Railway

All roads, paths or ways providing access to any part of the railway undertaker's land shall be kept open at all times during and after the development.

Site Layout

In order to avoid the need for access to their infrastructure, Network Rail recommend that all buildings should be situated at least 2 metres from their boundary fence, to allow construction and any future maintenance work to be carried out. You need to be satisfied that these works can take place from within your site. Where trees exist on Network Rail land the design of foundations close to the boundary must take into account the effects of root penetration in accordance with the Building Research Establishment's guidelines.

Piling

Where vibro-compaction/displacement piling plant is to be used in development, details of the use of such machinery and a method statement should be submitted for the approval of Network

Rail's Asset Protection Engineer prior to the commencement of works and the works shall only be carried out in accordance with the approved method statement.

Excavations/Earthworks

All excavations / earthworks carried out in the vicinity of Network Rail's property / structures must be designed and executed such that no interference with the integrity of that property / structure can occur. If temporary compounds are to be located adjacent to the operational railway, these should be included in a method statement for approval by Network Rail. Prior to commencement of works, full details of excavations and earthworks to be carried out near the railway undertaker's boundary fence should be submitted for approval of the Local Planning Authority acting in consultation with the railway undertaker and the works shall only be carried out in accordance with the approved details. Where development may affect the railway, consultation with the Asset Protection Engineer should be undertaken.

Environmental Issues

The design and siting of buildings should take into account the possible effects of noise and vibration and the generation of airborne dust resulting from the operation of the railway.

Landscaping

It is recommended no trees are planted closer than 1.5 times their mature height to the boundary fence. The developer should adhere to Network Rail's advice guide on acceptable tree/plant species. Any tree felling works where there is a risk of the trees or branches falling across the boundary fence will require railway supervision.

Plant, Scaffolding and Cranes

Any scaffold which is to be constructed adjacent to the railway must be erected in such a manner that at no time will any poles or cranes over-sail or fall onto the railway. All plant and scaffolding must be positioned, that in the event of failure, it will not fall on to Network Rail land.

3. CONSIDERATIONS

3.1 Site and Locality

The 0.27ha roughly rectangular site is located on the eastern side of Station Road within Yiewsley/West Drayton Town Centre, just to the south of the railway bridge where the Great Western Main Line crosses the road. The site comprises the detached two storey Blues Bar PH with a beer garden on its southern side at the front of the site and associated informal parking at the rear, accessed from Tachbrook Road/ Warwick Road on the eastern side of the site. On the northern side of the public house, the site provides a small retail unit/open yard area which is accessed via a gated entrance from Station Road and on the eastern side, the site comprises open yard areas which are in use for car parking and storage. There are a number of trees on site, mainly along its boundaries.

Station Road adjoins the site to the west which slopes down towards the north to pass under the railway bridge. Immediately adjoining the site to the north is the Grade II Listed Railway Arms PH which is elevated above the sunken level of Station Road and to the north and rear of this building is the raised embankment of the railway line. On the other side of the railway is the De Burgh Arms Hotel and Station Approach which provides access to the locally listed West Drayton Station. Adjoining the site at the rear is Tachbrook Road, on the opposite side of which is Kitchener House, a two storey commercial building and an attached large single storey garage/workshop which contains a number of commercial occupiers. Beyond this to the east and south east is a traditional housing area. Adjoining the site to the south at the rear of the site is a single storey office building, beyond which is terraced housing which fronts Warwick Road. To the south of the site on Station Road is a mix of two and three storey retail properties/parades with residential and commercial uses on the upper floors, which mainly characterise this part of the town centre.

The whole of the site is located within the Yiewsley/West Drayton Town Centre, with the front of the site forming part of the town centre's primary shopping frontage. The site is

also located within the Hayes/West Drayton Corridor. The site has a Public Transport Accessibility Level (PTAL) ranging between 2 on the eastern part of the site and 3 on the west, on a scale of 1 to 6 where 1 denotes the lowest level of accessibility and 6 the highest.

3.2 Proposed Scheme

This scheme involves the demolition of the existing public house and erection of two 'L'-shaped buildings sited around an internal courtyard comprising 14 x one-bedroom and 24 x two-bedroom flats and 237sqm of retail floorspace with associated parking, landscaping and amenity space.

The scheme has formed the subject of a pre-application enquiry and a previous application which was subsequently withdrawn and a number of revisions following the advice of officers as part of the consideration of the current proposal. This has included extending the red line boundary to include the adjoining unadopted Tachbrook Road and an appropriate notice has been placed in the local paper by the applicant.

Block A would front Station Road where the ground floor would provide 237sqm of retail space, returning along the side of the listed Railway Arms PH where the block would be set back from the road to accommodate a landscaped paved courtyard area. The remainder of the ground floor of Block A would provide parking and servicing areas, with the main pedestrian entrance being provided from the paved courtyard which also links through to the internal courtyard. The block would be predominantly three storeys and on the upper floors comprise a total of 10 flats (2 x one-bedroom and 3 x two-bedroom units on each of the first and second floors), and incorporate a cut away pitched roof which would conceal two roof gardens, accessed from small four storied lift and staircase towers within the internal corner of the block.

Block B would front Tachbrook Road where it would be three storeys, but return along the railway embankment within a four storey block. The three storey element would incorporate the same cut away pitched roof, concealing a roof top garden, whereas the four storey wing would have a more conventional crown roof, matching the slope and proportions of the rest of the roof. The main pedestrian entrance would be from Tachbrook Road, towards the southern end of the block with a new section of pavement provided to link Warwick Road with the lobby and bin storage areas. A separate footpath at the rear of the block would access the internal courtyard which would also directly link through to Station Road via the lobby area of Block A and the paved courtyard. A separate footpath would also run around the northern side of Block B, which would access the internal courtyard, but this would be gated from Tachbrook Road and is intended for emergency use. The block would comprise a total of 28 units, 6 units (1 x one-bedroom and 5 x two-bedroom units) together with servicing areas on the ground floor (of which 1 of the one-bedroom and 3 of the two-bedroom units would be fully wheelchair accessible), 9 units (3 x one-bedroom, 6 x two-bedroom units) on the first floor, 10 units (6 x one-bedroom and 4 x two-bedroom units) on the second floor and 3 units (all two-bedroomed) on the third floor.

Of the 38 flats, 25 (6 x one-bedroom and 19 x two bedroom units) would be for market housing with 13 (8 x one-bedroom and 5 x two bedroom) or 34% of the total being for social rental.

Residential parking for 28 cars, including 4 disabled person spaces would be provided within the internal courtyard and adjoining ground floor parking area within Block A which would be accessed through a gated archway entrance at the southern end of Block B

from Tachbrook Road/ Warwick Road. Two main cycle storage areas would be provided, with residential provision being made on the opposite sides of the vehicular entrance within Block B which would provide a total of 20 spaces and retail provision sited at the side of the returning wing of Block A adjacent to the Railway Arms PH which would provide 18 spaces. Servicing of the retail unit would take place from an enlarged servicing bay on Station Road. The internal courtyard also provides a play park area. Bin storage would be provided within each of the two blocks, adjacent to their main entrances.

The application is supported by the following documents (which have mainly been attached as appendices to the Planning, Design and Access Statement):-

Planning, Design and Access Statement:-

This provides an introduction to the statement and describes the site and surroundings. The planning history of the site is outlined, the proposed development described and relevant planning policy and legislation assessed. A planning analysis of the proposals is then provided and the design principles and concept discussed. Climate change mitigation and planning obligations are then briefly discussed before the report concludes that the scheme would satisfy a number of policy objectives and act as a catalyst for further regeneration to the south side of the railway line, which would add to the vitality of the centre. The layout, scale and massing of the development is appropriate to the local context and the opportunities arising through the emerging gateway associated with the Crossrail station at West Drayton.

Transport Statement:-

This provides an introduction to the study and assesses relevant national and local planning policy and guidance. The existing site is described, together with the local highway network and transport infrastructure. The assessment methodology is outlined, the proposed development described and likely impacts evaluated. The proposed parking provision and alternative sustainable transport options are discussed. The report then goes on to assess junction capacity and results are presented. A review of personal injury records is then undertaken. The report concludes by advising that the retail element would be unlikely to generate new vehicular trips due to its size and nature in its town centre surroundings where it would benefit from 'linked trips'. The junction capacity results show that committed development sites and traffic growth from 2013 to 2023 has the greatest bearing on increased traffic flows within the study area, and not the impact of the proposed development itself. The Station Road/ Station Approach mini-roundabout is the key junction in the vicinity and this junction is already likely to encounter capacity issues, prior to the introduction of committed development traffic and overall, the scheme would have a negligible impact on the adjacent highway network and no further assessment is required.

Arboricultural Report:-

This provides survey information and explains the tree classification used. It advises that all 12 trees on the site are self seeded and are growing either immediately adjoining existing buildings or within fence lines which should either be removed or are of low quality which do not merit retention.

Phase 1 (Desk Study) Investigation Report:-

This provides an introduction to the study, describes the site and its recorded history. The

geology, hydrogeology and hydrology of the site are briefly described and potential sources of pollution identified. A preliminary geotechnical assessment describes the construction techniques likely to be required and potential remediation measures. The study concludes by recommending that further intrusive ground investigation is carried out to assess on site ground conditions, chemical characteristics of the site, nature and strength of underlying strata, infiltration rates and potential gas emissions.

Sustainability, Energy and Environmental Report:-

This provides an introduction to the study and describes the proposed development. The report identifies and evaluates the various measures and available technologies that would assist in maximising the energy efficiency of the two blocks and conclusions are drawn and recommendations are made. The report then assesses the Code for Sustainable Homes and considers noise and vibration issues and results are presented and mitigation measures recommended.

Noise and Vibration Assessment:-

This provides an introduction to the study and a description of the site and the development proposals. Relevant acoustic criteria are discussed and the noise and vibration survey methodology outlined. Results of the noise and vibration surveys are presented, daytime and night-time noise levels determined and mitigation measures recommended.

Air Quality Assessment:-

This provides an introduction to the study and describes relevant legislation and policy. The study then establishes baseline conditions for air quality using Hillingdon's monitoring data and describes the study's methodology. Results are presented and the report concludes that air quality should not prevent planning permission from being granted.

Flood Risk Statement:-

This provides the background to the report and describes the site. The report identifies the site as being within Flood Zone 1 and assesses the source and extent of possible flood risk. Results are presented and storm water management and attenuation discussed. The report concludes by stating the development is suitable in this location which is free from any identified flood risk and flood risk therefore should not prevent planning permission from being granted, subject to suitable conditions to control the discussed proposed drainage strategy.

3.3 Relevant Planning History

Comment on Relevant Planning History

An application for a similar redevelopment scheme on this site (App. No. 65480/APP/2013/3034 refers) was withdrawn by the applicant on 20/1/14 to allow revisions to be made to the scheme in the light of various officer responses received.

A scheme for the re-development of Kitchener House on the opposite side of Tachbrook Road, approved on 10/12/13 is also relevant to the consideration of this application (App. No. 18218/APP/2013/2183 refers). This scheme is for a part single, two, three and four storey building with 250sqm of retail/commercial space and parking on the ground floor and 23 residential units (with the exception of a single bedroom and toilet of one of the flats on the corner of the ground floor) on the upper floors.

4. Planning Policies and Standards

UDP / LDF Designation and London Plan

The following UDP Policies are considered relevant to the application:-

Part 1 Policies:

PT1.E3	(2012) Strategy for Heathrow Opportunity Area
PT1.E5	(2012) Town and Local Centres
PT1.E7	(2012) Raising Skills
PT1.H1	(2012) Housing Growth
PT1.H2	(2012) Affordable Housing
PT1.HE1	(2012) Heritage
PT1.BE1	(2012) Built Environment
PT1.EM1	(2012) Climate Change Adaptation and Mitigation
PT1.EM5	(2012) Sport and Leisure
PT1.EM6	(2012) Flood Risk Management
PT1.EM8	(2012) Land, Water, Air and Noise
PT1.T1	(2012) Accessible Local Destinations
PT1.CI1	(2012) Community Infrastructure Provision
PT1.CI2	(2012) Leisure and Recreation

Part 2 Policies:

NPPF1	NPPF - Delivering sustainable development
NPPF2	NPPF - Ensuring the vitality of town centres
NPPF4	NPPF - Promoting sustainable transport
NPPF6	NPPF - Delivering a wide choice of high quality homes
NPPF7	NPPF - Requiring good design
NPPF10	NPPF - Meeting challenge of climate change flooding costal
NPPF12	NPPF - Conserving & enhancing the historic environment
LPP 2.15	(2011) Town Centres
LPP 3.1	(2011) Ensuring equal life chances for all
LPP 3.2	(2011) Improving health and addressing health inequalities
LPP 3.3	(2011) Increasing housing supply
LPP 3.4	(2011) Optimising housing potential
LPP 3.5	(2011) Quality and design of housing developments
LPP 3.6	(2011) Children and young people's play and informal recreation (strategies) facilities

LPP 3.8	(2011) Housing Choice
LPP 3.12	(2011) Negotiating affordable housing (in) on individual private residential and mixed-use schemes
LPP 3.13	(2011) Affordable housing thresholds
LPP 3.14	(2011) Existing Housing - Efficient use of stock
LPP 3.16	(2011) Protection and enhancement of social infrastructure
LPP 4.7	(2011) Retail and town centre development
LPP 4.8	(2011) Supporting a Successful and Diverse Retail Sector
LPP 5.2	(2011) Minimising Carbon Dioxide Emissions
LPP 5.3	(2011) Sustainable design and construction
LPP 5.6	(2011) Decentralised Energy in Development Proposals
LPP 5.7	(2011) Renewable energy
LPP 5.9	(2011) Overheating and cooling
LPP 5.10	(2011) Urban Greening
LPP 5.11	(2011) Green roofs and development site environs
LPP 5.12	(2011) Flood risk management
LPP 5.13	(2011) Sustainable drainage
LPP 5.15	(2011) Water use and supplies
LPP 5.21	(2011) Contaminated land
LPP 6.2	(2011) Providing public transport capacity and safeguarding land for transport
LPP 6.3	(2011) Assessing effects of development on transport capacity
LPP 6.5	(2011) Funding Crossrail and other strategically important transport infrastructure
LPP 6.9	(2011) Cycling
LPP 6.10	(2011) Walking
LPP 6.13	(2011) Parking
LPP 7.1	(2011) Building London's neighbourhoods and communities
LPP 7.2	(2011) An inclusive environment
LPP 7.3	(2011) Designing out crime
LPP 7.4	(2011) Local character
LPP 7.5	(2011) Public realm
LPP 7.6	(2011) Architecture
LPP 7.8	(2011) Heritage assets and archaeology
LPP 7.9	(2011) Heritage-led regeneration
LPP 7.14	(2011) Improving air quality
LPP 7.15	(2011) Reducing noise and enhancing soundscapes
LPP 7.19	(2011) Biodiversity and access to nature
LPP 7.21	(2011) Trees and woodland
LPP 8.2	(2011) Planning obligations
BE10	Proposals detrimental to the setting of a listed building
BE13	New development must harmonise with the existing street scene.

BE18	Design considerations - pedestrian security and safety
BE19	New development must improve or complement the character of the area.
BE20	Daylight and sunlight considerations.
BE21	Siting, bulk and proximity of new buildings/extensions.
BE22	Residential extensions/buildings of two or more storeys.
BE23	Requires the provision of adequate amenity space.
BE24	Requires new development to ensure adequate levels of privacy to neighbours.
BE26	Town centres - design, layout and landscaping of new buildings
BE38	Retention of topographical and landscape features and provision of new planting and landscaping in development proposals.
OE1	Protection of the character and amenities of surrounding properties and the local area
OE5	Siting of noise-sensitive developments
OE8	Development likely to result in increased flood risk due to additional surface water run-off - requirement for attenuation measures
H3	Loss and replacement of residential accommodation
H4	Mix of housing units
R2	Provision of recreation, entertainment and leisure facilities in Town Centres
R17	Use of planning obligations to supplement the provision of recreation, leisure and community facilities
AM2	Development proposals - assessment of traffic generation, impact on congestion and public transport availability and capacity
AM7	Consideration of traffic generated by proposed developments.
AM9	Provision of cycle routes, consideration of cyclists' needs in design of highway improvement schemes, provision of cycle parking facilities
AM14	New development and car parking standards.
AM15	Provision of reserved parking spaces for disabled persons
HDAS-LAY	Residential Layouts, Hillingdon Design & Access Statement, Supplementary Planning Document, adopted July 2006
LDF-AH	Accessible Hillingdon , Local Development Framework, Supplementary Planning Document, adopted January 2010
SPD-NO	Noise Supplementary Planning Document, adopted April 2006
SPG-AQ	Air Quality Supplementary Planning Guidance, adopted May 2002
SPG-CS	Community Safety by Design, Supplementary Planning Guidance, adopted July 2004
SPD-PO	Planning Obligations Supplementary Planning Document, adopted July 2008

5. Advertisement and Site Notice

5.1 Advertisement Expiry Date:- **23rd April 2014**

5.2 Site Notice Expiry Date:- Not applicable

6. Consultations

External Consultees

52 neighbouring properties have been consulted and re-consulted on revised plans, the application was advertised in the local press on 9/4/14 and 2 notices have been displayed on site on 9/4/14. No responses have been received.

A former ward councillor:

I am against this application because the density will create significant problems in an already overcrowded area of the ward. There will be additional traffic movements which will further deteriorate the air quality, parking requirements whilst may be in accordance with policy will never be sufficient. There will be additional retail which based on previous development in Yiewsley will remain vacant and will do nothing to stimulate confidence and thus trade in/for the local economy.

ENGLISH HERITAGE:

We do not wish to offer any comments on this occasion.

Recommendation

This application should be determined in accordance with national and local policy Guidance, and on the basis of your specialist conservation advice.

ENVIRONMENT AGENCY:

You should be using our Flood Risk Standing Advice (FRSA) to determine if we need to be consulted directly on an application regarding flood risk. This site is in Flood Zone 1 and is under a hectare. Therefore cell F5 of the consultation matrix applies and you did not need to consult us.

The main flood risk issue at this site is the management of surface water run-off and ensuring that drainage from the development does not increase flood risk either on-site or elsewhere.

We recommend the surface water management good practice advice in cell F5 is used to ensure sustainable surface water management is achieved as part of the development.

Surface water runoff rates and volumes from the site must be managed in accordance with the London Plan (July 2011) - which sets higher standards than the NPPF for the control of surface water run-off. Policy 5.13 - Sustainable drainage (page 155) of the London Plan states that "development should utilise sustainable urban drainage systems (SUDS) unless there are practical reasons for not doing so, and should aim to achieve greenfield run-off rates and ensure that surface water run-off is managed as close to its source as possible" in line with the drainage hierarchy.

If you have identified drainage problems at this site through your Strategic Flood Risk Assessment or Surface Water Management Plan, you may want to request a formal Flood Risk Assessment from the applicant in line with Flood Risk Assessment Guidance Note 1.

NETWORK RAIL:

Whilst there is no objection in principle to this proposal I give below my comments and requirements for the safe operation of the railway and the protection of Network Rail's adjoining land.

Fencing

If not already in place, the Developer/applicant must provide at their expense a suitable trespass proof fence (of at least 1.8m in height) adjacent to Network Rail's boundary and make provision for

its future maintenance and renewal without encroachment upon Network Rail land. Network Rail's existing fencing / wall must not be removed or damaged and at no point either during construction or after works are completed on site should the foundations of the fencing or wall or any embankment therein be damaged, undermined or compromised in any way. Any vegetation on Network Rail land and within Network Rail's boundary must also not be disturbed.

Drainage

It is noted in the Flood Risk Statement states that existing drainage to be utilised. Network Rail would require details of any additional drainage (temporary or permanent) to ensure there will be no adverse effect of Network Rail's earthworks. Additional or increased flows of surface water should not be discharged onto Network Rail land or into Network Rail's culvert or drains. In the interest of the long-term stability of the railway, it is recommended that soakaways should not be constructed within 20 metres of Network Rail's boundary.

Safety

No work should be carried out on the development site that may endanger the safe operation of the railway or the stability of Network Rail's structures and adjoining land. In particular, the demolition of buildings or other structures must be carried out in accordance with an agreed method statement. Care must be taken to ensure that no debris or other materials can fall onto Network Rail land. In view of the close proximity of these proposed works to the railway boundary the developer must contact Richard Selwood at Network Rail on AssetProtectionWestern@networkrail.co.uk before works begin.

Ground Levels

The developers should be made aware that Network Rail needs to be consulted on any alterations to ground levels. No excavations should be carried out near railway embankments, retaining walls or bridges.

Access to Railway

All roads, paths or ways providing access to any part of the railway undertaker's land shall be kept open at all times during and after the development.

Site Layout

It is recommended that all buildings be situated at least 2 metres from the boundary fence, to allow construction and any future maintenance work to be carried out without involving entry onto Network Rail's infrastructure. Where trees exist on Network Rail land the design of foundations close to the boundary must take into account the effects of root penetration in accordance with the Building Research Establishment's guidelines.

Piling

Where vibro-compaction/displacement piling plant is to be used in development, details of the use of such machinery and a method statement should be submitted for the approval of Network Rail's Asset Protection Engineer prior to the commencement of works and the works shall only be carried out in accordance with the approved method statement.

Excavations/Earthworks

All excavations / earthworks carried out in the vicinity of Network Rail's property / structures must be designed and executed such that no interference with the integrity of that property / structure can occur. If temporary compounds are to be located adjacent to the operational railway, these should be included in a method statement for approval by Network Rail. Prior to commencement of works, full details of excavations and earthworks to be carried out near the railway undertaker's boundary fence should be submitted for approval of the Local Planning Authority acting in consultation with the railway undertaker and the works shall only be carried out in accordance with the approved details. Where development may affect the railway, consultation with the Asset Protection

Engineer should be undertaken.

Environmental Issues

The design and siting of buildings should take into account the possible effects of noise and vibration and the generation of airborne dust resulting from the operation of the railway.

Landscaping

It is recommended no trees are planted closer than 1.5 times their mature height to the boundary fence. The developer should adhere to Network Rail's advice guide on acceptable tree/plant species. Any tree felling works where there is a risk of the trees or branches falling across the boundary fence will require railway supervision.

Plant, Scaffolding and Cranes

Any scaffold which is to be constructed adjacent to the railway must be erected in such a manner that at no time will any poles or cranes over-sail or fall onto the railway. All plant and scaffolding must be positioned, that in the event of failure, it will not fall on to Network Rail land.

Officer's comments:-

Network Rail's comments have been included within an informative. As regards their comments as regards site layout and that buildings should be sited at least 2m from their boundary, the nearest part of Block B would be sited some 0.9m from the boundary with the railway. However, Network Rail confirmed that they do not object to the proposals and the informative advises the applicant that they should be satisfied that they are able to carry out construction and future maintenance work within the confines of their site.

CROSSRAIL:

Thank you for your letter dated 30 October 2013 and the accompanying planning application documents relating to the above site, requesting the views of Crossrail Limited on the above application.

Crossrail is a proposed new railway that will link Heathrow and Maidenhead in the west to Shenfield and Abbey Wood in the east using existing Network Rail tracks and new tunnels under Central London.

The Crossrail Bill which was introduced into Parliament by the Secretary of State for Transport in February 2005 was enacted as the Crossrail Act on the 22nd July 2008. The first stage of Crossrail preparatory construction works began in early 2009. Main construction works have started with works to the central tunnel section to finish in 2018, to be followed by a phased opening of services.

Crossrail Limited administers a Direction issued by the Department for Transport on 24th January 2008 for the safeguarding of the proposed alignment of Crossrail.

The site of this planning application is identified outside the limits of land subject to consultation under the Safeguarding Direction.

The implications of the Crossrail proposals for the application have been considered and the detailed design of the proposed development needs to take account of the construction of Crossrail.

Therefore if, as the Local Planning Authority, you are minded to grant planning permission for the development, Crossrail Limited are of the view that the following condition should be applied: (in

view of the proximity of the site to our works).

Crossrail condition - concurrent working

None of the development hereby permitted shall be commenced until a method statement has been submitted to, and approved in writing, by the Local Planning Authority to include arrangements to secure that, during any period when concurrent construction is taking place of both the permitted development and of the Crossrail structures and tunnels in or adjacent to the site of the approved development, the construction of the Crossrail structures and tunnels is not impeded.

YIEWSLEY AND WEST DRAYTON TOWN CENTRE ACTION GROUP:

Comments on Revised Plans:

Amended plans have been noted.

However, they do not offer anything significant to change the comments submitted in the withdrawn application and the re-submission.

This development will have an adverse effect on the local environs and is an over development of the site.

Initial comments:

Following the withdrawal of the previous application, it has been noted that these proposed plans do now give some indication as to how the development will look in relation to the Grade II listed building and Station Road.

However, the current application does not really address the points raised in the previous application.

Previous comments (on App. No. 65480/APP/2013/3034):-

The redevelopment of the site situated next door to the Blues Bar is long overdue. However, the proposed demolition of the public house is not welcome.

The historical context in para 2.3 on page 4 of the Planning Statement is incorrect. There is both written and photograph evidence that The Engine PH (renamed Blues Bar 5 to 10 years ago) has been in situ since the mid/ late Victorian period (not the 1960s as stated) and was completely separate from the People's Cinema and the later garage.

It is difficult to assess from line drawings (no artist impression/graphics) the impact of the proposed West elevation on the Grade II listed building (The Railway Arms) particularly when it is not included on the plans - drawing 105. From studying the other various aspects and elevations it is clear that both Block A and Block B will have a detrimental effect both on the setting of the Railway Arms (Grade II) and on the street scene in Station Road/Tachbrook Road/Warwick Road.

The proposed 4 storey building (Block B) will, in itself, have a major impact on the houses in Warwick Road.

The design of Block A has no architectural value as it mimics the design of the existing buildings to the South of the Blues Bar, which in themselves are of a nondescript style and value. The proposals for Block A have no merit and do nothing to enhance the street scene. It is clear from the

drawings that the 3 storey elevation will in fact dominate this particular area of the street scene.

The 4 storey tenement design of Block B also has no architectural value and its dominance will be overbearing on all fronts.

There is concern over the impact of vehicular access onto Tachbrook Road; Warwick Road and the junction with Station Road.

The Planning Statement includes, presumably in justification of these proposals, photographs of modern developments in Yiewsley High Street. They have no bearing whatsoever in relation to Station Road.

The proposals clearly show that this will be an over development of the site and will have a detrimental effect on the local environs.

Internal Consultees

CONSERVATION/URBAN DESIGN OFFICER:

Proposal: Demolition of existing public house and erection of 38 flats and 237sqm of retail floorspace with parking, landscaping and amenity space

Background: This is a tight site adjacent to the statutory designated (Grade II listed) Railway Arms on West Drayton's principal shopping street. It extends to the rear to a narrow street with warehousing and small scale domestic housing. To one side, it is bounded by the railway tracks. Its current appearance is semi-derelict and the site is suitable for redevelopment and proper enhancement.

Comments: The scheme proposes two 'L' blocks (A & B) which are generally three storeys in height (plus roof). The layout provides for a courtyard arrangement as well as siting away from the heritage asset. The scale of the blocks has also been considered; three storey to Station Road and Tachbrook elevations, but higher to the railway cutting, reflecting the urban street scale.

The form and massing of the blocks are fairly extensive, but the applicant has provided a design which is traditional in appearance, and is convincing and well-articulated. It seeks to reinforce local distinctiveness which is characterised by inter-War development and is vaguely in the ' Tudorbethan' style. It therefore addresses the connections, by integrating the new development into existing built and historic environment.

The elevation to Station Road has a more commercial appearance/character with the upper levels more domestic in appearance. The open metal balconies pick up on the design from the public house. The roofscape has deep eaves with projecting half timbered gabled elements a feature. This is carried through to the courtyard where there are more projecting gables (with glazed balconies) at regular intervals. To the rear, more gables. The north elevation features a tower element decorated with diamond-shaped brick panels.

Inspiration has been taken from particular local materials, building forms and features within this locality of West Drayton. The massing is broken down by projecting vertical elements giving depth to the elevations and visual interest. The design should help raise the standard of design more generally in the area.

Servicing of the residential accommodation is from the courtyard and the proposed layout for vehicles and pedestrians appears rational and logical. The layout will also provide a buffer to the road, and sustain the appearance of the area.

The facades will mainly be finished in a combination of London Stock brick walls, sections of render and timber cladding and slate roof tiles. Solid timber doors, brick arches, stone cills are also a feature.

Unfortunately, there is limited scope for soft landscaping, with amenity space located to the roof. However, the curtilages and boundaries are well defined with soft landscaping where possible. More detail over materials and hard landscaping will be required.

This is a major thoroughfare. A good design and finish is a key aspect of sustainable development. The new building and final elevations contribute positively to streetscene. As such, the proposal would generally adhere to the advice given in the Hillingdon Design and Accessibility Statement (HDAS) Supplementary Planning Document - Residential Layouts, in particular, Section 1.0, which states 'Above all, development should seek to enhance and contribute positively to the appearance of an area.'

Condition all sample materials.

Conclusion: Acceptable.

HIGHWAY ENGINEER:

The development is for the demolition of an existing public house and the construction 38, 1 and 2 bedroom apartments and a single retail unit (237m²) within the existing site. As part of the proposals, 36 car and 38 cycle parking spaces will be provided to serve the proposed residential use. There are no proposals to provide car parking for the retail use, however, 12 cycle parking spaces will be provided for the use of staff and customers.

Vehicle and pedestrian access to the proposed residential use will be provided from Tachbrook Road with dedicated pedestrian link provided from Station Road. Servicing of the proposed apartments will be undertaken along Warwick Road and for the retail use, from Station Road via an existing lay-by, which will be extended.

When reviewing the Transport Assessment (TA) submitted in support of the proposals, an assessment of the existing and proposed trip generation at the site has been undertaken using the TRICS Database. However, it is noted that the number of selected sample sites are limited and not all sites are representative. Nevertheless, it is considered that any increase in vehicle trips as a result of the development, would not have a material impact along the adjacent highway network.

From assessment of the PTAL index within the adjacent area, this has been identified as 3, which is classified as moderate. Furthermore, it is noted that the site is located adjacent to all public transport facilities including West Drayton Rail Station, which will be upgraded as part of the Cross Rail development. As a result, it is considered that the proposed car parking provision is acceptable to serve the proposals.

In order to assess the proposed car parking layout, a swept path analyses has been undertaken within the TA. This has identified that some parking spaces within the site would be difficult to access for a larger car and would require excessive manoeuvring. While it is considered that these spaces can be accessed by a medium sized car, it is preferable that a car stackers are provided within the site, which should be covered by a suitably worded planning condition.

Therefore, subject to the details below being provided under a suitably worded planning condition/S106 Agreement, it is considered that the development would not be contrary to the policies of the adopted Hillingdon Local Plan, 2012, (Part 2).

Conditions/S106.

The parking provision within the site shall include 20% active and 20% passive electric charging points, which shall be provided before first occupation of the development.

The development shall not be occupied until details of the proposed cycle parking facilities have been submitted to and approved in writing by the LPA. Thereafter, the proposed cycle parking shall be provided before occupation and maintained and retained at all times for the use of the development.

The development shall not be occupied until the existing loading bay adjacent to the site along Station Road has been reconfigured and extended in accordance with the details first to be submitted and agreed in writing by the LPA.

The development shall not be occupied until the existing vehicle crossover providing access to the site from Station Road, is reinstated as pedestrian footway.

The development shall not be occupied until a Delivery and Servicing Management Plan has been submitted and approved in writing by the LPA.

Prior to the commencement of the development, a Construction Management plan is required to be submitted and approved in writing by the LPA prior to commencement of any works at the site. The Construction Management plan shall provide the details in relation to access (vehicular and pedestrian) and the parking provision for contracting staff and the delivery of materials' during construction.

TREE /LANDSCAPE OFFICER:

Landscape Character / Context:

The 0.27 hectare site is occupied by a pub fronting onto Station Road within the West Drayton's town centre. To the north, south and east the building is surrounded by open hard-standing which is currently used for parking and open storage. To the north lies the listed building, The Railway Arms, and the wooded embankment of a railway lines. The mixed uses of retail/commercial high street buildings with residential accommodation above lie to the south. The rear of the site (east) can be accessed via a side street off Warwick Road.

There is a line of self-set trees (Sycamores) running through the site which currently provide some visual relief from the semi-derelict, poor quality environment. These trees are not protected by TPO or Conservation Area designation.

Proposal:

The proposal is to demolish the existing public house and erect 38 flats and 237m² of retail floorspace with parking, landscaping and amenity space. This application follows a previous submission (ref. 2013/3034) which was withdrawn.

Landscape Considerations:

Saved policy BE38 seeks the retention and utilisation of topographical and landscape features of merit and the provision of new planting and landscaping wherever it is appropriate.

- A topographic survey by Midland Survey, drawing No.12195/1, confirms the location and approximate spread of the existing trees, which are to be found around the site boundaries
- The trees have been assessed in an Arboricultural Report by Floyd Matcham. The report confirms that the 12No. trees surveyed are self sets (ash and sycamore) which have been permitted to grow hard against buildings and fences. Many of the trees are multi-stemmed and some have been damaged.

- Of the twelve trees, 2No. are 'R' graded ('U' according to the current British Standard) and the remaining trees are 'C' - poor quality trees which are not considered a constraint on development. All will be removed to facilitate the development.
- These trees have a limited useful life expectancy amenity value and their loss is justified, provided that suitable replacement planting is secured as part of the new development.
- The report confirms that 'replacement trees in long term sustainable positions are an important part of the landscape proposals for the site'.
- The Design & Access Statement includes reference to the landscape enhancement objectives in sections 5.3, 5.6, 5.8, 5.12 and 5.13.
- Wythe Holland drawing No. 1720_101 Rev E Ground Floor Plan, indicates a building layout which wraps around the north, east and west boundaries, with parking spaces and limited amenity space in a central courtyard which extend to the southern boundary. This masterplan indicates areas of soft landscape within the proposed layout.
- New planting will feature in the open courtyard to the side of the retail unit fronting onto Station Road. This open space with tree planting has the potential to make a significant and welcome contribution to the west Drayton public realm, as illustrated on sketch ref. 1720-110.
- Within the site, at ground level, areas of soft landscape are indicated around the edges of the building and, notably, associated with the 'play park' within the central courtyard. Indicative greening of this space is shown on sketch ref. 1720-113
- Additional private / communal space and environmental enhancement is to be provided in the form of intensive green roofs, which will create additional accessible amenity space for residents in the form of roof gardens, as illustrated on sketch ref. 1720-112.
- If the application is recommended for approval, landscape conditions should be imposed to ensure that the proposals preserve and enhance the character and appearance of the area in accordance with the submitted plans and illustrations.

Recommendations:

This application addresses the landscape issues discussed at a post-application meeting on 22 January 2014, in response to the 2013/3034 submission.

No objection subject to conditions RES6, RES7, RES9 (parts 1, 2, 3, 4, 5 and 6) and RES11.

Comments on amendments received 2/7/14:

Since the attached comments were made, revised drawing were submitted in early July to address a shortfall in car parking spaces. The affect of this is the removal of the childrens' play area from the central courtyard. A new play area and soft landscape has been proposed at the north end of Tachbrook Road, a cul de sac, bounded to the north by the railway embankment.

Recommendations:

No objection subject to the previous landscape conditions and:

- Specifically, provision should be made for the planting of a suitable specimen tree (20-25cm girth, minimum) within the centre of the courtyard. The tree should be selected to create a focal point and reduce the detrimental impact of the car park on this residential scheme.
- The space for the childrens' play area is potentially an improvement on the original location. The opportunity should be taken to screen the area with a high quality landscape scheme for children and their carers, with soft landscape screening (trees and hedges) particularly at the southern end.

ACCESS OFFICER:-

The site has a total area of 0.27 hectares and comprises a rectangular area of land, located on the eastern side of Station Road within Yiewsley/West Drayton town centre.

The proposed residential flats are to be located in one block of 10 flats situated over and attached to the retail unit fronting Station Road, Block A, in addition, there is to be one block of 28 flats fronting Tachbrook Road, Block B. There are to be a mixture of 1 and 2 bedroom flats in each

block.

In assessing this application, reference has been made to London Plan July 2011, Policy 3.8 (Housing Choice) and the Council's Supplementary Planning Document "Accessible Hillingdon" adopted May 2013.

Lifts and stairs are to be provided giving access to the upper floors. It is stated within the Design & Access Statement that four wheelchair accessible units have been incorporated into the design; these are confirmed on plan. 28 car parking spaces are to be provided of which four would be accessible. The retail area would not provide car parking facilities.

The remaining flats demonstrate compliance with the requisite Lifetime Homes Standards for planning purposes.

Conclusion: No objection - Level access should be provided which is covered by the Building Regulations.

ENVIRONMENTAL PROTECTION OFFICER (NOISE):

I have considered the Noise and Vibration Assessment report 31429 prepared by Hepworth Acoustics. The report makes recommendations for the acoustic specification of double glazing in the residential properties in order to provide satisfactory internal noise levels. I agree that the recommendations on double glazing acoustic specification together with acoustically treated ventilation system contained on the Hepworth Acoustics report are satisfactory and, when implemented and maintained, will provide suitable internal noise climate.

In order to ensure that the recommendations of the report are complied with, I suggest informing the applicant as follows:

It is important that the double glazing /ventilator acoustic specification recommendations in Noise and Vibration Assessment report 31429 are implemented in full, including the recommendations regarding balconies in section 5.8 of the report. Failure to comply with the recommendations could result in a requirement for expensive remedial works.

I also agree the measured vibration levels are within the guidelines provided in BS6472:2008 and no additional mitigation measures will be required.

The following two conditions are recommended for fixed mechanical plants and separation between commercial/residential premises respectively:

Noise affecting residential property

The rating level of noise emitted from the plant and/or machinery hereby approved shall be at least 5 dB below the existing background noise level. The noise levels shall be determined at the nearest residential property. The measurements and assessment shall be made in accordance with British Standard 4142 "Method for rating industrial noise affecting mixed residential and industrial areas".

Reason:

To safeguard the amenity of the surrounding area in accordance with policy OE1 of the Hillingdon Unitary Development Plan.

Sound insulation of commercial premises

The development shall not begin until a sound insulation scheme for the control of noise

transmission to the adjoining [dwellings] [premises] has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include such combination of sound insulation and other measures as may be approved by the LPA. Thereafter, the scheme shall be implemented and maintained in full compliance with the approved measures.

Reason:

To safeguard the amenity of the occupants of surrounding properties in accordance with policy OE1 of the Hillingdon Unitary Development Plan.

I also recommend the following informative:

Control of environmental nuisance from construction work

Nuisance from demolition and construction work is subject to control under the Control of Pollution Act 1974, the Clean Air Act 1993 and the Environmental Protection Act 1990. You should ensure that the following are complied with:

- (i) Demolition and construction works should only be carried out between the hours of 0800 and 1800 on Monday to Friday and between the hours of 0800 and 1300 on Saturday. No works should be carried out on Sundays, Public or Bank Holidays;
- (ii) All noise generated during such works should be controlled in compliance with British Standard 5228, and use "best practicable means" as defined in section 72 of the Control of Pollution Act 1974;
- (iii) Measures should be taken to eliminate the release of dust, odours and other emissions caused by the works that may create a public health nuisance. Guidance on control measures is given in "The control of dust and emissions from construction and demolition: best practice guidelines", Greater London Authority, November 2006; and
- (iv) No bonfires that create dark smoke or cause nuisance to local residents should be allowed at any time.

You are advised to consult the Council's Environmental Protection Unit to seek prior approval under Section 61 of the Control of Pollution Act 1974 if you anticipate any difficulty in carrying out the works other than within the normal working hours set out above, and by means that would minimise disturbance to adjoining premises. For further information and advice, contact the Environmental Protection Unit, 3S/02 Civic Centre, High Street, Uxbridge, Middlesex UB8 1UW (tel. 01895 250155).

ENVIRONMENTAL PROTECTION OFFICER (AIR QUALITY):

The following information was submitted with regard to air quality:

- Air Quality Assessment Station Road, West Drayton by REC Ltd for Caldecotte Consultants, dated 10 May 2013

The following information was submitted with regard to land contamination:

- Phase 1 (Desk Study) Investigation Report on a Site at Blues Bar Public House, West Drayton, by Opus International Consultants (UK) Ltd for Caldecotte Consultants, dated 18 June 2013

Please consider the following comments with regard to air quality and land contamination. It is recommended the following conditions are included in any permission that may be given.

Air Quality

The proposed development is within the declared AQMA and in an area which currently appears to

be exceeding the EU limit value based on CERC modelling for 2011 carried out on behalf of the London Borough of Hillingdon. The emission sources in the modelling appears to be from the main road and the railway, which appears to be contributing significantly to the modelling output, which is indicated to be slightly above 45.0 mg/m³ across the application site.

The nearest NO_x tubes in the area are located at HD51 (located to the north east on Colham Road a little over 300 metres from the site on a residential road), HD67 (located to the north west on Tavistock Road a little over 400 metres from the site on a residential road), and HD204 (located to the north of the site, just over 450 metres away on Fairfield Road) indicated a bias adjusted NO₂ level of 36.2, 29.2 mg/m³ and 38.7 mg/m³ respectively for 2012. The NO₂ levels at HD51 is slightly higher than 2011, and for HD67, it was slightly lower. HD204 is a new tube located approximately 37 metres from the High Street on Fairfield Road. (Only 6 months of data was collected in 2012, therefore the tube results had to be annualised as well as bias adjusted. However, full year of monitoring for 2013 at HD204 is likely to be consistent with the previous year's results.) Based on this monitoring location it is considered possible the NO₂ annual mean may be exceeding 40 mg/m³ at the facade of the proposed building at the ground floor level, as it is located almost immediately adjacent to the High Street. It is possible it may be close to but below the EU limit value at the first floor level, however, it is unclear how the building would impact on the dispersion of NO_x from the road or railway. We would discourage balconies or residential rooms on the High Street facade of the development as a precaution.

The air quality assessment considers the impact on the receptors to be introduced to the site at the ground floor and first floor level, however the impact from the development has been scoped out as only 28 parking spaces were indicated. The air quality assessment did not have trip generation information as the Transport Assessment does not appear to have been available until August 2013. Clarification is required with regard to the AADT data used in the air quality assessment, as the Transport Assessment has been updated again (dated March 2014). It is not clear from the information that the cumulative impacts from other nearby developments were considered within the air quality assessment. Although the air quality assessment indicates the use of data from 2011 to be more conservative and 'robust', it appears it still underestimates NO₂ annual mean in the vicinity of the development site which is indicated to be 34.1 to 37.4 at ground floor level and 34.1 to 36.1 at the first floor level.

As the development is in an area already suffering poor air quality and is likely to make the air quality worse the following are requested:

Section 106

Section 106 obligation for a £12,500 should be sought for contribution to the air quality monitoring network in the area.

There is a likelihood the EU limit value would be exceeded at the facade of the building on the ground floor, although it is less clear if this would be the case on the first floor. The facade of the building is not far removed from the edge of the road. It is also unclear how the building would impact on the dispersion of NO_x from the road and the railway (more likely to have stationary diesel trains due to proximity to the station). The ingress of polluted air condition is recommended for the block facing the High Street and should also be considered with regard to any exhaust flue(s) at the site and in relation to car park emissions. Natural ventilation indicated in the Sustainable Design and Construction Report - Energy and Environmental Aspects, and includes trickle ventilation. However this may not be sufficient, especially for the residential units nearest and facing the High Street. Therefore the ingress of polluted air condition is still recommended for the application.

Air Quality Condition 1: Ingress of Polluted Air

Before the development is commenced a scheme for protecting the proposed accommodation from external air pollution, and pollution emissions shall be submitted and approved by the LPA. Any works which form part of such a scheme shall be completed before any part of the development is first occupied or used and measures put in place to ensure it is maintained for the life of the development.

Reason: To safeguard residential amenity in accordance with policy OE1 of the Hillingdon Unitary Development Plan.

Notes: In areas where there the air pollution levels are above, or close to, the national and European limits, this is designed to safeguard the future residents/users of the site from the ingress of the poor outdoor air quality. The design must take into account climate change pollutants and ensure there are no trade-offs between local and global pollutant emissions. Suitable ventilation systems will need to: take air from a clean location or treat the air and remove pollutants; designed to minimise energy usage; be sufficient to prevent summer overheating; have robust arrangements for maintenance.

(Officer comment:- Air quality is covered by Part F of the Building Regulations and there is no reason why a separate planning condition is required to deal with this point.)

Based on the Sustainable Design and Construction Report - Energy and Environmental Aspects - the use of a gas fired CHP is no longer proposed due to insufficient space and choice issues, and biomass has been excluded on air quality grounds. The Code for Sustainable Homes Pre-Assessment Estimator Tool indicated boilers that emit less than 40mg/kWh of NOx is assumed to be used to pick up three points, and this is also referred to in the Sustainable Design document. No details of the unit or the flue details have been provided, although broad specifications provided in relation to CO2. The following condition is recommended with regard to energy generation on site. We would recommend using ultra low NOx boilers, to limit emissions. If it is clear low NOx boilers will be used at the site, the condition below will not be necessary.

Air Quality Condition 2 - Details of Energy Provision

Before the development is commenced, details of any plant, machinery or fuel burnt, as part of the energy provision for the development shall be submitted to the LPA for approval. This shall include pollutant emission rates with or without mitigation technologies. Where a scheme to mitigate emissions is required, this shall be submitted to the LPA for approval. The said scheme should be implemented, and maintained for the life of the development.

REASON: To safeguard the amenity of neighbouring properties in accordance with policy OE1 of the Hillingdon Unitary Development Plan.

Notes: This condition relates to the operational phase of residential or commercial development and is intended for the protection of future residents in a designated AQMA and Smoke Control Area. Advice on the types of authorised fuels and appliances can be found at www.defra.gov.uk.

Land Contamination

The desk study is quite limited and has not identified any specific contamination issues, although there was a reference to possible asbestos containing materials in the building and made ground on site and contamination associated with fuel spills and leaks due to historical garage on site. The proposed development is such that direct contact pathways are largely minimised, although there does appear to be amenity space at ground floor level in the middle of the site based on one drawing. Suitable assessment of gas and vapour contamination is required as well as any contamination that could affect building materials and water pipes etc. The application also seems

to have areas of green landscaping at and above ground floor level, therefore please ensure any imported soils are tested for contamination. The standard contaminated land condition and imported soils condition are recommended.

RES26 - Contaminated Land

(i) The development hereby permitted shall not commence until a scheme to deal with contamination has been submitted in accordance with the Supplementary Planning Guidance Document on Land Contamination and approved by the Local Planning Authority (LPA). The scheme shall include all of the following measures unless the LPA dispenses with any such requirement specifically and in writing:

(a) A desk-top study carried out by a competent person to characterise the site and provide information on the history of the site/surrounding area and to identify and evaluate all potential sources of contamination and impacts on land and water and all other identified receptors relevant to the site;

(b) A site investigation, including where relevant soil, soil gas, surface and groundwater sampling, together with the results of analysis and risk assessment shall be carried out by a suitably qualified and accredited consultant/contractor. The report should also clearly identify all risks, limitations and recommendations for remedial measures to make the site suitable for the proposed use; and

(c) A written method statement providing details of the remediation scheme and how the completion of the remedial works will be verified shall be agreed in writing with the LPA prior to commencement, along with details of a watching brief to address undiscovered contamination.

(ii) If during development works contamination not addressed in the submitted remediation scheme is identified, the updated watching brief shall be submitted and an addendum to the remediation scheme shall be agreed with the LPA prior to implementation; and

(iii) All works which form part of the remediation scheme shall be completed and a comprehensive verification report shall be submitted to the Council's Environmental Protection Unit before any part of the development is occupied or brought into use unless the LPA dispenses with any such requirement specifically and in writing.

REASON: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems and the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors in accordance with policy OE11 of the Hillingdon Unitary Development Plan Saved Policies (September 2007).

Imported Materials Condition

No contaminated soils or other materials shall be imported to the site. All imported soils for landscaping purposes shall be clean and free of contamination. Before any part of the development is occupied, all imported soils, or reused onsite topsoils shall be independently tested for chemical contamination, and the results of this testing shall be submitted and approved in writing by the Local Planning Authority. All soils used for gardens and/or landscaping purposes shall be clean and free of contamination.

Note: The Environmental Protection Unit (EPU) must be consulted for their advice when using this condition.

REASON: To ensure that the occupants of the development are not subject to any risks from soil contamination in accordance with policy OE11 of the Hillingdon Unitary Development Plan Saved Policies (September 2007).

SUSTAINABILITY OFFICER:-

I have no objections to the proposed development. However, the energy report is dated last year and refers to an out of date London Plan Policy 5.2. The following following condition is therefore necessary:

Condition

Prior to the commencement of development a detailed energy assessment shall be submitted showing how the development will reduce carbon emissions by 40% from a 2010 Building Regulations compliant development. The assessment shall clearly show:

- 1) the baseline energy demand (kwhr and kgCO₂) for each element of the regulated energy use (e.g. space heating, hot water and electricity) for all the relevant uses (e.g. residential, commercial etc).
- 2) the methods to improve the energy efficiency of the development and how this impacts on the baseline emissions and where they will be included within the development.
- 3) full details, specification and location of low carbon and renewable energy technology and the impacts they have on the baseline
- 4) how the technology will be maintained and managed throughout the lifetime of the development.

The development must proceed in accordance with the approved details.

REASON

To ensure appropriate carbon savings are delivered in accordance with London Plan Policy 5.2.

FLOOD AND WATER MANAGEMENT OFFICER:

The FRA is acceptable and I would just like the following condition applied.

Sustainable Water Management

Prior to commencement, a scheme for the provision of sustainable water management shall be submitted to, and approved in writing by the Local Planning Authority. The scheme shall clearly demonstrate how it (follows the strategy set out in Flood Risk Assessment and Surface Water Drainage Strategy, produced by POUS dated October 2013 Ref D1132 R03 incorporates sustainable urban drainage in accordance with the hierarchy set out in Policy 5.15 of the London Plan and will:

- i. provide information on all Suds features including the method employed to delay and control the surface water discharged from the site and:
 - a. calculations showing storm period and intensity and volume of storage required to control surface water and size of features to control that volume.
 - b. any overland flooding should be shown, with flow paths depths and velocities identified as well as any hazards, (safe access and egress must be demonstrated).
 - c. measures taken to prevent pollution of the receiving groundwater and/or surface waters;
 - d. how they or temporary measures will be implemented to ensure no increase in flood risk from commencement of construction.
- ii. provide a management and maintenance plan for the lifetime of the development of arrangements to secure the operation of the scheme throughout its lifetime. Including appropriate details of Inspection regimes, appropriate performance specification, remediation and timescales for the resolving of issues.
- iii. provide details of the body legally responsible for the implementation of the management and

maintenance plan.

The scheme shall also demonstrate the use of methods to minimise the use of potable water through water collection, reuse and recycling and will:

- iii. incorporate water saving measures and equipment.
- iv. provide details of water collection facilities to capture excess rainwater;
- v. provide details of how rain and grey water will be recycled and reused in the development.

Thereafter the development shall be implemented and retained/maintained in accordance with these details for as long as the development remains in existence.

REASON

To ensure that surface water run off is controlled to ensure the development does not increase the risk of flooding contrary to Policy EM6 Flood Risk Management in Hillingdon Local Plan: Part 1- Strategic Policies (Nov 2012) Policy 5.12 Flood Risk Management of the London Plan (July 2011) and National Planning Policy Framework (March 2012) and the Planning Practice Guidance (March 2014). To be handled as close to its source as possible in compliance with Policy 5.13 Sustainable Drainage of the London Plan (July 2011), and conserve water supplies in accordance with Policy 5.15 Water use and supplies of the London Plan (July 2011).

S106 OFFICER:

Proposal

Demolition of existing public house and erection of 38 flats and 237sqm of retail floorspace with parking, landscaping and amenity space

38 Flats

25 market units - 6 x 1bed & 19 x 2bed

13 Social Rent units - 8 x 1 bed & 5 x 2 bed

237sqm of retail floorspace

Resulting population = 61.19

S106 Obligations sought:

1. Highway: S278/S38 for Highways works sought.
2. Affordable Housing: 34% Provision as proposed.
3. Education: £98,896 (with full nomination rights).
4. Health: £216.67 x 61.19 = £13,258.04
5. Construction Training: Training Cost: £2500 per £1m build cost + coordinator costs: 38/160 x £71,675
6. Libraries: £23 x 61.19 = £1407.37
7. Air Quality: £12,500
8. Project Management & Monitoring Fee: 5% of total cash contributions

7. MAIN PLANNING ISSUES

7.01 The principle of the development

The National Planning Policy Framework (NPPF) at paragraph 2 states that "Planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise". Within Policy 3.3 of the London Plan, the Mayor recognises the pressing need for more homes in London in order to promote opportunity and provide a real choice for all Londoners in ways that meet their needs at a price they can afford. Policy 3.4 of the London Plan seeks to ensure new development proposals make optimum use of the site with housing that respects local context and its location.

Further, Government guidance requires Local Authorities to make the best use of urban land within the Borough while safeguarding the quality of the surrounding environment and the amenity of neighbouring residents. This guidance is reflected in Development Plan Policy. The National Planning Policy Framework states that local authorities should actively promote sustainable development attaching significant weight to the provision of housing and affordable housing.

No objections are raised to the loss of the existing public house building which not being statutorily or locally listed, does not have sufficient architectural or historical merit to warrant its retention. Furthermore, there are no policies that specifically resist the loss of a public house use and given the proximity of surrounding public houses either side of the railway line, no objections could be raised on social grounds to the loss of this facility.

The application site forms part of the Yiewsley/West Drayton Town Centre. Policy 2.15 of the London Plan advises that proposals should sustain and enhance the vitality and viability of the centre and accommodate economic and/or housing growth through intensification and selective expansion in appropriate locations. The front part of the site also forms part of the town centre's primary shopping frontage and the scheme does not involve the loss of any retail floor space, whereas 237sqm of retail floor space is proposed along approximately half of the site's Station Road frontage. This amount is considered to be appropriate to the overall size of the town centre and would enhance its role and function and being within the town centre, the site is sequentially preferable so as to accord with Policy 4.7 of the London Plan.

At the local level, in terms of specific housing policy, Policy H4 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) states that the Council will seek to achieve a balance in the mix of housing types and sizes in order to create a more mixed and balanced community. Of the 38 units, 13 would be affordable.

The provision of a total of 38 self-contained flats and 237sqm of retail floor space is considered acceptable in principle, in land use terms, given the site's location within the town centre.

7.02 Density of the proposed development

The London Plan (July 2011) provides guidance on appropriate densities for residential development, depending upon the type of location and the Public Transport Accessibility Level (PTAL). This site has a PTAL ranging from 2 to 3 (where 6 represents the highest level of public transport accessibility and 1 the lowest), and Table 3.2 in the London Plan advises that an appropriate residential density for this urban site would range from 200-450 habitable rooms per hectare (hr/ha) and 70-170 units per hectare (u/ha) for the size of proposed units which have a typical size of 2.7 - 3.0 habitable rooms per unit (hr/u). The Council's HDAS: 'Residential Layouts' also advises that larger rooms over 20sqm and capable of subdivision should be counted as 2 rooms and this has been factored into the density calculation, adding another 15 habitable rooms.

Although this is a mixed use scheme, the majority of the proposed floor space is for residential. Taking the residential element, this scheme equates to a unit density of 141 u/ha and a habitable room density of 426 hr/ha, which is in accordance with the Mayor's guidelines.

The Council's design guidance also advises that numerical densities, whilst useful for undertaking an initial assessment should not be relied upon as the sole means of assessing housing proposals, as density is only an indicator of the acceptability of a

scheme in comparison to its surroundings and a more fuller assessment is required to assess the acceptability of proposed schemes. The development is in accordance with the internal floor area standards of Policy 3.5 of the London Plan and has an acceptable level of external amenity space for the dwellings. Furthermore, the height and massing of the development is also considered acceptable in the context of this town centre site and the mixed character of the surrounding area.

In this respect, the development would not be out of character with the immediate or wider surrounding area and the scheme would have an appropriate density, which would accord with strategic guidance.

7.03 Impact on archaeology/CAs/LBs or Areas of Special Character

The proposals would not be likely to affect any archaeological remains and the application site is not located within or on the fringes of a conservation area or an area of special local character.

The application site is adjacent to the Grade II Listed Railway Arms PH and therefore any re-development on this site would have an impact upon the setting of this listed building. However, it is considered that the scheme has been sufficiently set back from this heritage asset and has been sensitively designed so that it would not harm the setting of the listed building. An open landscaped courtyard area would be provided adjacent to the listed building and the Council's Urban Design/Conservation Officer is fully supportive of the scheme.

7.04 Airport safeguarding

There are no airport safeguarding issues raised by this application.

7.05 Impact on the green belt

This site does not form part of the Green Belt, nor is it located close to the Green Belt boundary. As such, no Green Belt issues are raised by the proposal.

7.07 Impact on the character & appearance of the area

Policy BE13 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) requires new development to harmonise with the existing street scene or other features of the area which it is desirable to retain or enhance. Policy BE19 requires development within residential areas to complement and improve the character of the area whereas Policy BE26 requires the design, layout and landscaping of new buildings within town centres to reflect the role, overall scale and character of the town centre, particularly as a focus for shopping and employment activity.

The existing site is characterised by the two storey public house fronting Station Road and its enclosed beer garden on its southern side with open yard areas comprising the majority of the remainder of the site at the rear. The site has a somewhat unkempt appearance and its sensitive redevelopment would improve the character and appearance of this part of the town centre.

The Council's Urban Design /Conservation Officer advises that the overall scale of the proposed buildings is appropriate, reflecting the urban street scale, with the blocks generally being three storied in height with pitched roofs above adjoining the road frontages, one of which steps up to four storeys on the northern part of the site, adjacent to the elevated railway line.

The block fronting Station Road would roughly maintain the alignment of the adjoining retail parade to the south. A landscaped courtyard would open onto Station Road and separate the new development from the listed Railway Arms PH to the north. The layout

of the buildings also provides an opportunity to enclose an internal courtyard which offers the scope for some planting, with a landscaped children's play area sited at the rear, at the end of Tachbrook Road, adjacent to the railway embankment.

Although the form and massing of the scheme is fairly extensive, the application proposes a design which is traditional in appearance and is both convincing and well-articulated. The massing of the blocks is broken down by projecting vertical elements giving depth to the elevations and visual interest and the design seeks to reinforce local distinctiveness which is characterised by inter-war development and is vaguely in the 'Tudorbethan' style. On the Station Road frontage, the commercial retail unit on the ground floor integrates with the residential units above. Adjacent to the listed Railway Arms PH, open metal balconies pick up on the balconied design of the public house which would help to frame the courtyard. The roofscape has deep eaves with projecting half timbered gabled elements being a feature which is carried through to the courtyard where there are more projecting gables (with glazed balconies) at regular intervals and to the rear. The north elevation features a tower element decorated with diamond-shaped brick panels.

The facades would mainly be finished in a combination of London Stock brick walls, sections of render and timber cladding and slate roof tiles. Solid timber doors, brick arches, stone cills would also feature. Although there is limited scope for soft landscaping, with the main amenity space located on the roof, the Urban Design/ Conservation Officer considers that curtilages and boundaries have been well defined with soft landscaping where possible. The children's play area would replace a car wash facility at the end of Tachbrook Road and with appropriate landscaping, this would significantly enhance the appearance of Tachbrook Road.

It is therefore considered that the development successfully integrates with surrounding buildings in this urban and historic environment and the development will contribute positively to the streetscene. Subject to the conditioning of all materials, no objections are raised on design grounds and the scheme complies with Policies BE13, BE19 and BE26 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) and Policies 7.4 and 7.6 of the London Plan (July 2011).

7.08 Impact on neighbours

Policies BE20, BE21 and BE24 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) seek to protect the amenities of surrounding residential properties from new development in relation to loss of sunlight, dominance and loss of privacy respectively. The Council's Supplementary Planning Document HDAS: Residential Layouts provides further clarification in that it advises that buildings of two or more storeys should maintain at least a 15m separation distance from adjoining properties to avoid appearing overdominant and a 21m distance between facing habitable room windows and private amenity space (considered to be a 3m deep 'patio' area adjoining the rear elevation of a property) should be maintained to safeguard privacy.

As regards loss of sunlight, the proposed blocks would mainly overshadow the railway embankment and the Railway Arms PH to the north. Although the public house does contain residential accommodation, this is on the first floor, where there are no first floor side windows facing the application site and its rear facing windows would not be materially affected by overshadowing. The rear garden space is also mainly used as a beer garden. As such, it is considered that the proposal would not result in any unacceptable overshadowing of neighbouring residential property.

In terms of dominance and overlooking, the nearest main elevations of residential

properties would be the first floor rear elevation of the Railway Arms PH and the flats above the shops on the opposite side of Station Road. Although there are currently no residential properties immediately opposite the site on Tachbrook Road, there is an extant permission for the partial residential redevelopment of this site (App. No. 18218/APP/2013/2183 refers).

As regards the Railway Arms, the proposed side elevation of Block B would be sited directly to the rear of the building but at a distance of over 22m from the public house's main rear elevation. Block A would be sited to the side of its curtilage. Taking a 45 degree line of sight from the nearest first floor rear window, at its nearest point Block A would be sited some 11.9m away from this window and at this point, the block would be set back some 2.3m from the side boundary. This proposed elevation would also contain main habitable room windows and balconies but given their siting, it is only the two bedroom windows of Flat 5 on the first floor and Flat 10 on the second floor that would allow more direct views overlooking the rear elevation of the public house, but given the design of the block, with the bedrooms being within a projecting gable feature, which also includes windows serving the bedrooms in its sides, the rear windows could be obscure glazed and made to be unopenable below a height of 1.8m, controlled by condition. On this basis, it is considered that the scheme, given the proposed relationship and separation distances would not be harmful to the residential amenity of the public house occupants in this town centre location.

As regards the flats on the upper floors within the retail parades on the opposite side of Station Road, the frontages of these units face the road and would be sited some 20.9m away from the nearest part of Block A so that their existing amenity would not be unduly affected.

As regards the proposed redevelopment at Kitchener House, that scheme was for a part three storey block fronting Tachbrook Road, rising to 4 stories at the northern end of the site, with residential accommodation on the upper floors with main habitable room windows overlooking the road. That scheme would be set back some 0.9m from the back edge of Tachbrook Road, which would reduce to 0.5m on the overhanging upper floors as compared to this scheme, which would be set back some 3.5m from the road. The main elevation of Block B of the current proposal would be sited some 15.0m from the nearest part of the proposed frontage of the flatted block at Kitchener House, with its balconies sited some 13.7m away. Although this distance does comply with the Council's minimum 15m distance as regards dominance, it does not comply with the 21m distance regarding privacy. However, this is an across the road relationship, where privacy is naturally compromised. Furthermore, a landscaped children's play area would be sited along much of the length of the two proposed buildings and with appropriate selective tree planting, additional screening could be provided in the longer term. A landscaping scheme to include tree planting in Tachbrook Road/the children's play area has been conditioned.

As regards the roof top amenity areas, their design, concealed within the pitched roofs of the buildings would ensure that the potential for overlooking from their use would not arise.

It is therefore considered that the scheme would not result in any significant adverse impact upon the amenities of existing and proposed surrounding residential occupiers, in accordance with Policies BE20, BE21 and BE24 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

7.09 Living conditions for future occupiers

The proposed flats would have internal floor areas ranging from 50sqm to 53sqm for the 1 bedroom, 2 person and 70sqm to 92sqm for the 2 bedroom, 4 person flats. These areas are sufficient to ensure that all the units satisfy the Mayor's minimum floor space standards for these types of unit of 50sqm and 70sqm respectively.

The residential amenity afforded by the flats would be acceptable, with their habitable rooms providing an acceptable outlook and natural lighting. All the units would have their own patio areas/balconies, which would range in size from 4sqm to 8sqm, with a typical average size of 6sqm. The distance across the internal courtyard between the two blocks would be greater than 21m and the scheme has been carefully designed to ensure that there is defensible space to ground floor habitable room windows and the opportunities for overlooking between adjoining flats in the internal corners of the blocks has been minimised. As mentioned in Section 7.08 above, if the planning permission on Kitchener House on the opposite side of Tachbrook Road were to be implemented, there would be main habitable room windows overlooking each other within a 15m distance, which reduces to 13.7m on the proposed balconies, but this is an across the street relationship where reduced standards of privacy can be expected. In the longer term, the provision of landscaping on Tachbrook Road/ children's play area between the two buildings could provide ample scope for tree planting which could provide an element of screening between the buildings. This has been conditioned.

As regards external amenity space, the Council's amenity space standards require a minimum total of 880sqm of shared amenity space to be provided for the 38 units. The patio areas/balconies would provide a combined total of 222sqm of private amenity space, with the two roof gardens on Block A and the larger garden on Block B providing 525sqm of shared space. In addition, the children's play area would provide a further 350sqm of usable amenity space, to give a good mix and interest of space that totals 1,097sqm, satisfying the Council's standard.

It is therefore considered that the scheme would afford an appropriate level of amenity for future occupiers, in accordance with policies 3.5 and 3.6 of the London Plan (July 2011) and policies BE20, BE21, BE23 and BE24 of the adopted Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

7.10 Traffic impact, Car/cycle parking, pedestrian safety

The application site is centrally located within the Yiewsley/West Drayton town centre, adjacent to West Drayton Railway Station and bus services on Station Road. The site has a moderate public transport accessibility with a PTAL rating ranging from 2 to 3 which would be further upgraded once Crossrail has been implemented.

The Highway Engineer advises that the proposals include 36 car and 38 cycle parking spaces to serve the residential element. There would be no parking to serve the retail unit, although 12 cycle spaces would be provided for use by staff and customers.

Vehicle and pedestrian access to the proposed flats would be provided from Tachbrook Road, with a pedestrian link provided from Station Road. Servicing of the flats would also be undertaken via Tachbrook Road with the retail unit being serviced from Station Road via an existing service lay-by which would be extended.

A Transport Assessment has been submitted in support of the application. The Council's Highway Engineer has reviewed the report and advises that the existing and proposed trip generation at the site has been undertaken using the TRICS Database. However, the number of selected sample sites are limited and not all sites are representative.

Nevertheless, The Highway Engineer concludes that any increase in vehicle trips as a result of the development would not have a material impact along the adjacent highway network.

The scheme has been revised, following advice from the Council's Highway Engineer, and now includes 36 off-street car parking spaces which equates to a ratio of 1:0.95. This is the same ratio used on two recently approved schemes on Padcroft Works and 21 High Street and is very close to the 1:1 ratio on the Kitchener House scheme. This has involved more of the central courtyard area being used for car parking where the children's play area was originally proposed. The application site has now been extended to include the adjoining unadopted Tachbrook Road and an appropriate notice has been placed in the local paper by the applicant.

The Highway Engineer advises that given the PTAL level of the site, which is located immediately adjacent to all public transport facilities including West Drayton Rail Station, which will be upgraded as part of the Cross Rail development, the proposed car parking provision is now acceptable to serve the proposals. Furthermore, in order to assess the proposed car parking layout, a swept path analyses has been undertaken. The Highway Engineer advises that some of the parking spaces within the site would be difficult to access for a larger car and would require excessive manoeuvring. While it is considered that these spaces can be accessed by a medium sized car, it is preferable that car stackers are provided within the site, which should be covered by a suitably worded planning condition.

The Highway Engineer concludes that subject to conditions/S106 Agreement to ensure that the parking provision within the site shall include 20% active and 20% passive electric charging points; details of the proposed cycle parking facilities; the existing loading bay adjacent to the site along Station Road being reconfigured and extended; the existing vehicle crossover providing access to the site from Station Road, being reinstated as pedestrian footway; the submission of a Delivery and Servicing Management Plan and Construction Management plan, the scheme is acceptable from a highway perspective.

7.11 Urban design, access and security

A secure by design condition has been added.

7.12 Disabled access

Policies 3.8 and 7.2 of the London Plan (July 2011) promote housing choice and require all new development to provide an inclusive environment that achieves the highest standards of accessibility and inclusive design respectively. The Council's Supplementary Planning Document "Accessible Hillingdon" (May 2013) provides detailed design guidance on accessibility issues.

The scheme would provide four wheelchair accessible units on the ground floor within Block B, with the remainder being compliant with Lifetime Home Standards. The development proposes a lift within each of the blocks that would allow wheelchair access to all of the flats and roof top amenity space on Block B.

The Council's Access Officer has reviewed the proposals and advises that the scheme is acceptable from an accessibility perspective.

7.13 Provision of affordable & special needs housing

The development would introduce a total of 38 dwellings, therefore triggering the affordable housing requirement threshold of 10 units as set out in Policy 3.13 of the London Plan.

Policy H2 of the Hillingdon Local Plan: Part 1 - Strategic Policies relates to Affordable Housing with the Council seeking 35% of all new units in the borough delivered as affordable housing. The Council note however, that subject to the provision of robust evidence, it will adopt a degree of flexibility in its application of Policy H2, to take account, of tenure needs in different parts of the borough as well as the viability of schemes.

The scheme proposes 13 units of affordable housing which equates to 34.2%, the nearest split of the proposed housing to satisfy the 35% recommended requirement and the scheme would therefore fully comply with relevant Local Plan and London Plan policies and make a valuable contribution towards the Borough's affordable housing stock.

7.14 Trees, landscaping and Ecology

Saved policy BE38 of the Hillingdon Local Plan advises that new development should retain topographical and landscape features of merit and that new planting and landscaping should be provided wherever it is appropriate.

The Council's Tree/Landscaping Officer advises that there is a set of self-seeded Sycamore trees running through the site and a wooded railway embankment adjoins part of the site's northern boundary. The trees are not protected by TPO or conservation area designation.

The trees have been assessed within the submitted Arboricultural Report which identifies them as Ash and Sycamore trees which are of poor quality and do not constrain development. They are all proposed for removal.

The Council's Tree Officer advises that the trees have a limited useful life expectancy and are not significant in terms of their amenity value so that their loss is justified, provided that suitable replacement planting is secured as part of the new development.

The proposed layout, with most of the amenity space being provided at roof top level and the ground floor, particularly the central courtyard being utilised for parking, only allows limited opportunities for landscaping. However, the Council's Tree Officer advises that new planting, including tree planting will feature in the open courtyard fronting onto Station Road which has the potential to make a significant and welcome contribution to the west Drayton public realm. Within the site, there is limited landscaping and the small central area would be suitable to provide a specimen tree. The landscaped children's play area on Tachbrook Road would also be suitable to provide additional landscaping and tree planting which would make a significant improvement to the street scene. Landscaping within the roof top gardens would also assist in greening the area.

The Council's Tree Officer concludes that landscape conditions should be imposed to ensure that the proposals preserve and enhance the character and appearance of the area in accordance with the submitted plans and illustrations. As such, the scheme complies with Saved policy BE38 of the Hillingdon Local Plan (November 2011).

Ecology

The site has no significant ecological interest.

7.15 Sustainable waste management

The scheme makes adequate provision, integral within the buildings for waste and recycling at two points adjacent to the main entrances to each of the blocks.

7.16 Renewable energy / Sustainability

The Council's Sustainability Officer advises that the submitted energy assessment is

inadequate as it refers to outdated London Plan policy but subject to the imposition of a condition, the scheme could be made to comply with current sustainability policies. The recommended condition has been attached.

7.17 Flooding or Drainage Issues

The Council's Flood and Water Management Officer advises that the submitted Flood Risk Statement is acceptable and no objections are raised to the granting of permission of flood risk grounds subject to the imposition of a suitable condition. This forms part of the officer recommendation.

7.18 Noise or Air Quality Issues

The application site is located within the Borough's Air Quality Management Area (AQMA). The Council's Environmental Protection Officer has assessed the Air Quality Assessment submitted with the application and details a number of limitations of the study but concludes that given the nature of the scheme, the impact of the proposal upon air quality would not be significant and recommends two conditions which are attached.

7.19 Comments on Public Consultations

No comments have been received from neighbouring properties.

7.20 Planning obligations

Policy R17 of the adopted Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) is concerned with securing planning benefits related to the scale and type of development. The policy is supported by more specific supplementary planning guidance.

The following would be required to mitigate the impact of the development:

1. Affordable Housing: 34% Provision as proposed.
2. Highway: S278/S38 for Highways works sought.
3. Education: £98,896 (with full nomination rights).
4. Health: £216.67 x 61.19 = £13,258.04
5. Construction Training: Training Cost: £2500 per £1m build cost + coordinator costs: 38/160 x £71,675
6. Libraries: £23 x 61.19 = £1407.37
7. Air Quality: £12,500
8. Project Management & Monitoring Fee: 5% of total cash contributions

The applicant has agreed to the above heads of terms. As such, the scheme complies with Policy R17 of the Hillingdon Local Plan: Part Two - Saved UDP Policies.

The development also represents chargeable development under the Mayor's Community Infrastructure Levy which would equate to £35 per sq.m of floorspace (including within the basement car park) adjusted for inflation.

7.21 Expediency of enforcement action

There are no enforcement issues raised by this application.

7.22 Other Issues

Land Contamination

A land contamination desk study has been submitted with the application which concludes that further investigation of the site is required. The study has been reviewed by the Council's Environmental Health Protection Officer. They advise that the study is quite limited, but do not raise objection to the scheme, subject to appropriate conditions to ensure that further assessment of gas and vapour contamination as well as any

contamination that could affect the buildings and water pipes etc. is undertaken. They also require a condition to ensure that imported soils are tested for contamination. These conditions are recommended.

8. Observations of the Borough Solicitor

General

Members must determine planning applications having due regard to the provisions of the development plan so far as material to the application, any local finance considerations so far as material to the application, and to any other material considerations (including regional and national policy and guidance). Members must also determine applications in accordance with all relevant primary and secondary legislation.

Material considerations are those which are relevant to regulating the development and use of land in the public interest. The considerations must fairly and reasonably relate to the application concerned.

Members should also ensure that their involvement in the determination of planning applications adheres to the Members Code of Conduct as adopted by Full Council and also the guidance contained in Probity in Planning, 2009.

Planning Conditions

Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal. Planning conditions should only be imposed where Members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

Planning Obligations

Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act 1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

Equalities and Human Rights

Section 149 of the Equalities Act 2010, requires the Council, in considering planning applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The requirement to have due regard to the above goals means that members should consider whether persons with particular protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic. Where equalities issues arise, members should weigh up the equalities impact of the proposals against the other material considerations relating to the planning application. Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be taken into account in weighing up the merits of an application. The weight to be given to any equalities issues is a matter for the decision maker to determine in all of the circumstances.

Members should also consider whether a planning decision would affect human rights, in particular the right to a fair hearing, the right to respect for private and family life, the protection of property and the prohibition of discrimination. Any decision must be proportionate and achieve a fair balance between private interests and the public interest.

9. Observations of the Director of Finance

10. CONCLUSION

This application would remove a poor quality building and an extensive area of hardstanding and provide a mixed retail and residential re-development scheme comprising 38 residential units of 35% affordable housing, four of which would be fully wheelchair accessible.

It is considered that the scheme would make a positive contribution to the street scene and the character and appearance of the area and would provide suitable accommodation for its future occupiers. It is considered that there would be no adverse impacts upon surrounding residential occupiers or the surrounding highway network.

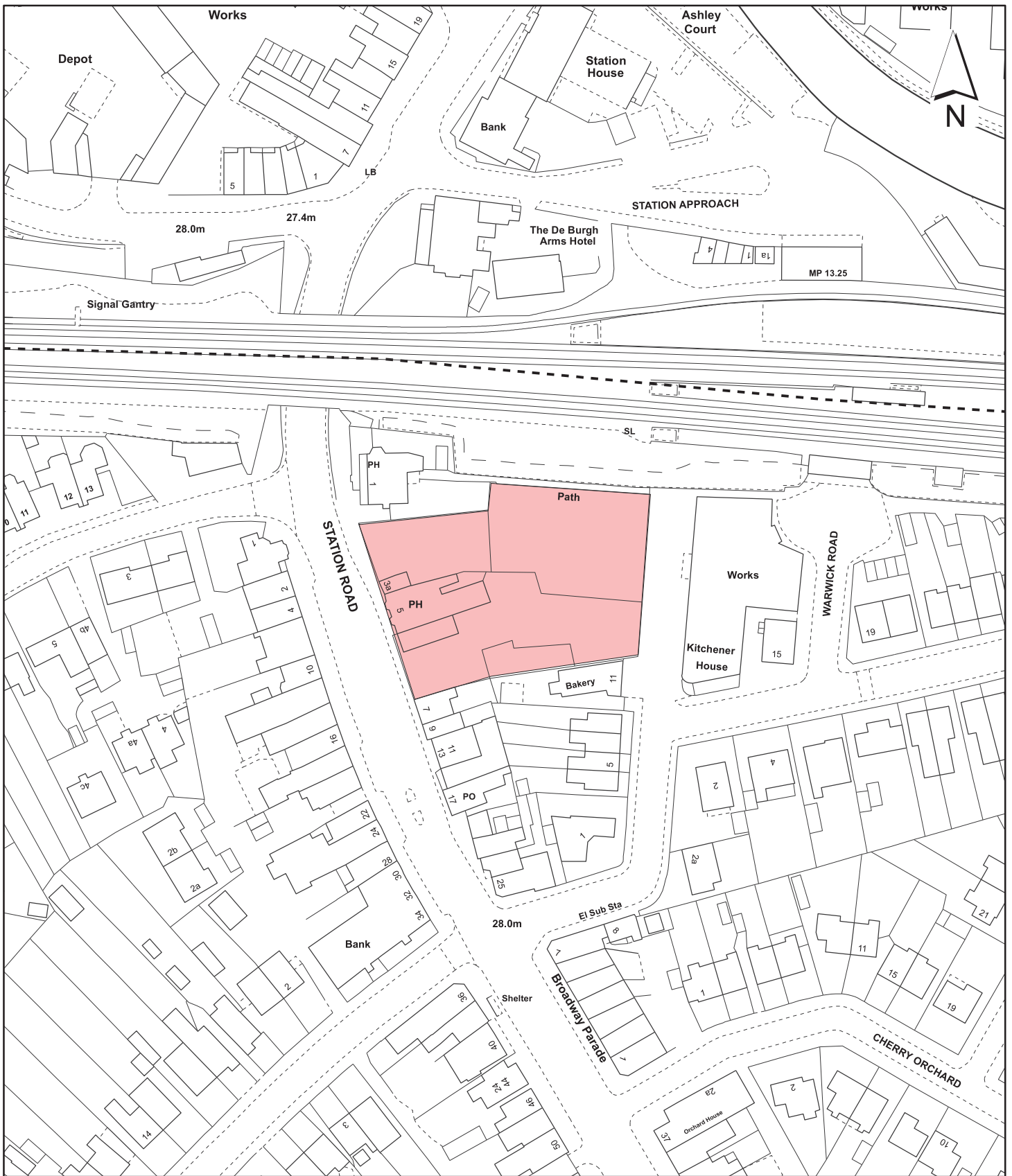
The scheme would also provide a commensurate package of planning benefits and is therefore recommended for approval.



11. Reference Documents

National Planning Policy Framework (March 2012)
Planning Practice Guidance (March 2014)
London Plan (July 2011)
Mayor's Housing Supplementary Planning Guidance, November 2012
Hillingdon Local Plan (November 2012)
Consultation Responses

Contact Officer: Richard Phillips

Telephone No: 01895 250230



<p>Notes</p> <p> Site boundary</p> <p>For identification purposes only.</p> <p>This copy has been made by or with the authority of the Head of Committee Services pursuant to section 47 of the Copyright, Designs and Patents Act 1988 (the Act). Unless the Act provides a relevant exception to copyright.</p> <p>© Crown copyright and database rights 2014 Ordnance Survey 100019283</p>	<p>Site Address</p> <p style="text-align: center;">5 Station Road West Drayton</p>		<p>LONDON BOROUGH OF HILLINGDON</p> <p>Residents Services Planning Section</p> <p>Civic Centre, Uxbridge, Middx. UB8 1UW Telephone No.: Uxbridge 250111</p>
	<p>Planning Application Ref:</p> <p style="text-align: center;">65480/APP/2014/1018</p>	<p>Scale</p> <p style="text-align: center;">1:1,250</p>	 <p style="text-align: center;">HILLINGDON LONDON</p>
	<p>Planning Committee</p> <p style="text-align: center;">MajorPage 174</p>	<p>Date</p> <p style="text-align: center;">August 2014</p>	

Plans for Major Applications Planning Committee

6th August 2014



HILLINGDON
LONDON



INVESTOR IN PEOPLE

Report of the Head of Planning, Sport and Green Spaces

Address LAND TO THE WEST OF LAUREL LANE WEST DRAYTON

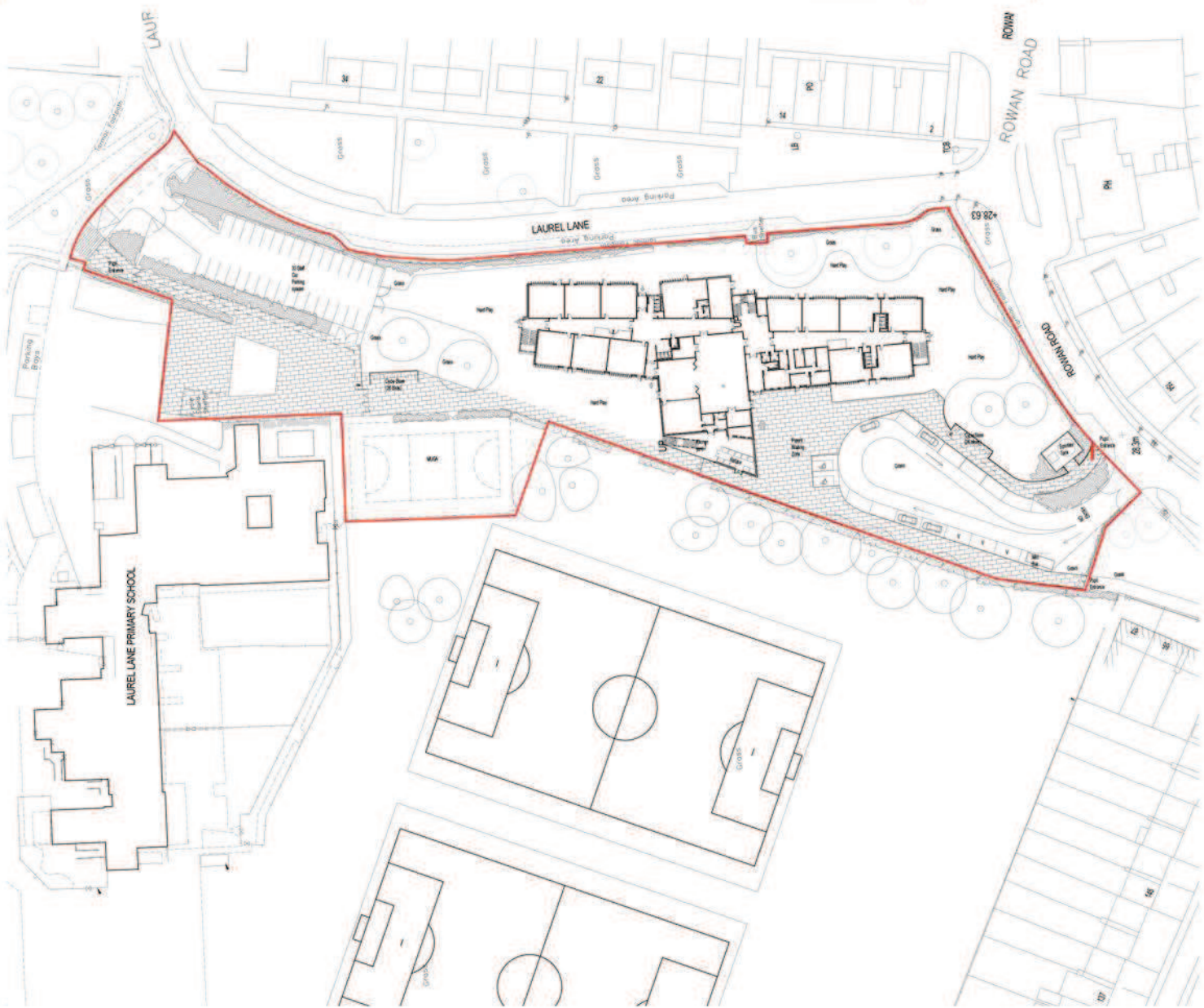
Development: New 2 storey Junior School (5 forms of entry) including new vehicular and pedestrian accesses, alterations to an existing footpath, creation of pick-up/drop-off area, associated car parking, landscaping, playground, provision of a Multi-Use Games Area (MUGA) and ancillary development.

LBH Ref Nos: 70019/APP/2014/1807

Date Plans Received: 27/05/2014

Date(s) of Amendment(s):

Date Application Valid: 30/05/2014



Location Plan
Scale 1:1250

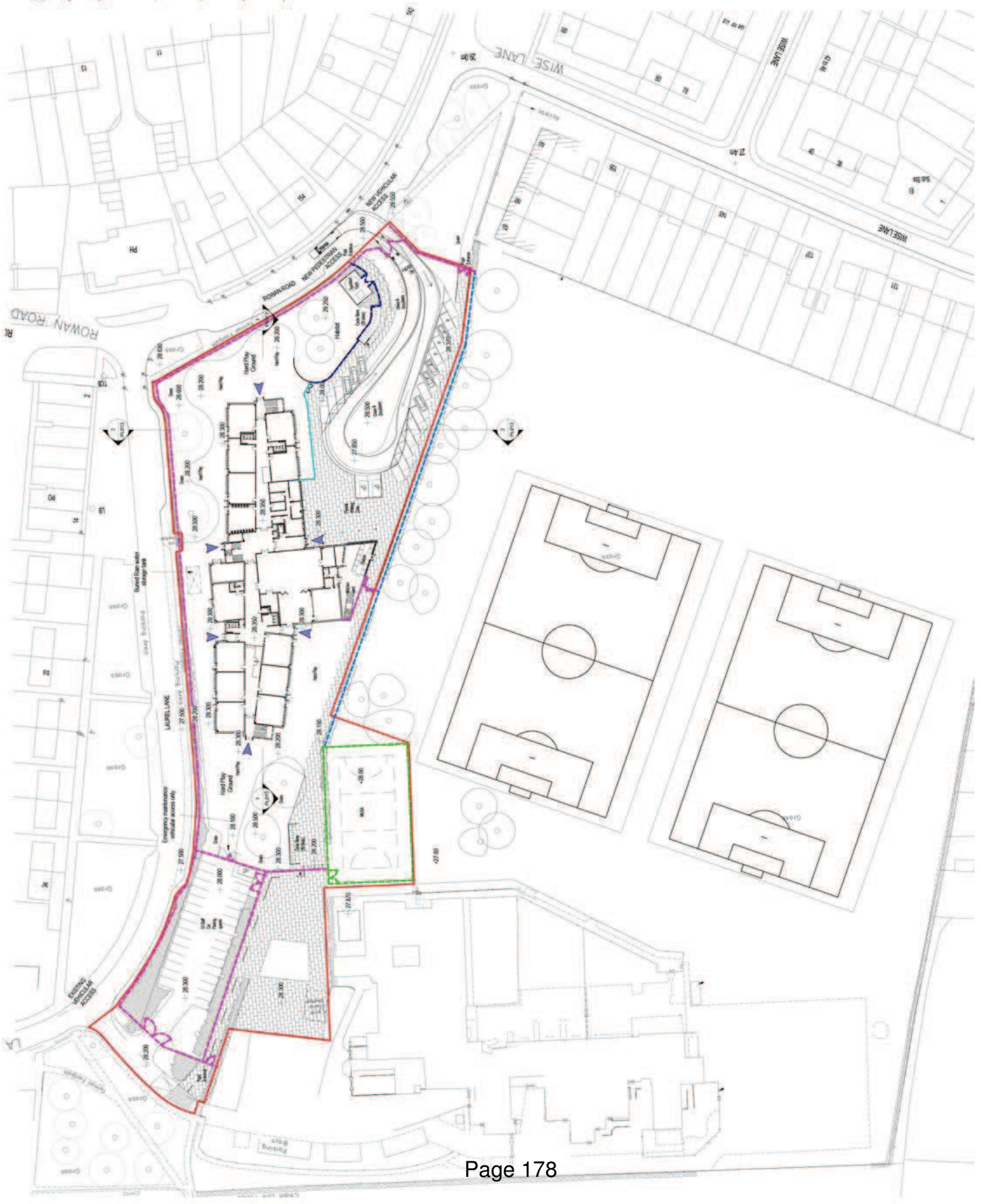
1 Site Plan
Scale 1:500

Revisions	
No.	Description
1	Final Design
2	Final Design
3	Final Design
4	Final Design
5	Final Design
6	Final Design
7	Final Design
8	Final Design
9	Final Design
10	Final Design

hunters
south
architects



Project Name: Proposed New Junior School, Laurel Lane
 Location: Laurens & Stiel Plan
 Date: 10/2011
 Drawn: J. Jones
 Checked: J. Jones
 Scale: 1:500
 Plot No: Z0008
 Plot No: PL003
 Rev: B



- Key to Boundary Treatments**
- Site Boundary line
 - Type A - secure play ground fence comprising vertical metal bow top, 2.1m high
 - Type B - metal bow top, 1.6m high
 - Type C - 3m high MUGA Sports' fencing, 3.0 m high.
 - Type D - 2.1m circular timber pole fence, 2.1 m high.
 - Type E - existing school fence.

Scale 1:500

0 5 10 15 20 Meters

Revision	Date	By	Check
1	14.12.2024	PL	PL
2	14.12.2024	PL	PL
3	14.12.2024	PL	PL
4	14.12.2024	PL	PL
5	14.12.2024	PL	PL
6	14.12.2024	PL	PL
7	14.12.2024	PL	PL
8	14.12.2024	PL	PL
9	14.12.2024	PL	PL
10	14.12.2024	PL	PL

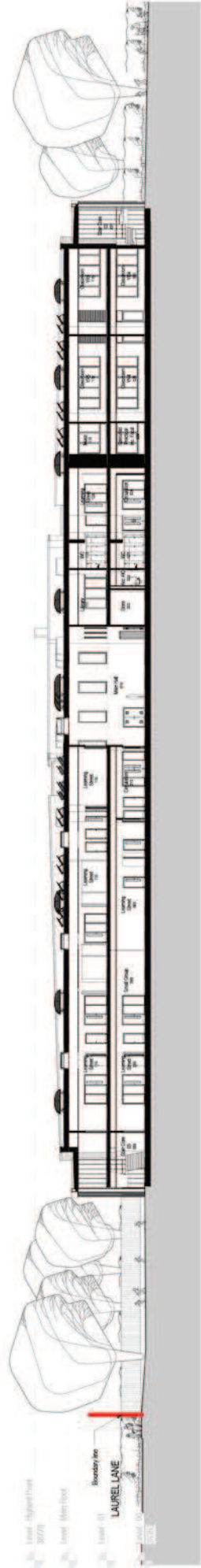
Author: PL
 Designer: PL
 Checker: PL
 Date: 14.12.2024
 No. of Sheets: 10
 Sheet No.: 10
 Project Name: Proposed New Junior School, Label Lane
 Client: [Redacted]
 Drawing Title: Proposed Site Layout

hunters south architects

North Arrow

Proposed New Junior School, Label Lane
 Proposed Site Layout

Date: 14.12.2024
 Sheet: 10
 Checked By: PL
 Drawn By: PL
 Date: 14.12.2024
 Project No: Z0008
 Drawing No: PL005
 Rev: D



1 Section 1:200



2 Section 1:200



Revisions

No.	Description	Date	By
1	Final Submission	15/03/2018	JLH
2	Planning Submission	02/03/2018	JLH
3	Discussions	02/03/2018	JLH

Drawn by: JLH
 Checked by: JLH
 Date: 15/03/2018
 Project No: Z0008
 Plot No: PL013
 Sheet: B

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 West Melbourne VIC 3207
 Australia
 Phone: +61 (0)3 9347 1000
 Email: info@hunterssouth.com.au
 Website: www.hunterssouth.com.au

Project Name: Proposed New Junior School, Laurel Lane
 Discipline: Proposed Site Sections
 Scale: 1:200 (A1)
 Date: May 2018
 Checked By: JLH
 Drawn By: JLH



Scale 1 to 300

Revisions	
No.	Description
1.	Planning Information
2.	Planning Information
3.	Planning Information
4.	Planning Information
5.	Planning Information
6.	Planning Information
7.	Planning Information
8.	Planning Information
9.	Planning Information
10.	Planning Information
11.	Planning Information
12.	Planning Information
13.	Planning Information
14.	Planning Information
15.	Planning Information
16.	Planning Information
17.	Planning Information
18.	Planning Information
19.	Planning Information
20.	Planning Information
21.	Planning Information
22.	Planning Information
23.	Planning Information
24.	Planning Information
25.	Planning Information
26.	Planning Information
27.	Planning Information
28.	Planning Information
29.	Planning Information
30.	Planning Information

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architects

Project Name: Proposed New Junior School, Laurel Lane
 Location: Proposed Ground Floor Plan
 Date: May 2018
 Drawn By: Z0008
 Checked By: PL007
 Scale: 1:200 (A1)
 Title: For Planning



Scale 1 to 300



Scale: 1/8" = 1'-0" 0 1 2 3 4 5 6 7 8 9 10 Feet

Revisions:

No.	Description	Date	By	Checked
1	Issue for Review	11/10/2010	JS	JS
2	Issue for Review	11/10/2010	JS	JS
3	Issue for Review	11/10/2010	JS	JS
4	Issue for Review	11/10/2010	JS	JS
5	Issue for Review	11/10/2010	JS	JS
6	Issue for Review	11/10/2010	JS	JS
7	Issue for Review	11/10/2010	JS	JS
8	Issue for Review	11/10/2010	JS	JS
9	Issue for Review	11/10/2010	JS	JS
10	Issue for Review	11/10/2010	JS	JS

Notes:
 1. All dimensions are in feet and inches.
 2. All dimensions are to the center of the wall unless otherwise noted.
 3. All dimensions are to the center of the door unless otherwise noted.
 4. All dimensions are to the center of the window unless otherwise noted.
 5. All dimensions are to the center of the column unless otherwise noted.
 6. All dimensions are to the center of the beam unless otherwise noted.
 7. All dimensions are to the center of the slab unless otherwise noted.
 8. All dimensions are to the center of the ceiling unless otherwise noted.
 9. All dimensions are to the center of the floor unless otherwise noted.
 10. All dimensions are to the center of the wall unless otherwise noted.

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Project Name: Proposed New Junior School, Laurel Lane
 Location: Proposed First Floor Plan
 Date: May 2010
 Drawn By: JS
 Checked By: JS
 Scale: 1/8" = 1'-0"

North Arrow

Project No: Z0008
 Drawing No: PL008
 Sheet: D



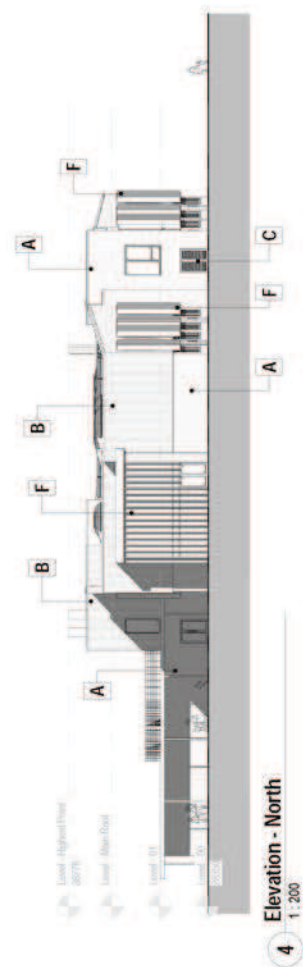
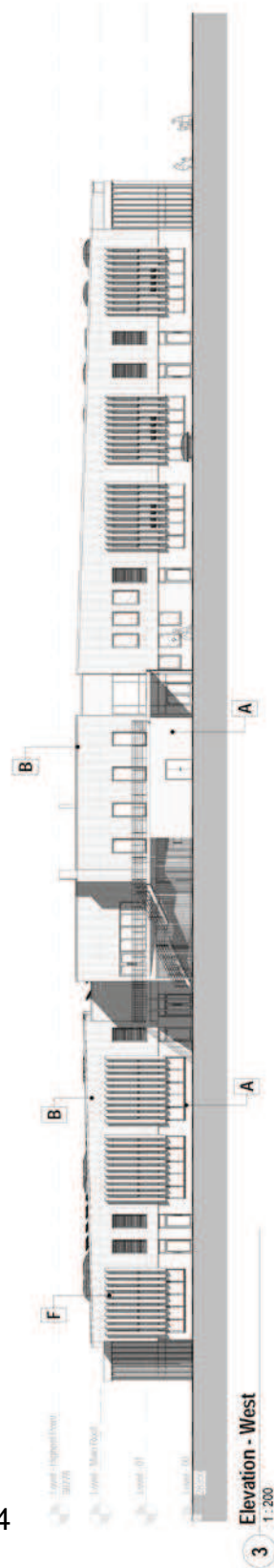
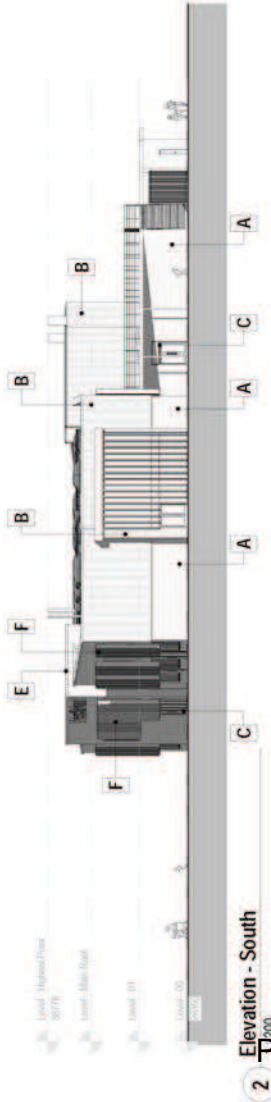
- Key To Roof Areas**
- Roof Access Walk ways
 - Ecological Roof Area 1020sq.m
 - Roof Lights
 - Photovoltaic Panels 200sq.m
 - Roof Ventilators 2000 x 2000m



No.	Description	Date	By	Checked By
1.	Planning Submission	20/03/2018	AS	
2.	Planning Submission	20/03/2018	AS	
3.	Planning Submission	20/03/2018	AS	
4.	Planning Submission	20/03/2018	AS	
5.	Planning Submission	20/03/2018	AS	
6.	Discharge	20/03/2018	AS	
7.	Discharge	20/03/2018	AS	
8.	Discharge	20/03/2018	AS	
9.	Discharge	20/03/2018	AS	
10.	Discharge	20/03/2018	AS	

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Project Name: Proposed New Junior School, Laurel Lane
 Site Name: Proposed Roof Plan
 Date: May 2018
 Drawn By: Z0008
 Checked By: PL009
 Scale: 1:200 (A1)
 Sheet: For Planning
 No. 00008
 Rev. C



Type	Revised By	Revised Description
1		Initial Elevation Set
2		Revised Elevation Set
3		Revised Elevation Set
4		Revised Elevation Set
5		Revised Elevation Set
6		Revised Elevation Set
7		Revised Elevation Set
8		Revised Elevation Set
9		Revised Elevation Set
10		Revised Elevation Set



Revisions	No.	Date	Revised By	Description
1	1	10/20/2011	JL	Initial Elevation Set
2	2	10/20/2011	JL	Revised Elevation Set
3	3	10/20/2011	JL	Revised Elevation Set
4	4	10/20/2011	JL	Revised Elevation Set
5	5	10/20/2011	JL	Revised Elevation Set
6	6	10/20/2011	JL	Revised Elevation Set
7	7	10/20/2011	JL	Revised Elevation Set
8	8	10/20/2011	JL	Revised Elevation Set
9	9	10/20/2011	JL	Revised Elevation Set
10	10	10/20/2011	JL	Revised Elevation Set

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Project Name: Proposed New Junior School, Laurel Lane
 Location: Proposed Elevations
 Scale: 1/8" = 1'-0" (For Planning)
 Date: May 2011
 Checked By: JAL/SL
 Drawing No.: Z0008 PL010 C
 Date: 04/11



1 Section
1:200



2 Section
1:200



3 Section
1:200

Scale 1:10,000

Revisions

No.	Description	Date	By	Checked
1.	Planning Submission	20/03/2018	AS	AS
2.	Planning Submission	20/03/2018	AS	AS
3.	Planning Submission	20/03/2018	AS	AS
4.	Planning Submission	20/03/2018	AS	AS

Author: AS
 Designer: AS
 Checker: AS
 Date: 20/03/2018
 No. Sheets: 1
 Project Name: Proposed New Junior School, Laurel Lane
 Project No: Z0008
 Drawing No: PL012
 Title: For Planning

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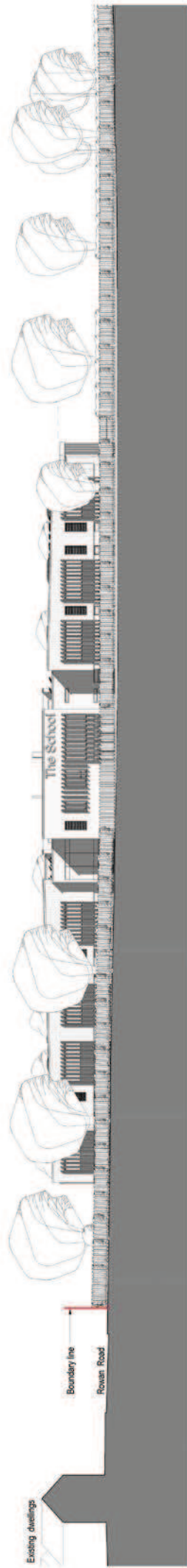
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architects

Project Name: Proposed New Junior School, Laurel Lane
 Project No: Z0008
 Drawing No: PL012
 Title: For Planning

Date: 20/03/2018
 Checked By: AS
 Drawn By: AS

Scale: 1:200 (A1)
 Sheet: For Planning

Project Name: Proposed New Junior School, Laurel Lane
 Project No: Z0008
 Drawing No: PL012
 Title: For Planning



1 Laurel Lane Elevation
1:250



2 Rowan Road Elevation
1:250

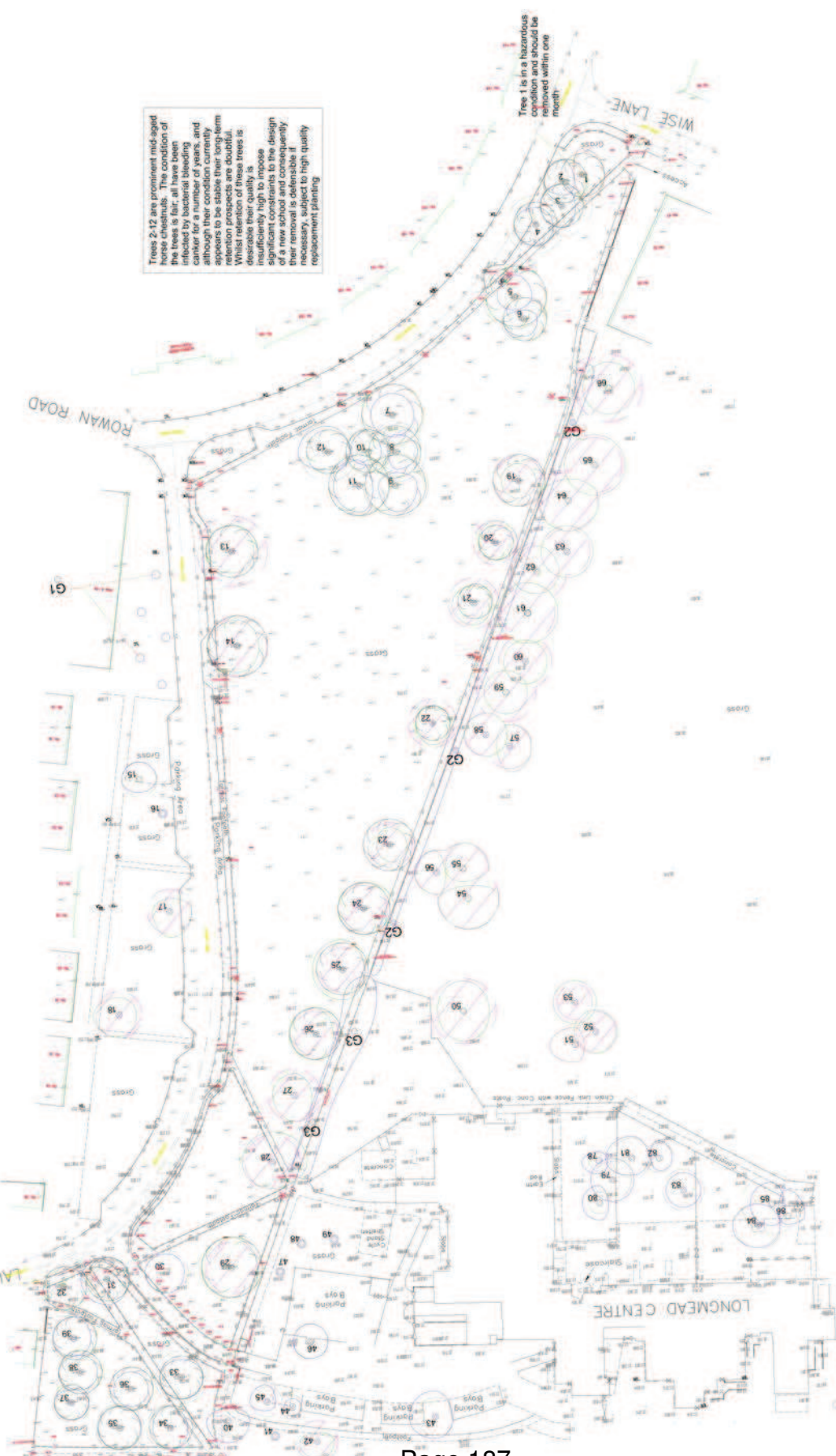
Scale 1:250 0 5 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 Meters

No.	Description	Date	Drawn By
1	Prepared	2018-04-11	JAK
2	Revised	2018-04-11	JAK
3	Revised	2018-04-11	JAK
4	Revised	2018-04-11	JAK
5	Revised	2018-04-11	JAK
6	Revised	2018-04-11	JAK
7	Revised	2018-04-11	JAK
8	Revised	2018-04-11	JAK
9	Revised	2018-04-11	JAK
10	Revised	2018-04-11	JAK

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Project Name: Proposed New Junior School, Laurel Lane
 Site Name: Proposed Street Elevations
 Date: 11/20/2018
 Drawn: JAK
 Job No: Z0008
 Plot No: PL011
 Rev: A



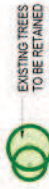
Trees 2-12 are prominent mid-aged horse chestnuts. The condition of the trees is fair, all have been infested by bacterial bleeding canker for a number of years, and appear to be stable their long term retention prospects are doubtful. Whilst retention of these trees is desirable their quality is insufficiently high to impose a high quality design of a new school and consequently their removal is defensible if necessary, subject to high quality replacement planting.

Tree 1 is in a hazardous condition and should be removed within one month

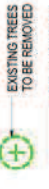
Notes

TREE TYPE SCHEDULE

- T1 - Horse Chestnut, h=13m
- T2 - Pedunculate oak, h=13m
- T3 - Norway maple, h=12-13m
- T4 - Whitebeam or "Lutescens", h=10-11m
- T5 - Tree of heaven, h=10-13m
- T6 - Ash, h=14
- T7 - Sweet Gum, h=11
- T8 - Hornbeam, h=12m
- T9 - Yew, h=7m
- T10 - Flowering Cherry, h=8
- T11 - Hillier Cherry, h=8
- T12 - Hornbeam, h=8
- T13 - Softwood, h=8m
- T14 - Tilia sp, h=4-14m
- T15 - Acer sp, h=4m
- T16 - Common Lime, h=10-15m
- T17 - Cappadocian maple, h=14-15
- T18 - Swedish whitebeam, h=8-12m
- T19 - Indian beech tree, h=13m
- T20 - Purple plum, h=10m
- T21 - Cherry, h=8m
- T22 - Myrobalan Plum, h=5m
- T23 - Plum, h=5m
- T24 - Spangore, h=7m



EXISTING TREES TO BE RETAINED



EXISTING TREES TO BE REMOVED

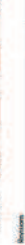


SITE BOUNDARY

NOTE

- OTHER TREE NUMBERS DENOTE REFERENCES SHOWN ON PATRICK STILLEMAN TREE SURVEY PLAN & ARBOCULTURAL REPORT - PLUZZ.
- FOR FURTHER INFORMATION REFER TO PLUZZ TREE SURVEY REPORT

Scale 1:500

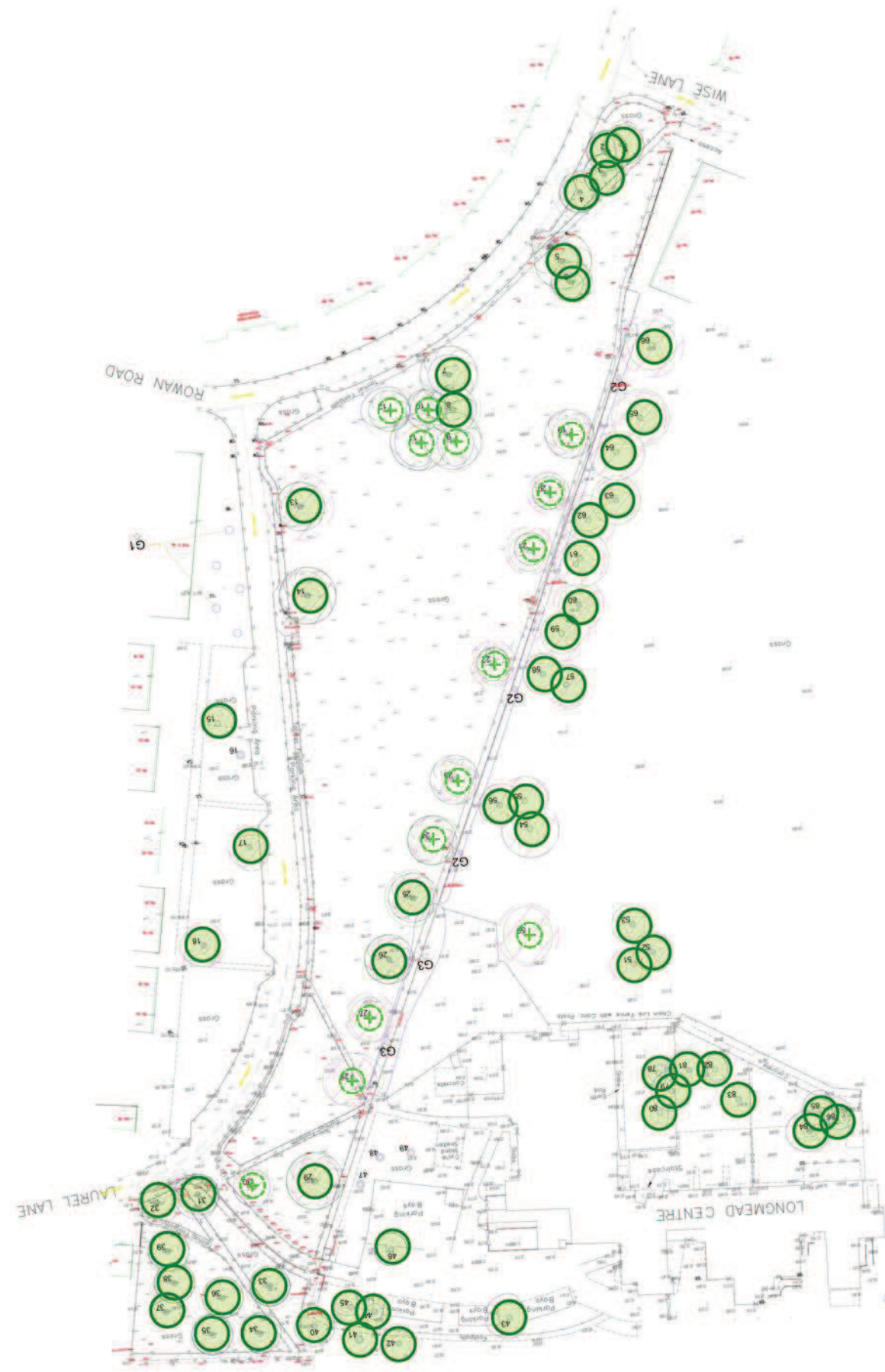


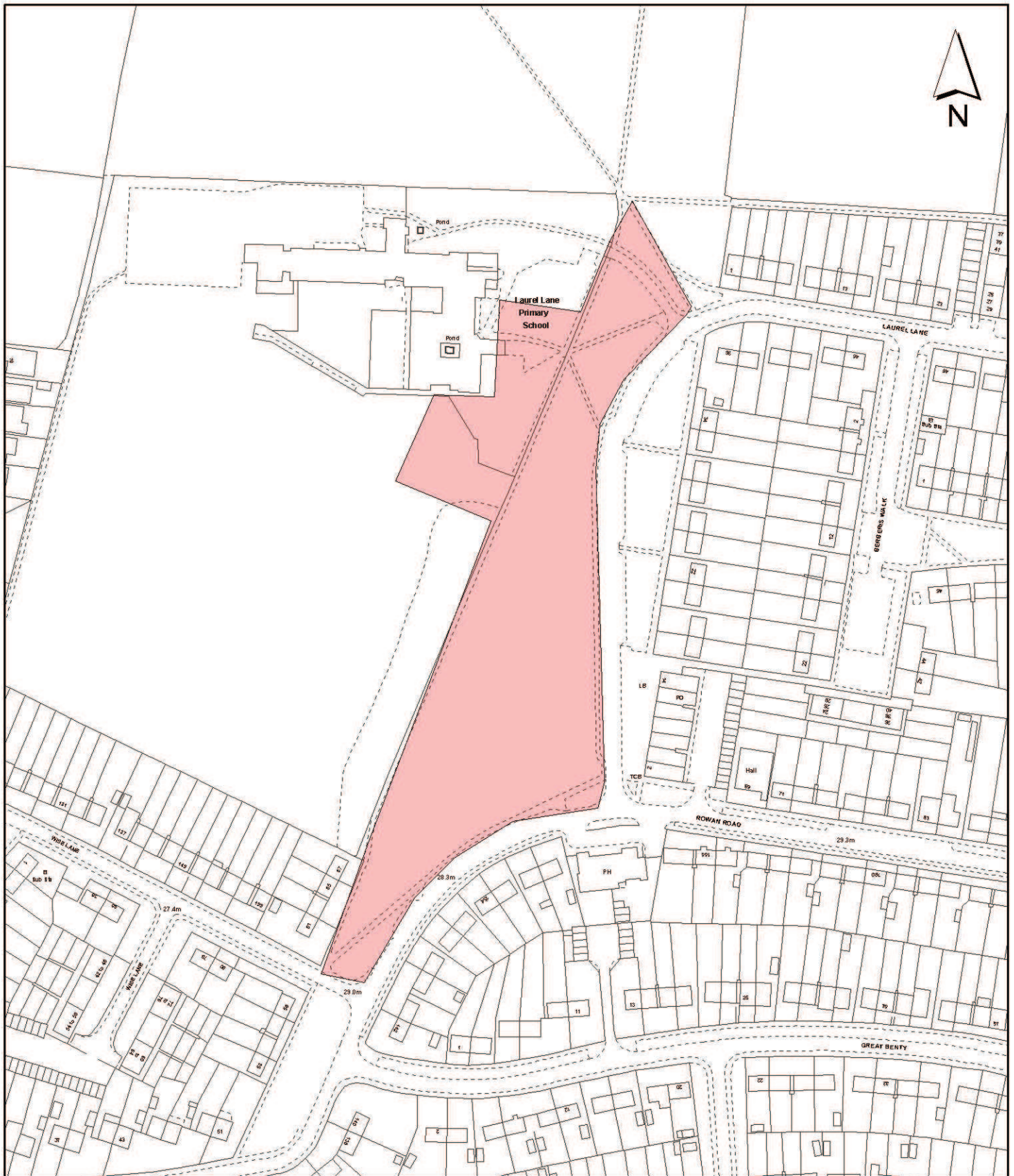
No.	Priority	Location	Tree Data	Date	Surveyed
A	Priority	Location	Tree Data	Date	Surveyed
B	Priority	Location	Tree Data	Date	Surveyed

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Project Name: Proposed New Junior School, Laurel Lane
 Site Address: Laurel Lane, Newham, London E8 3JN
 Date: 11/03/2019
 Scale: 1:500 (A1)
 Title: Tree Retention Plan
 Client: Z0008
 Drawing No: PL006
 Sheet: B





Notes

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Site Address

**Land To The West Of Laurel Lane
 West Drayton**

Planning Application Ref:
70019/APP/2014/1807

Planning Committee
Major Page 189

Scale
1:2,000

Date
August 2014

**LONDON BOROUGH
 OF HILLINGDON**

**Residents Services
 Planning Section**
 Civic Centre, Uxbridge, Middx. UB8 1UW
 Telephone No.: Uxbridge 250111



HILLINGDON
 LONDON

Report of the Head of Planning, Sport and Green Spaces

Address FORMER RAF EASTCOTE LIME GROVE RUISLIP

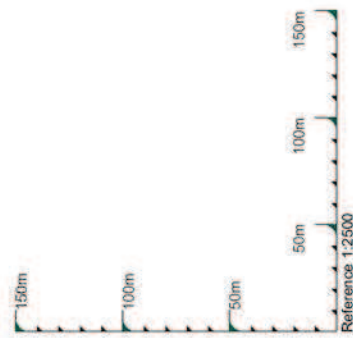
Development: Section 73 Amendment Application to vary the internal layout of the Community Centre, to retain pillars and subdivide the approved open plan layout (varying condition 11 of permission ref: 10189/APP/2007/3383 which approved the re-development of the former RAF Eastcote site).

LBH Ref Nos: 10189/APP/2014/1842

Date Plans Received: 28/05/2014

Date(s) of Amendment(s):

Date Application Valid: 02/06/2014



dha architecture ltd
 Brooklands Farm Business Park
 Botley Lane
 Birtfield
 Berkshire
 RG42 5OX

t. 0118 934 9666
 e. surveys@dhaarchitecture.co.uk
 w. www.dhaarchitecture.co.uk

raf eastcote

17.06.2014 (revised)
 preliminary
 000 @ AS
 JAH
 reference: 005585-WIM-WL-LOC100

rev: 001
 by: JAH



Ground Floor

Copyright © 2005 DHA Architecture Limited. All rights reserved. Limited reproduction and distribution permitted for the sole purpose of the planning of this named development only.
 Ordnance Survey data © Crown Copyright. All rights reserved. Licence number 100042153.

Do not scale - refer to figured dimensions only.

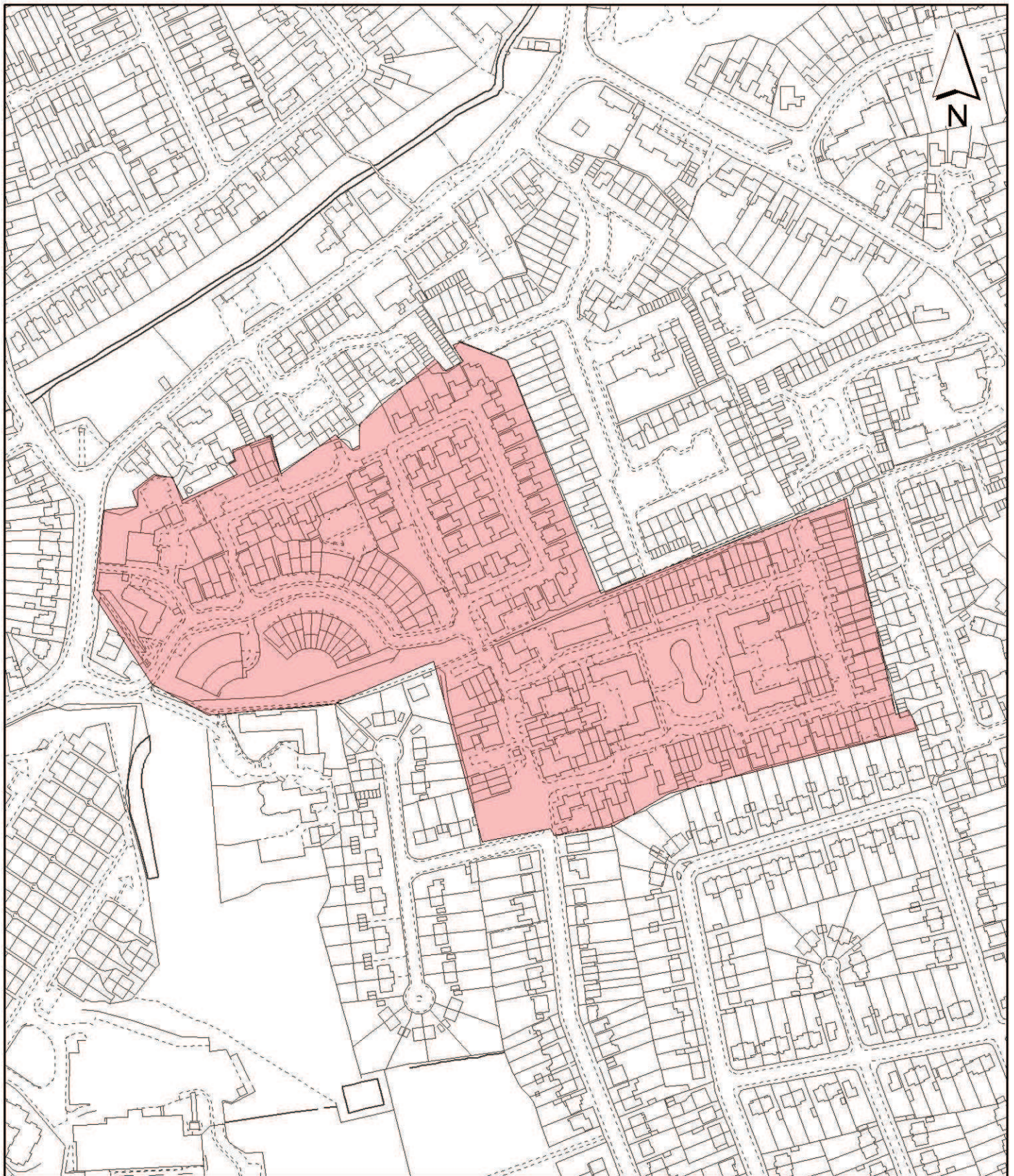
H 06/08/13 10/07 Community Centre redesign
 G 18/04/12 21/07 Revised floor plans + Community Centre
 F 27/06/08 01/08 Revised floor plans, provide location of hours display cabinet
 E 13/03/08 01/08 Revised floor plans, provide location of hours display cabinet
 D 13/03/08 01/08 Revised floor plans, provide location of hours display cabinet
 C 02/03/08 04/03 Community Centre redesign
 B 18/11/07 18/01 Revise design for wheelchair access, replace steel stairs to
 A 11/08/07 04/08 Approved to use Planning Commission - 1051 Standard
 Rev. Date By Description



RAF Eastcote

Apartment Block M
 Ground Floor Plan
 16.04.07
 1:100@A3
 5585/W/M/W/L/M/P1
 H

DHA Architecture Ltd
 The Black Barn, Spring Road
 Colchester, Essex, CO1 1JH
 Tel: 01206 481022 Fax: 01206 481071



Notes

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Site Address

**Former RAF Eastcote
Lime Grove
Ruislip**

Planning Application Ref:

10189/APP/2014/1842

Planning Committee

Major Page 193

Scale

1:3,200

Date

August 2014

**LONDON BOROUGH
OF HILLINGDON**

**Residents Services
Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW
Telephone No.: Uxbridge 250111



HILLINGDON
LONDON

Report of the Head of Planning, Sport and Green Spaces

Address ENTERPRISE HOUSE, 133 BLYTH ROAD HAYES

Development: Erection of extensions at roof level, erection of external bridge links on the rear elevation and internal works associated with the change of use of part of enterprise house to create 96 residential units (class C3) and associated car parking, retention of approximately 4,500 sqm (GIA) of employment use (Classes B1 and B8) at ground and first floor levels and cafe.

LBH Ref Nos: 11623/APP/2013/3606

Date Plans Received: 16/11/2013 **Date(s) of Amendment(s):** 25/06/2014
22/01/2014
09/04/2014
25/04/2014
03/04/2014
08/05/2014
05/12/2013

REVISION	ISSUE	DATE
P0	Pre-Planning Issue	28.07.13
P1	Updated to show parking	29.08.13
P2	Ballasts removed, Parking updated	12.08.13
P3	Ground floor updated, Generator added, Ground floor updated	16.10.13
P4	Ground floor updated	17.10.13
P5	Ground floor updated, including disabled car parking added, cycle storage altered	30.10.13
P6	Ground floor updated, disabled car parking removed	01.11.13
P7	Parking spaces updated	24.08.14



NOTE
 Only immediate surroundings have been recently surveyed. Other building information has been taken from Ordnance Survey map. Parking area to the north of the site to be surveyed before work commences on site.

E = Electric Car Charging Point

Notes
 1. All Dimensions To Be Checked On Site
 2. To Be Used In conjunction with Registered Drawings

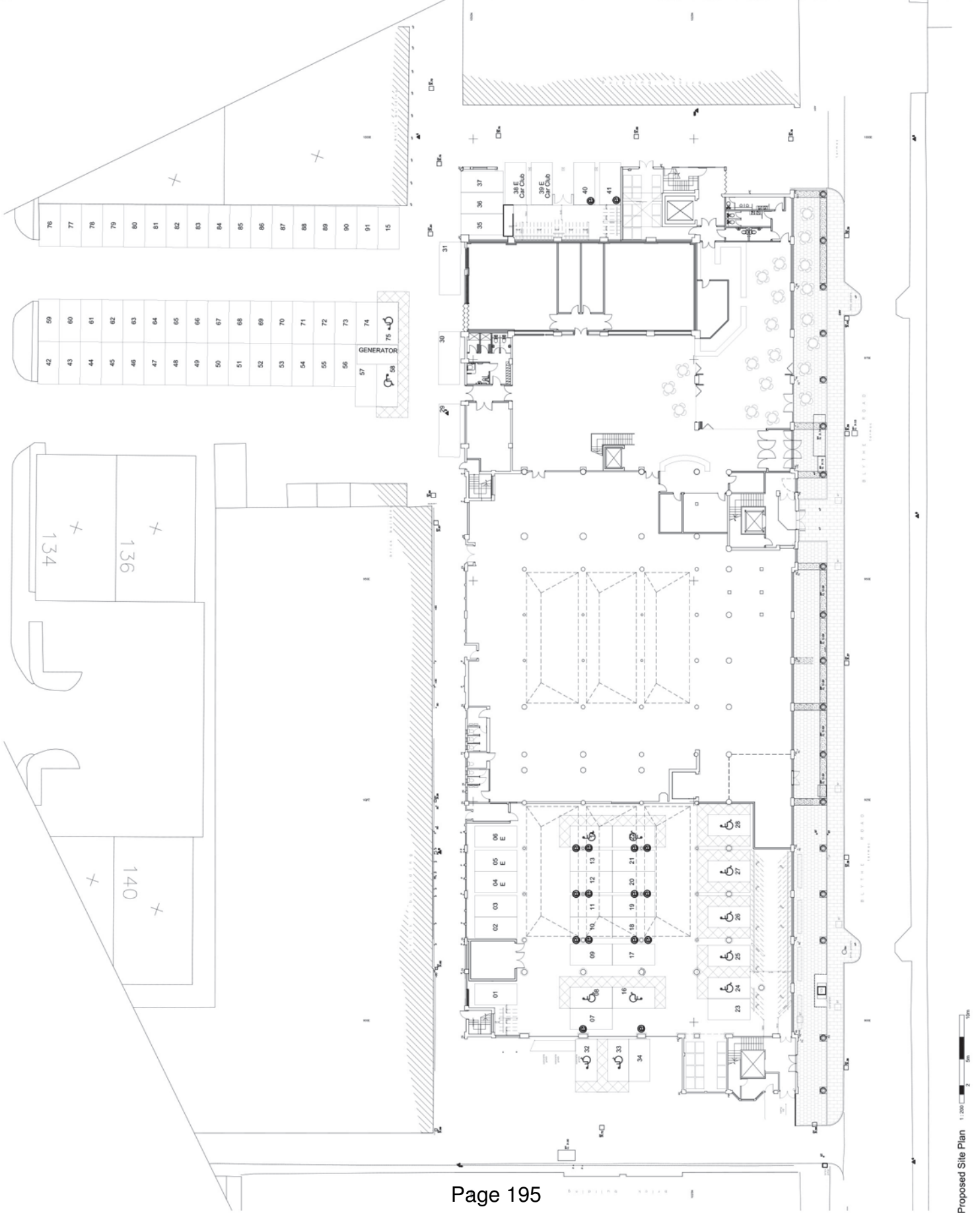
References
 1. Drawings
 2. Schedule

Hazards
 1.
 2.

Enterprise House
 Workspace Group Plc

FSP ARCHITECTS & PLANNERS
 22 BOSTON PLACE
 LONDON, NW1 6ER
 Tel: 020 7723 6099
 Fax: 020 7723 6098
 e-mail: info@fsp.co.uk
 www.fsp.co.uk

We warrant that this drawing, any discrepancies between drawings in this set of drawings and the contract documents shall be subject to the arbitration of the arbitrator.
 DRAWING STATUS - PLANNING
 DRAWING NUMBER: 524
 PROJECT TYPE: L
 ELEMENT: (-)
 NUMBER: 100
 REVISION: P7



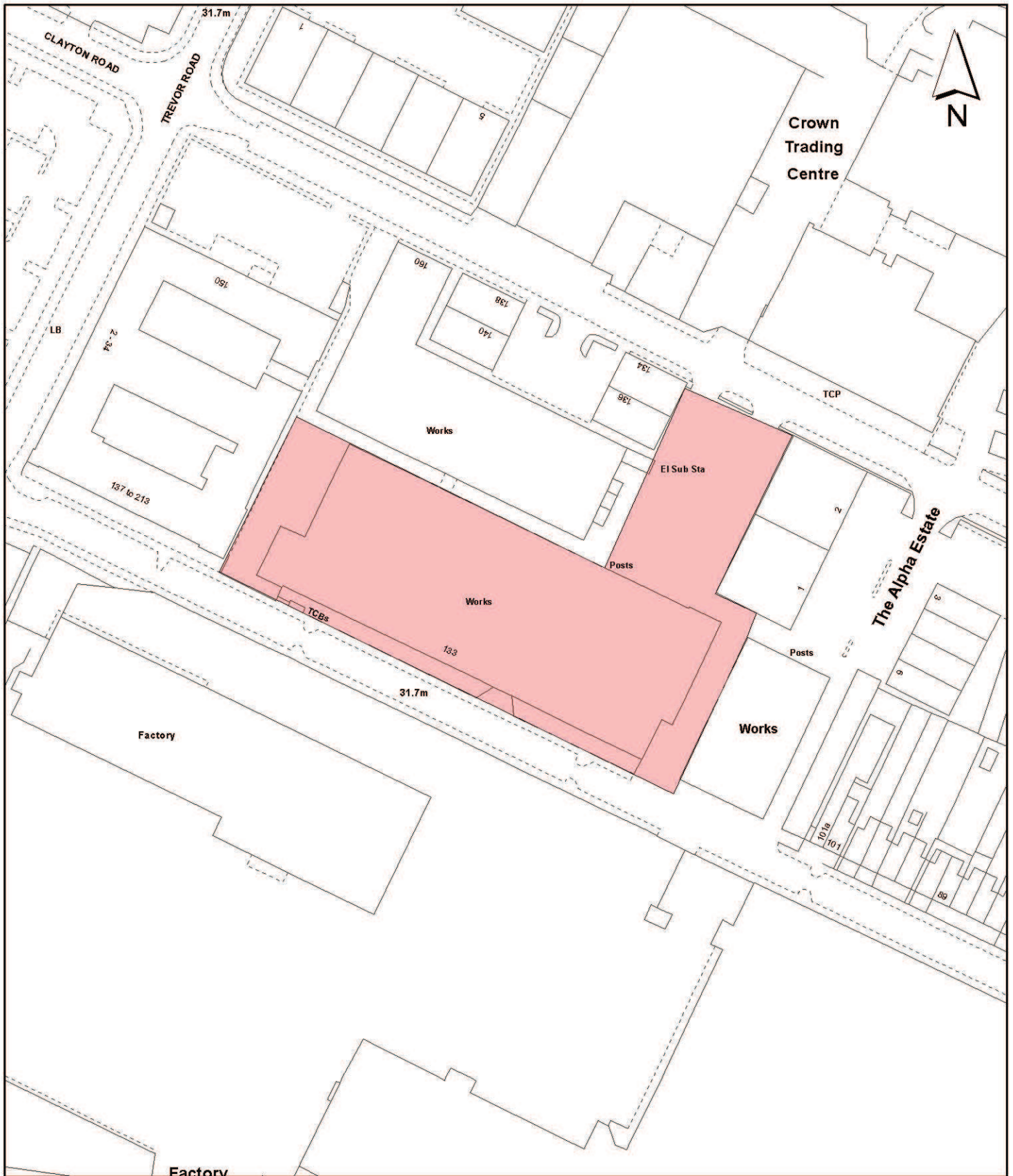
REV	DATE	BY	CHKD
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3	15/08/2014
4	15/08/2014
5	15/08/2014
6	15/08/2014
7	15/08/2014
8	15/08/2014
9	15/08/2014
10	15/08/2014



Notes	
1. All dimensions are to the center of the wall.	
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8. All dimensions are to the center of the wall.	
9. All dimensions are to the center of the wall.	
10. All dimensions are to the center of the wall.	

NOTES	NOTES
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9. All dimensions are to the center of the wall.	9. All dimensions are to the center of the wall.
10. All dimensions are to the center of the wall.	10. All dimensions are to the center of the wall.

KEY	Window Detail
RESIDENTIAL ACCOMMODATION	Window Detail
BUSINESS ACCOMMODATION	Window Detail
KEY - Door Renovation	FD - Window Detail
1. Replacement door with existing frame.	1. Window Detail
2. Replacement door with new frame.	2. Window Detail
3. Replacement door with new frame and sill.	3. Window Detail
4. Replacement door with new frame and sill and lintel.	4. Window Detail
5. Replacement door with new frame and sill and lintel and head.	5. Window Detail
6. Replacement door with new frame and sill and lintel and head and jamb.	6. Window Detail
7. Replacement door with new frame and sill and lintel and head and jamb and stop.	7. Window Detail
8. Replacement door with new frame and sill and lintel and head and jamb and stop and weatherstripping.	8. Window Detail
9. Replacement door with new frame and sill and lintel and head and jamb and stop and weatherstripping and paint.	9. Window Detail
10. Replacement door with new frame and sill and lintel and head and jamb and stop and weatherstripping and paint and hardware.	10. Window Detail



<p>Notes</p> <p> Site boundary</p> <p>For identification purposes only.</p> <p>This copy has been made by or with the authority of the Head of Committee Services pursuant to section 47 of the Copyright, Designs and Patents Act 1988 (the Act). Unless the Act provides a relevant exception to copyright.</p> <p>© Crown copyright and database rights 2014 Ordnance Survey 100019283</p>	<p>Site Address</p> <p align="center">Enterprise House 133 Blyth Road Hayes</p>		<p align="center">LONDON BOROUGH OF HILLINGDON</p> <p align="center">Residents Services Planning Section</p> <p align="center">Civic Centre, Uxbridge, Middx. UB8 1UW Telephone No.: Uxbridge 250111</p>
	<p>Planning Application Ref:</p> <p align="center">11623/APP/2013/3606</p>	<p>Scale</p> <p align="center">1:1,250</p>	
	<p>Planning Committee</p> <p align="center">Major Page 205</p>	<p>Date</p> <p align="center">August 2014</p>	
		 HILLINGDON LONDON	

Report of the Head of Planning, Sport and Green Spaces

Address ENTERPRISE HOUSE, 133 BLYTH ROAD HAYES

Development: Erection of extensions at roof level, erection of external bridge links on the rear elevation and internal works associated with the change of use of part of enterprise house to create 96 residential units (class C3) and associated car parking, retention of approximately 4,500 sqm (GIA) of employment use (Classes B1 and B8) at ground and first floor levels and cafe. (Application for Listed Building Consent)

LBH Ref Nos: 11623/APP/2013/3592

Date Plans Received: 16/11/2013

Date(s) of Amendment(s):

Date Application Valid: 21/01/2014

REVISION	ISSUE	DATE
P0	Pre-Planning Issue	28.07.13
P1	Updated to show parking	29.08.13
P2	Basements removed, Parking updated	12.08.13
P3	Basement removed, Generator added, Ground floor updated	16.10.13
P4	Ground floor updated	17.10.13
P5	Ground floor updated, including disabled car parking added, cycle storage added	30.10.13
P6	Basement removed, cycle storage removed	04.11.13
P7	Parking spaces updated	24.08.14



NOTE
 Only immediate surroundings have been recently surveyed. Other building information has been taken from Ordnance Survey map. Parking area to the north of the site to be surveyed before work commences on site.

E = Electric Car Charging Point

Notes
 1. All Dimensions To Be Checked On Site
 2. To Be Used In conjunction with Registered Drawings

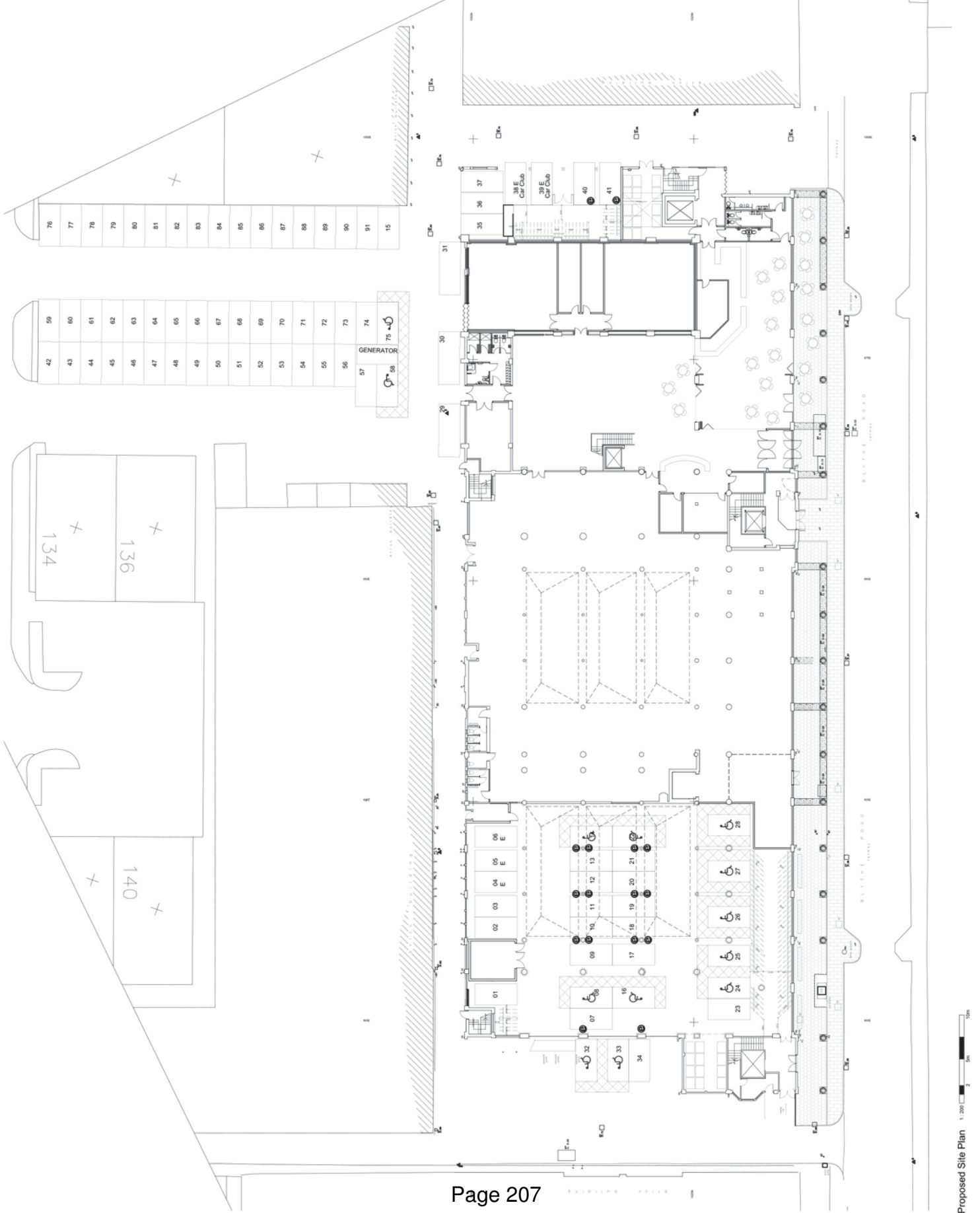
References
 1. Drawings
 2. Schedule

Hazards
 1.
 2.

**Enterprise House
 Workspace Group Plc**

FSP ARCHITECTS & PLANNERS
 22 BOSTON PLACE
 LONDON, NW1 6ER
 Tel: 020 7723 6099
 Fax: 020 7723 6098
 e-mail: info@fsp.co.uk
 www.fsp.co.uk

We warrant that this drawing, any discrepancies between drawings in this set of drawings and the contract documents shall be subject to the arbitration of the arbitrator.
 DRAWING STATUS - PLANNING
 DRAWING NUMBER: 524
 PROJECT TYPE: L
 ELEMENT: (-)
 NUMBER: 100
 REVISION: P7



NO.	DATE
01	10/11/2023
02	10/11/2023
03	10/11/2023
04	10/11/2023
05	10/11/2023
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Typical Ping Floor Plan.

Window Detail

Window Detail: Indicate window location and type. Use the following symbols for window types: Single Glazed, Double Glazed, etc.

Window Detail: Indicate window location and type. Use the following symbols for window types: Single Glazed, Double Glazed, etc.

KEY

RESIDENTIAL ACCOMMODATION

RESIDENTIAL ACCOMMODATION

KEY - Door Renovation

1. Renovation of door and frame

2. Replacement of door and frame

3. Replacement of door and frame

4. Replacement of door and frame

5. Replacement of door and frame

6. Replacement of door and frame

7. Replacement of door and frame

8. Replacement of door and frame

9. Replacement of door and frame

10. Replacement of door and frame

NOTES

1. All work to be done in accordance with the contract documents and the relevant building codes.

2. The contractor shall be responsible for obtaining all necessary permits and approvals.

3. The contractor shall ensure that all work is completed in a timely and efficient manner.

4. The contractor shall maintain the site in a clean and safe condition at all times.

5. The contractor shall provide regular progress reports to the architect.

6. The contractor shall be responsible for the removal and disposal of all waste materials.

7. The contractor shall ensure that all work is completed to the satisfaction of the architect.

8. The contractor shall provide a final inspection report upon completion of the work.

9. The contractor shall be responsible for the protection of all existing structures and services.

10. The contractor shall ensure that all work is completed in accordance with the contract documents.

Notes

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7. The contractor shall ensure that all work is completed to the satisfaction of the architect.

8. The contractor shall provide a final inspection report upon completion of the work.

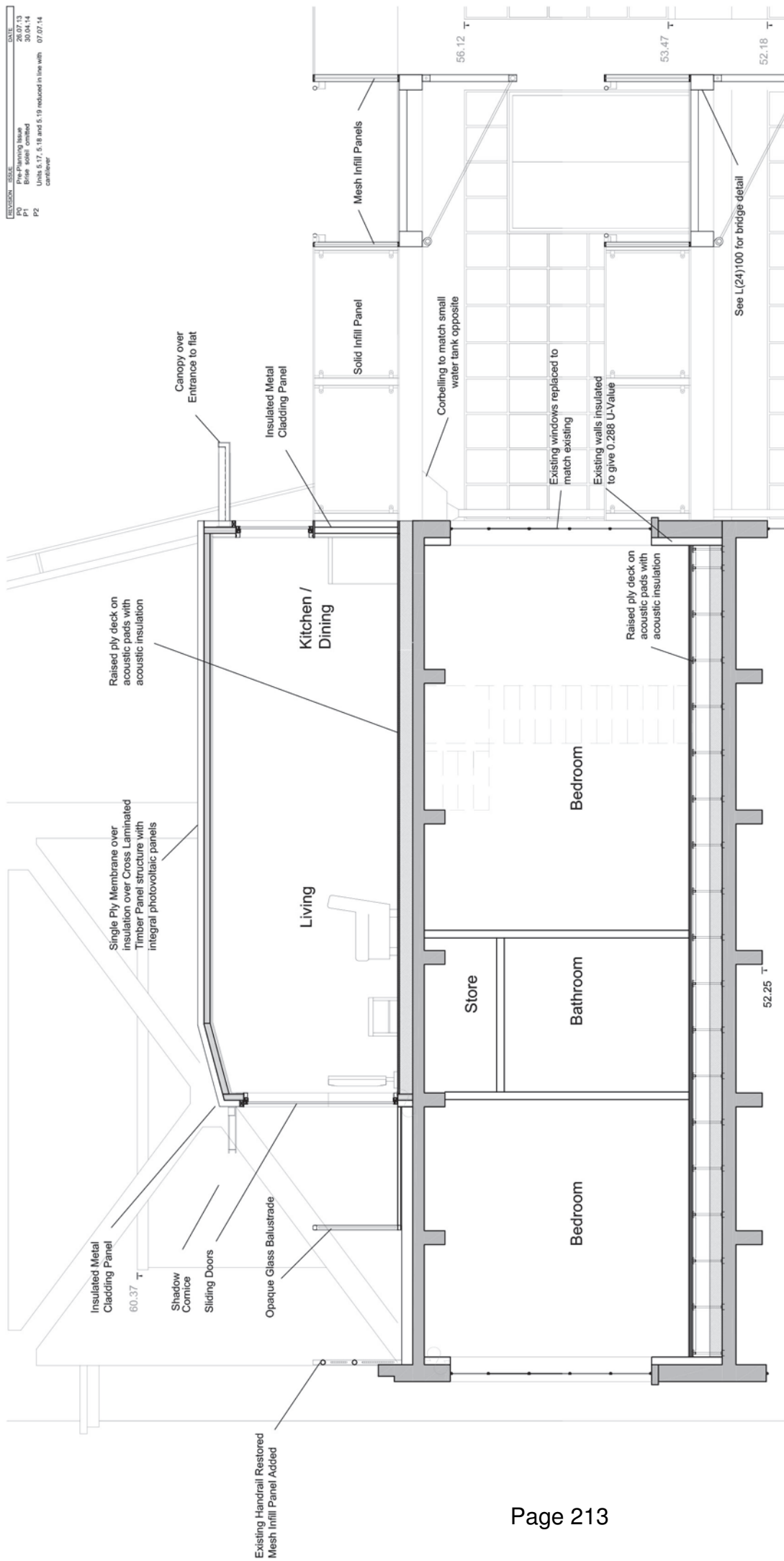
9. The contractor shall be responsible for the protection of all existing structures and services.

10. The contractor shall ensure that all work is completed in accordance with the contract documents.

Enterprise House
Workspace Group Plc
FSP ARCHITECTS & PLANNERS

102 L 102 PS

REVISION	ISSUE	DATE
P1	Initial design Basic wall outline	30.04.14
P2	Units 5, 17, 5, 18 and 5, 19 reduced in line with cantilever	07.07.14



524 L(--)-600.01 Proposed Flat Roof Penthouse Section



Notes

1. All Dimensions To Be Checked On Site
 2. Architect to be notified if existing is damaged or in need of repair

References

1. Drawings
2. Specification
3. Schedules

Hazards

- 1.
- 2.
- 3.

Enterprise House Workspace Group Pic

DRAWING STATUS :- PLANNING		CHECKED	MF
TITLE: FLAT ROOF PENTHOUSE SECTION		CW	MF
SCALE		1:50	
DRAWING NUMBER		REVISION	
PROJECT TYPE	ELEMENT	NUMBER	P2
524 L	(--)	600	



FSP ARCHITECTS & PLANNERS
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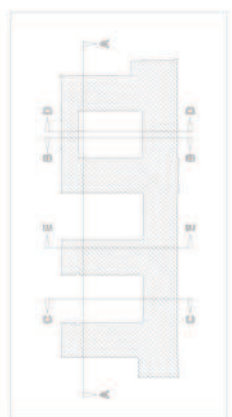
do not scale from this drawing. any discrepancies between drawings or between drawings and actual site conditions should immediately be brought to the attention of the architects.

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20	2019.11.13



2019.11.13

524 Lj-2019.01 Proposed Long Section A scale 1:100



Notes

- 1. All dimensions are in millimeters.
- 2. All dimensions are to the centerline of the member unless otherwise stated.

Enterprise House
 Workspace Group Pte
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 11, ROBINSON ROAD
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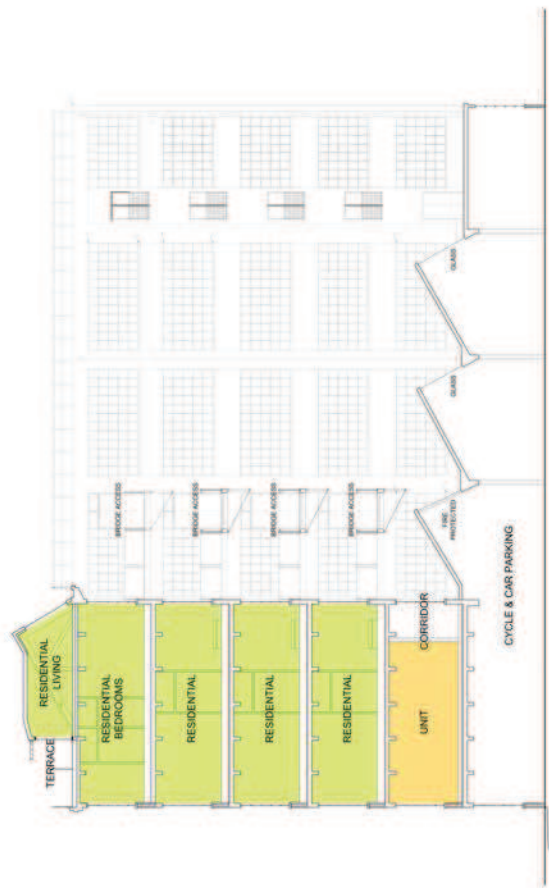
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Notes
 1. To be read in conjunction with the Contract Documents.
 2. All dimensions are to the face of the work unless otherwise stated.
 3. All work to be in accordance with the current editions of the Building Regulations.
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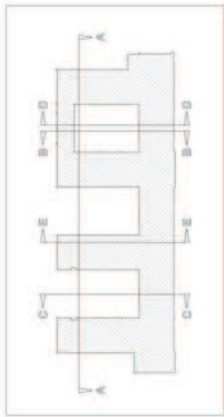
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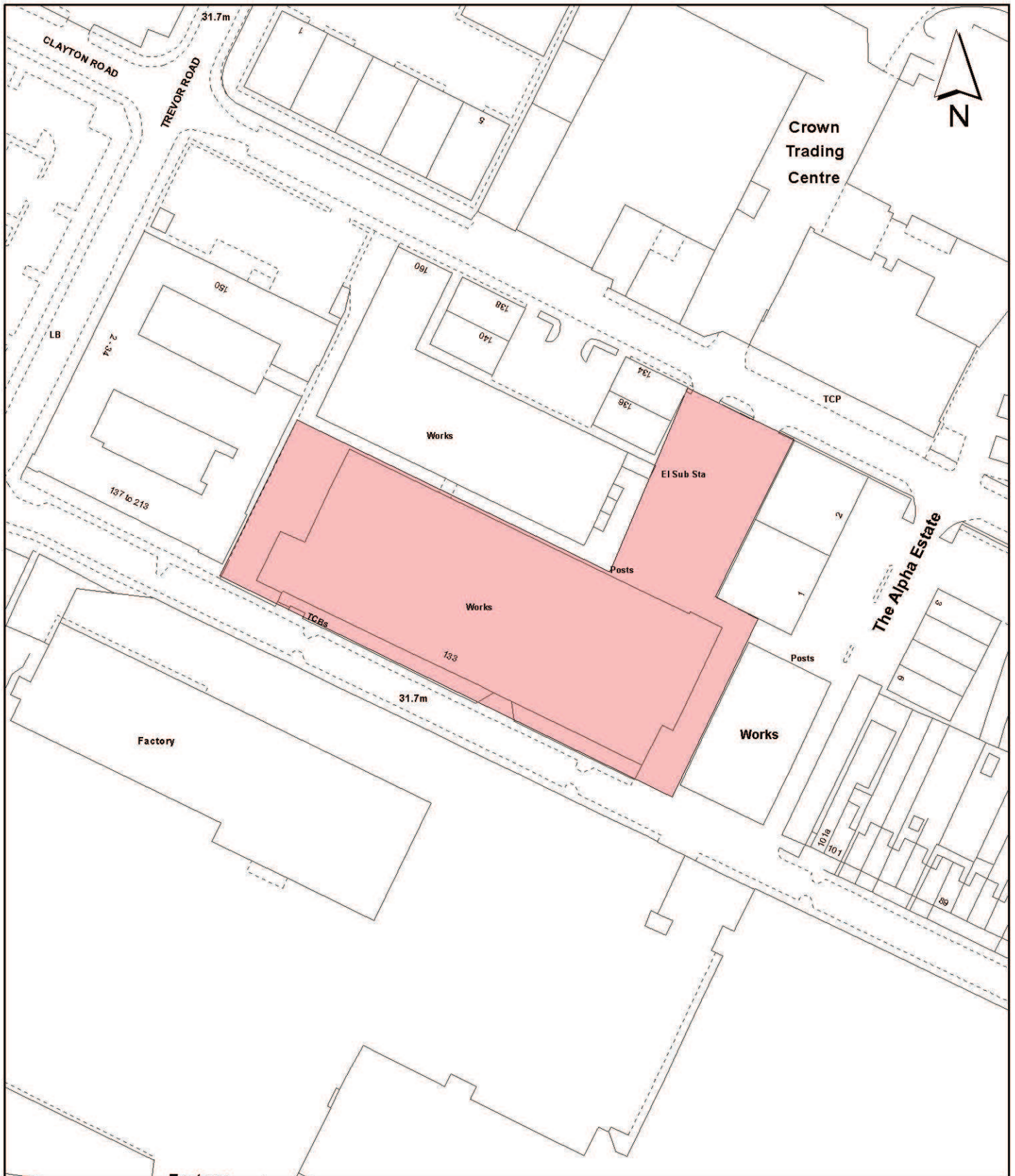



524 LL-201.02 Proposed Cross Section C scale 1:100



524 LL-201.01 Proposed Cross Section B scale 1:100





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	<p>Planning Application Ref:</p> <p align="center">11623/APP/2013/3592</p>	<p>Scale</p> <p align="center">1:1,250</p>	
	<p>Planning Committee</p> <p align="center">Major Page 220</p>	<p>Date</p> <p align="center">August 2014</p>	



HILLINGDON
LONDON

Report of the Head of Planning, Sport and Green Spaces

Address 5 STATION ROAD WEST DRAYTON

Development: Demolition of existing public house and erection of 38 flats and 237sqm of retail floorspace with parking, landscaping and amenity space AMENDED PLANS RECEIVED

LBH Ref Nos: 65480/APP/2014/1018

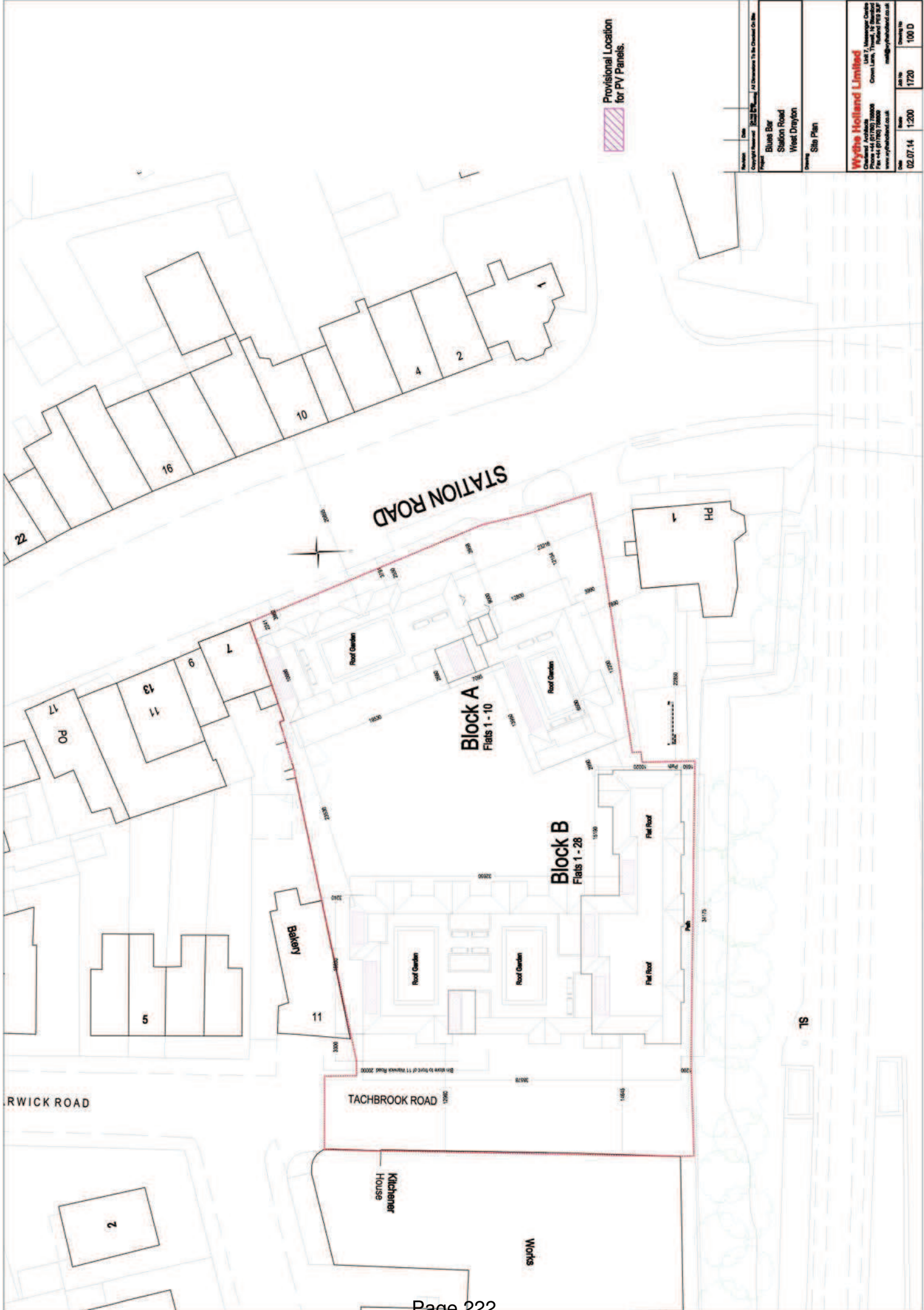
Date Plans Received: 24/03/2014

Date(s) of Amendment(s):

03/06/2014

Date Application Valid: 24/03/2014

24/03/2014



Provisional Location
for PV Panels.

Revision	Date	Drawn by	Checked by
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Project Blue Bar Station Road West Drayton			
Drawing Site Plan			
Wythe Holland Limited Limited Architects 14th Floor, 15 Broad Street Bristol, BS1 2TA Phone: +44 (0)117 206000 Fax: +44 (0)117 206000 www.wytheholland.co.uk			
Date	Scale	Job No	Drawing No
02.07.14	1:200	1720	100 D

STATION ROAD



Retail
237 sqm

36 spaces total

Bakery

Way Through

11

Cycles 10 No.

Security Gate

Entrance

Bins Refuse

6

Entrance

3

5

4

Path

Path

collection

area

Bin Store

lift

Lobby

Staff Area

Shelf Area

Bin Store

lift

Lobby

Staff Area

Shelf Area

Bin Store

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Lobby

Staff Area

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Revision	Date	Drawn by	Checked by
01	24.07.14	1:100	1720
Copyright Material - Review Only. Not for Redistribution. All Dimensions To Be Checked On Site. Project: Blue Bar Station Road West Drayton Drawing: Ground Floor Plan			
Wythe Holland Limited 1st Floor, 100, The Quadrant, West Drayton, Middlesex, UK Phone: +44 (0)1895 780000 Fax: +44 (0)1895 78000 www.wytheholland.co.uk			
Date	24.07.14	Scale	1:100
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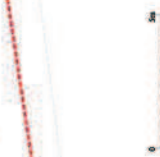


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Project	Blue Bar Station Road West Drayton		
Drawing	Ground Floor Plan		
Wythe Holliday Limited 14th Floor, 15th Floor, 16th Floor 100, 101, 102, 103, 104, 105, 106, 107, 108, 109, 110, 111, 112, 113, 114, 115, 116, 117, 118, 119, 120, 121, 122, 123, 124, 125, 126, 127, 128, 129, 130, 131, 132, 133, 134, 135, 136, 137, 138, 139, 140, 141, 142, 143, 144, 145, 146, 147, 148, 149, 150, 151, 152, 153, 154, 155, 156, 157, 158, 159, 160, 161, 162, 163, 164, 165, 166, 167, 168, 169, 170, 171, 172, 173, 174, 175, 176, 177, 178, 179, 180, 181, 182, 183, 184, 185, 186, 187, 188, 189, 190, 191, 192, 193, 194, 195, 196, 197, 198, 199, 200, 201, 202, 203, 204, 205, 206, 207, 208, 209, 210, 211, 212, 213, 214, 215, 216, 217, 218, 219, 220, 221, 222, 223, 224, 225, 226, 227, 228, 229, 230, 231, 232, 233, 234, 235, 236, 237, 238, 239, 240, 241, 242, 243, 244, 245, 246, 247, 248, 249, 250, 251, 252, 253, 254, 255, 256, 257, 258, 259, 260, 261, 262, 263, 264, 265, 266, 267, 268, 269, 270, 271, 272, 273, 274, 275, 276, 277, 278, 279, 280, 281, 282, 283, 284, 285, 286, 287, 288, 289, 290, 291, 292, 293, 294, 295, 296, 297, 298, 299, 300, 301, 302, 303, 304, 305, 306, 307, 308, 309, 310, 311, 312, 313, 314, 315, 316, 317, 318, 319, 320, 321, 322, 323, 324, 325, 326, 327, 328, 329, 330, 331, 332, 333, 334, 335, 336, 337, 338, 339, 340, 341, 342, 343, 344, 345, 346, 347, 348, 349, 350, 351, 352, 353, 354, 355, 356, 357, 358, 359, 360, 361, 362, 363, 364, 365, 366, 367, 368, 369, 370, 371, 372, 373, 374, 375, 376, 377, 378, 379, 380, 381, 382, 383, 384, 385, 386, 387, 388, 389, 390, 391, 392, 393, 394, 395, 396, 397, 398, 399, 400, 401, 402, 403, 404, 405, 406, 407, 408, 409, 410, 411, 412, 413, 414, 415, 416, 417, 418, 419, 420, 421, 422, 423, 424, 425, 426, 427, 428, 429, 430, 431, 432, 433, 434, 435, 436, 437, 438, 439, 440, 441, 442, 443, 444, 445, 446, 447, 448, 449, 450, 451, 452, 453, 454, 455, 456, 457, 458, 459, 460, 461, 462, 463, 464, 465, 466, 467, 468, 469, 470, 471, 472, 473, 474, 475, 476, 477, 478, 479, 480, 481, 482, 483, 484, 485, 486, 487, 488, 489, 490, 491, 492, 493, 494, 495, 496, 497, 498, 499, 500, 501, 502, 503, 504, 505, 506, 507, 508, 509, 510, 511, 512, 513, 514, 515, 516, 517, 518, 519, 520, 521, 522, 523, 524, 525, 526, 527, 528, 529, 530, 531, 532, 533, 534, 535, 536, 537, 538, 539, 540, 541, 542, 543, 544, 545, 546, 547, 548, 549, 550, 551, 552, 553, 554, 555, 556, 557, 558, 559, 560, 561, 562, 563, 564, 565, 566, 567, 568, 569, 570, 571, 572, 573, 574, 575, 576, 577, 578, 579, 580, 581, 582, 583, 584, 585, 586, 587, 588, 589, 590, 591, 592, 593, 594, 595, 596, 597, 598, 599, 600, 601, 602, 603, 604, 605, 606, 607, 608, 609, 610, 611, 612, 613, 614, 615, 616, 617, 618, 619, 620, 621, 622, 623, 624, 625, 626, 627, 628, 629, 630, 631, 632, 633, 634, 635, 636, 637, 638, 639, 640, 641, 642, 643, 644, 645, 646, 647, 648, 649, 650, 651, 652, 653, 654, 655, 656, 657, 658, 659, 660, 661, 662, 663, 664, 665, 666, 667, 668, 669, 670, 671, 672, 673, 674, 675, 676, 677, 678, 679, 680, 681, 682, 683, 684, 685, 686, 687, 688, 689, 690, 691, 692, 693, 694, 695, 696, 697, 698, 699, 700, 701, 702, 703, 704, 705, 706, 707, 708, 709, 710, 711, 712, 713, 714, 715, 716, 717, 718, 719, 720, 721, 722, 723, 724, 725, 726, 727, 728, 729, 730, 731, 732, 733, 734, 735, 736, 737, 738, 739, 740, 741, 742, 743, 744, 745, 746, 747, 748, 749, 750, 751, 752, 753, 754, 755, 756, 757, 758, 759, 760, 761, 762, 763, 764, 765, 766, 767, 768, 769, 770, 771, 772, 773, 774, 775, 776, 777, 778, 779, 780, 781, 782, 783, 784, 785, 786, 787, 788, 789, 790, 791, 792, 793, 794, 795, 796, 797, 798, 799, 800, 801, 802, 803, 804, 805, 806, 807, 808, 809, 810, 811, 812, 813, 814, 815, 816, 817, 818, 819, 820, 821, 822, 823, 824, 825, 826, 827, 828, 829, 830, 831, 832, 833, 834, 835, 836, 837, 838, 839, 840, 841, 842, 843, 844, 845, 846, 847, 848, 849, 850, 851, 852, 853, 854, 855, 856, 857, 858, 859, 860, 861, 862, 863, 864, 865, 866, 867, 868, 869, 870, 871, 872, 873, 874, 875, 876, 877, 878, 879, 880, 881, 882, 883, 884, 885, 886, 887, 888, 889, 890, 891, 892, 893, 894, 895, 896, 897, 898, 899, 900, 901, 902, 903, 904, 905, 906, 907, 908, 909, 910, 911, 912, 913, 914, 915, 916, 917, 918, 919, 920, 921, 922, 923, 924, 925, 926, 927, 928, 929, 930, 931, 932, 933, 934, 935, 936, 937, 938, 939, 940, 941, 942, 943, 944, 945, 946, 947, 948, 949, 950, 951, 952, 953, 954, 955, 956, 957, 958, 959, 960, 961, 962, 963, 964, 965, 966, 967, 968, 969, 970, 971, 972, 973, 974, 975, 976, 977, 978, 979, 980, 981, 982, 983, 984, 985, 986, 987, 988, 989, 990, 991, 992, 993, 994, 995, 996, 997, 998, 999, 1000.			
Date	24.07.14	Scale	1:200
Job No	1720	Drawing No	114 C

STATION ROAD



Revision	Date	Client	Project
		Blue Bar	Station Road
			West Drayton
			First Floor Plan
<p>Wythe Holland Limited Chartered Architects Ltd 7, Mansinger Centre Crown Lane, Trowell, N. Stafford Staffs ST16 3BP Tel: +44 (0)1827 70000 Fax: +44 (0)1827 70000 www.wytheholland.co.uk</p>			
Date	Scale	Job No	Drawing No
02.07.14	1:100	1720	102 C



STATION ROAD



Revision	Date	Client	Project
		Blue Bar	Station Road
			West Drayton
			Second Floor Plan
<p>Wythe Holliday Limited Chartered Architects 144-147 (01753) 700000 Fax: +44 (0)1753 700009 www.wytheholliday.co.uk</p>			
Date	Scale	Job No	Drawing No
02.07.14	1:100	1720	103 C

STATION ROAD

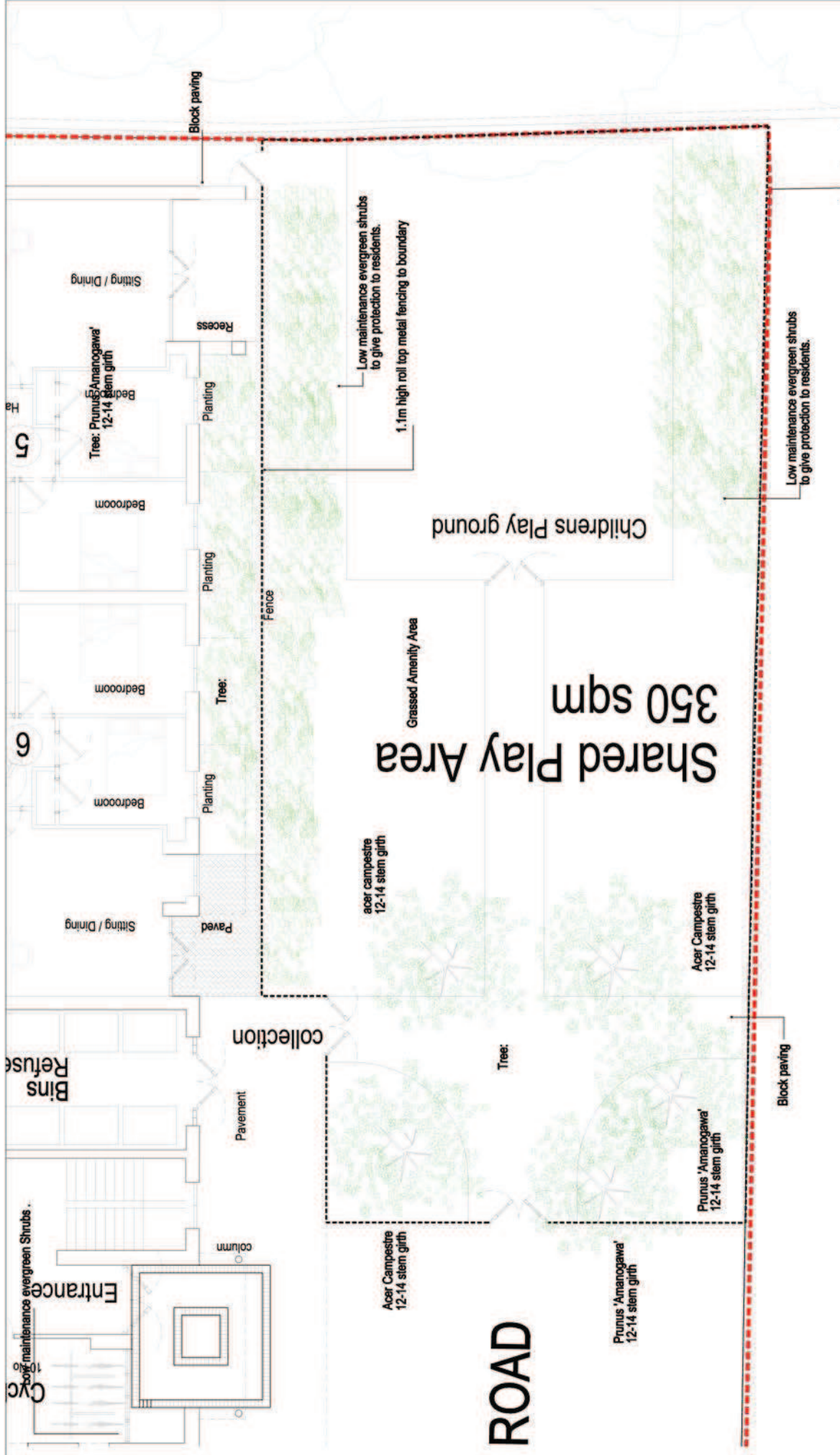


Revision	Date	Client	Project	Drawn by	Checked by	Scale	Date	Job No	Drawing No
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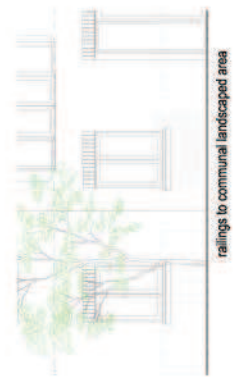
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Wythe Holland Limited
 Chartered Architects Ltd 7, Mansergh Close
 Crown Lane, Trowell, N. Bedfordshire
 Bedfordshire MK43 9BP
 Tel: +44 (0)1753 78888
 Fax: +44 (0)1753 78889
 www.wytheholland.co.uk





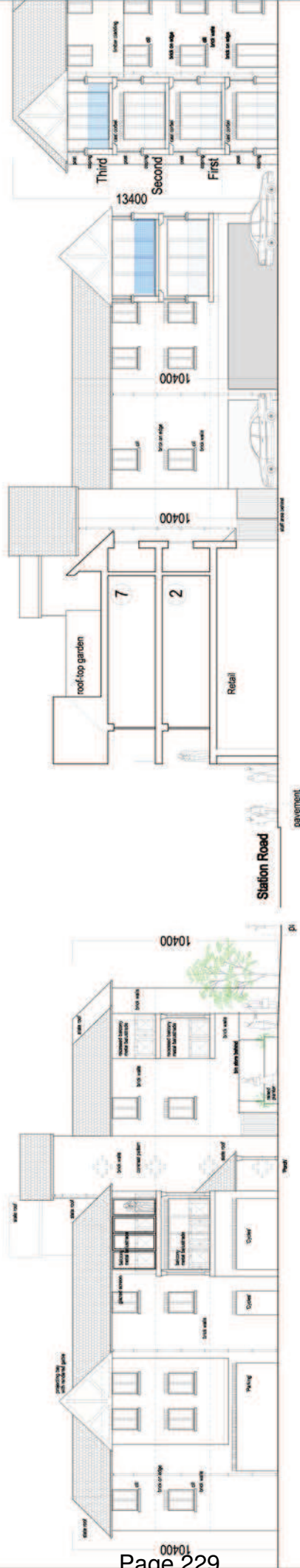
Revision	Date	Author
Copyright Information	Scale	As Drawn
Project: Blue Bar Station Road West Drayton		
Drawing: Shared Courtyard Landscaping		
Wythe Holland Limited Unit 7, Millington Close Crown Lane, Thurst, N. Stafford Staffs ST16 3BP Tel: +44 (0)1902 788808 Fax: +44 (0)1902 788809 www.wytheholland.co.uk		
Date	Issue	Drawing No
30.08.14	1:50	1720
		108 C





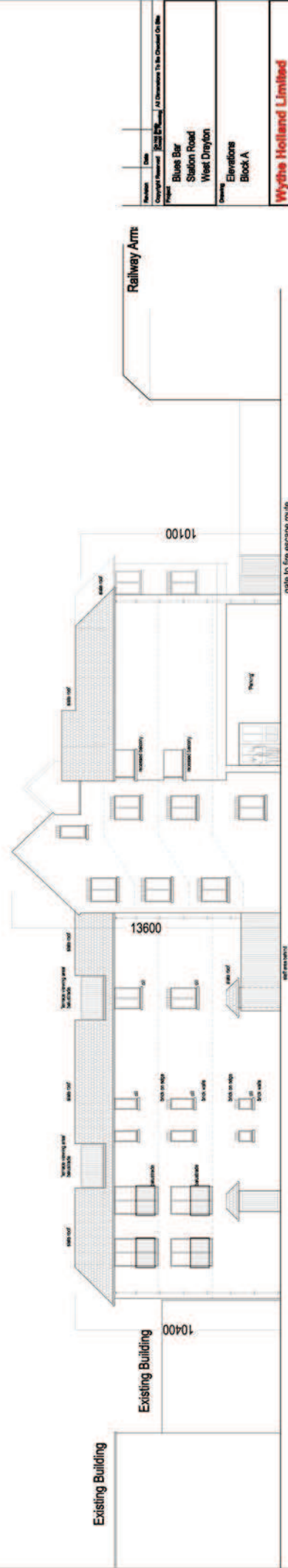
West Elevation to Station Road

datum 25.000



North Elevation

Section - South Elevation



East Elevation to Courtyard

datum 25.000

Revision	Date	Drawn by	Checked by
Completed	24.07.14	1720	105 D
Project: Blue Bar Station Road West Drayton			
Drawing: Elevations Block A			
Wythe Holliday Limited Limited Architects Phone: +44 (0)1703 708008 Fax: +44 (0)1703 708009 www.wytheholliday.co.uk			
Unit 7, Magazine Gate Crown Lane, Trowel, N. Stafford National P13 3AP Staffordshire			



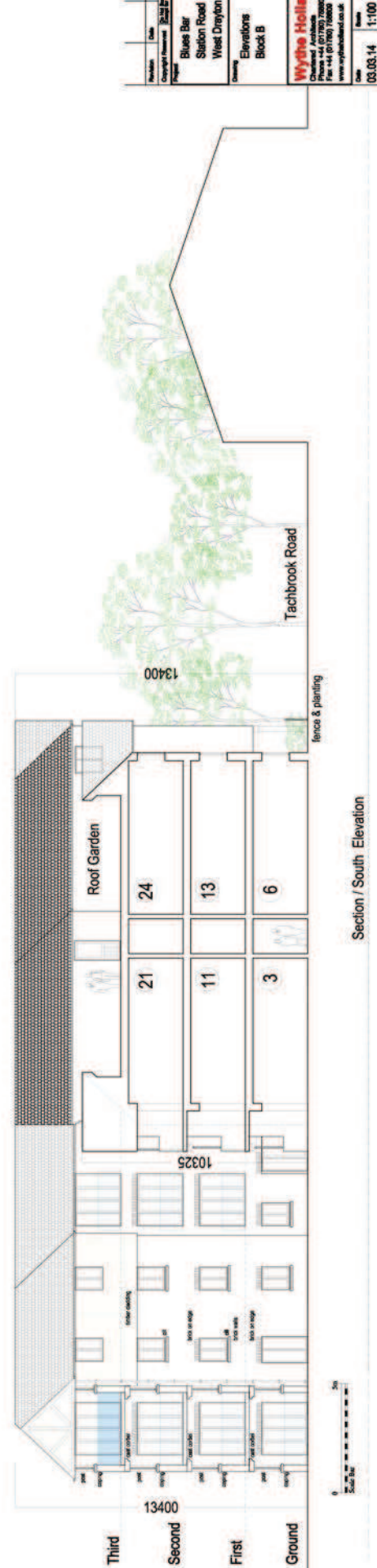
South Elevation

datum 25,000



Courtyard Section / West Elevation

datum 25,000



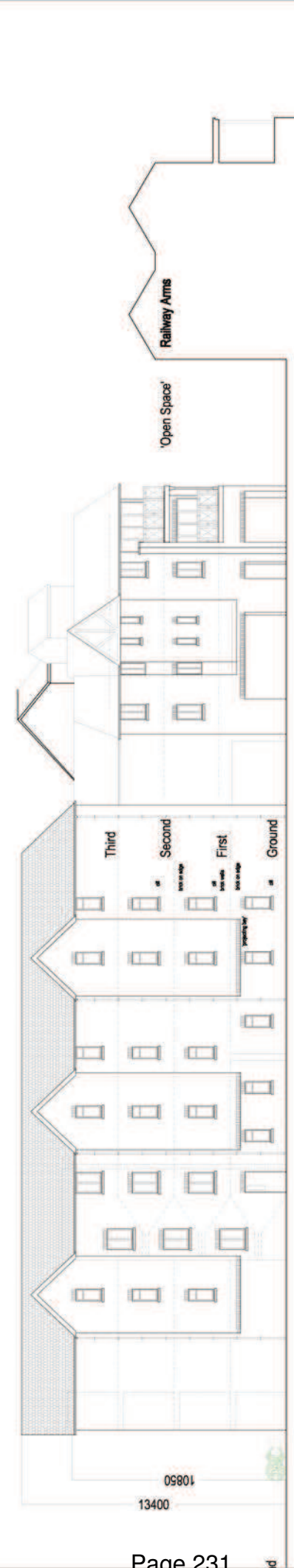
Section / South Elevation

Revision	Date	Drawn by	Checked by
1	03.03.14	1:100	1720
Copyright Reserved. All Rights Reserved. All Dimensions To Be Checked On Site		Blues Bar Station Road West Drayton	
Drawing		Elevations Block B	
Wythe Holland Limited 1st Floor, Millers Close Green Lane, Thame, Oxfordshire Phone +44 (0)1780 728200 Fax +44 (0)1780 728209 www.wytheholland.co.uk		Job No 107 B Drawing No 107 B	



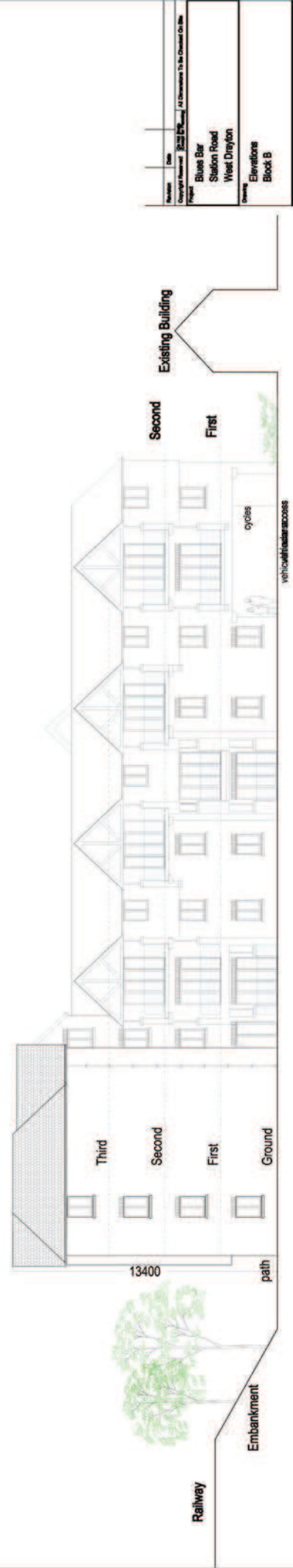
East Elevation to Tachbrook Road

datum 25,000



North Elevation to Railway Station

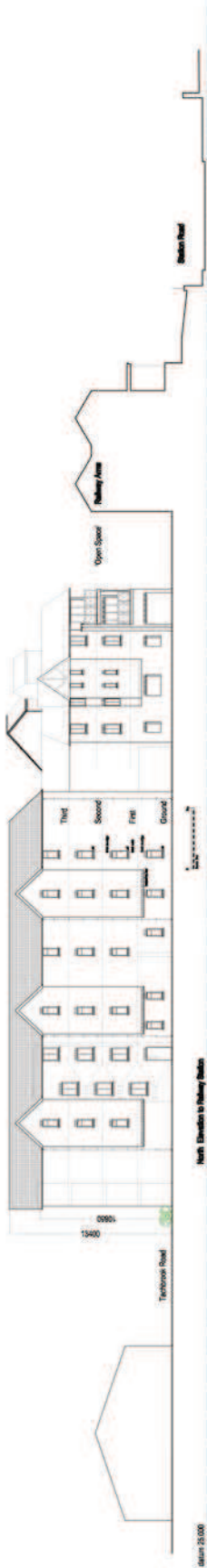
datum 25,000



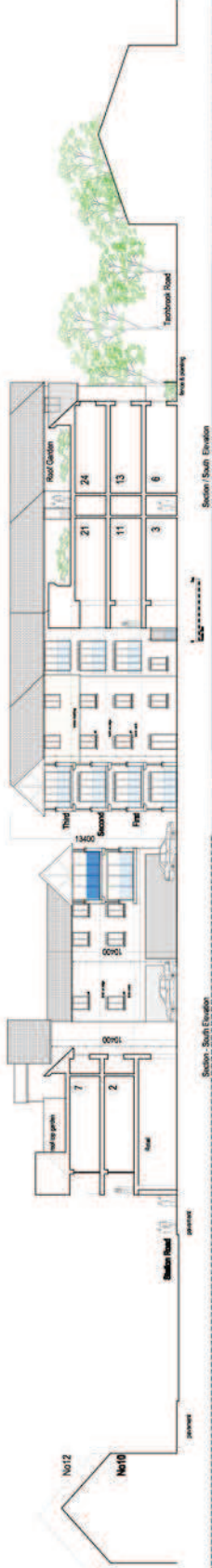
West Elevation

datum 25,000

Revision	Date	Client	Project	Drawings
		Blue Bar	Station Road	Elevations
			West Drayton	Block B
<p>Wythe Holland Limited Chartered Architects Ltd 7, Mansinger Close Crown Lane, Trowell, N. Bedford Bedfordshire MK43 9JP Tel: +44 (0)1753 70000 Fax: +44 (0)1753 70000 www.wytheholland.co.uk</p>				
Date	Scale	Job No	Drawing No	
24.07.14	1:100	1720	106 E	

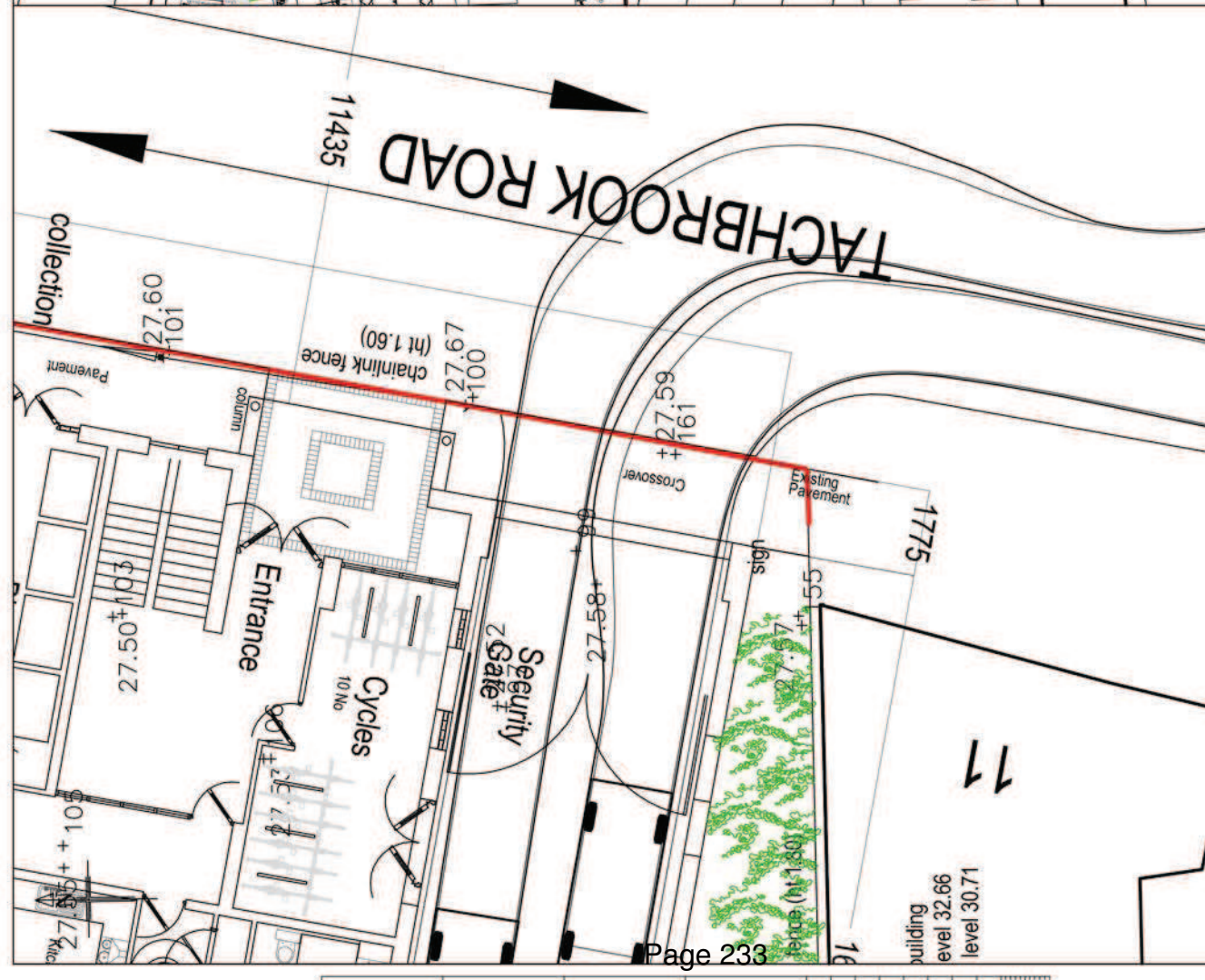



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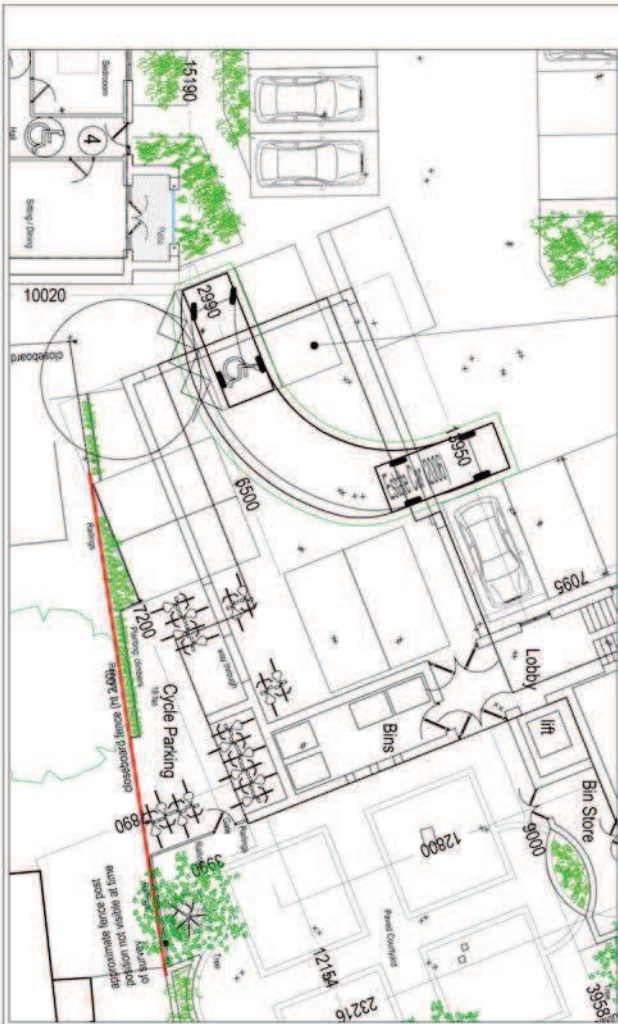
Scale: 1:200

Revision	Date	By	Checked
Copyright Reserved	2014	WYDHA	All Dimensions To Be Checked On Site
Project			
Blues Bar			
Station Road			
West Drayton			
Drawing			
Section / Elevations 1:200			
Wydha Holland Limited			
Unit 1, Millbrook Centre			
Station Road, West Drayton, Middlesex, UK			
Phone +44 (0)1753 729200			
Fax +44 (0)1753 728800			
www.wydha.com			
Date	Scale	Quantity	Sheet No.
03.03.14	1:100	1720	108 B



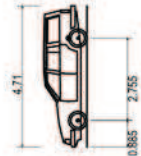
 OPUS Dorchester Office 441 132 2220		Proposed Mixed Use Development Station Road West Drayton		Swept Path Analysis	
1:2 site layout 20/04/2014 10:00 AM	1:200 11/03/14 10:03 AM	1:200 11/03/14 10:03 AM	1:200 11/03/14 10:03 AM	1:200 11/03/14 10:03 AM	1:200 11/03/14 10:03 AM
Project No: J-01132.00 Date: 11/03/14		Project No: 605 Date: R2		Project No: J-01132.00 Date: 11/03/14	

File Path: D:\Projects\2014\11\11435\11435.dwg
 Plot Date: 20/04/2014 12:11:18
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 Plot Scale: 1:200
 Plot Size: 300x300
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 Plot Color: Black
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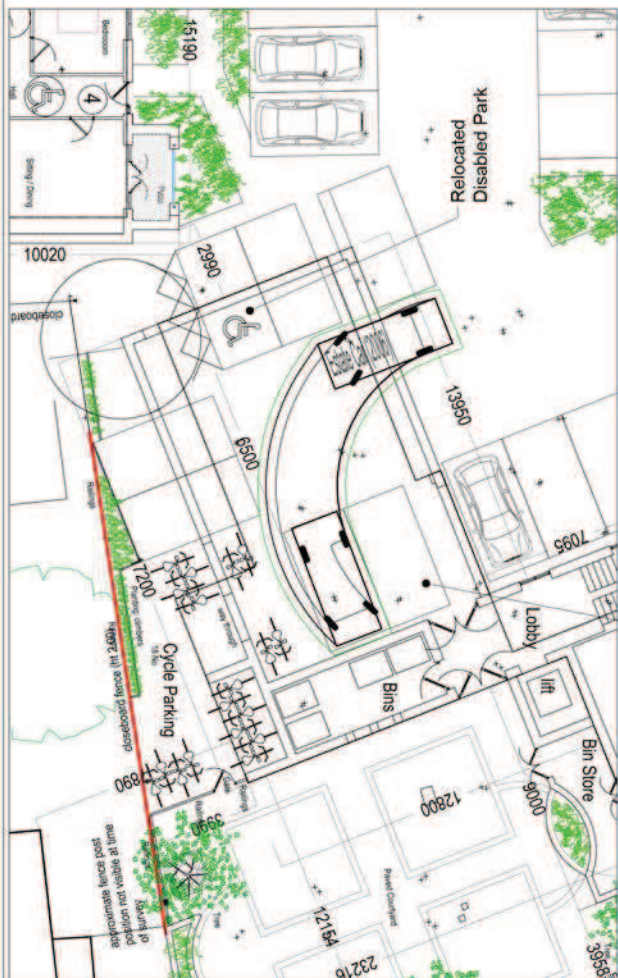


Tracking 2

Moved car parks
250mm South

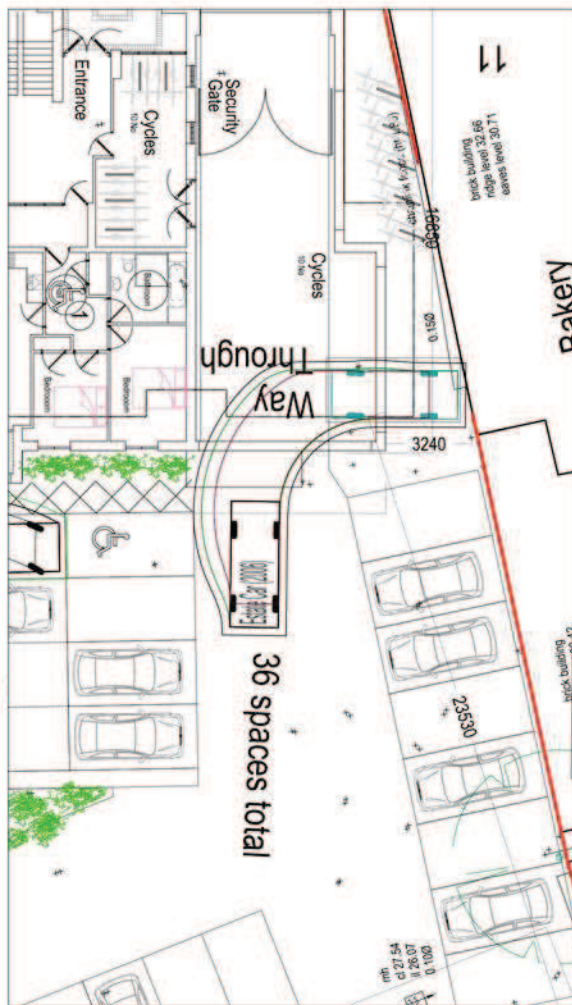


- Estate Car (2006)
- Overall Length 4.710m
- Overall Width 1.804m
- Overall Body Height 1.442m
- Min Body Ground Clearance 0.207m
- Max Track Width 1.756m
- Lock to Lock Time 4.00s
- Kerb to Kerb Turning Radius 5.950m



Tracking 1

Moved car parks 100mm South



Tracking 3

36 spaces total

Blue Bar
Station Road, West Drayton
Swept Path

Table 3: 2007 Parking
Birmingham Office
+44 (0) 121 1506

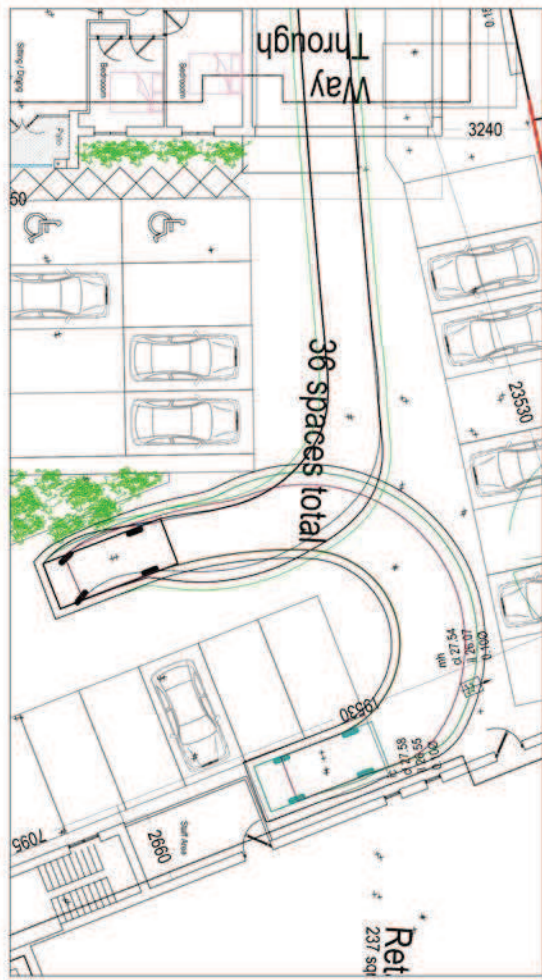
Plan 1

Approved	Author	MFC	Checked	Date
S.R.	M.C.		S.R.	11/09/11

Original Sheet Size: A1 (841x1189) Plot Date: 20 Jul 2014 @ 4:31 PM Path: C:\Projects\2014\Projects\West Drayton\1726 Blue Bar Drayton Tracking 2014 (841x1189)



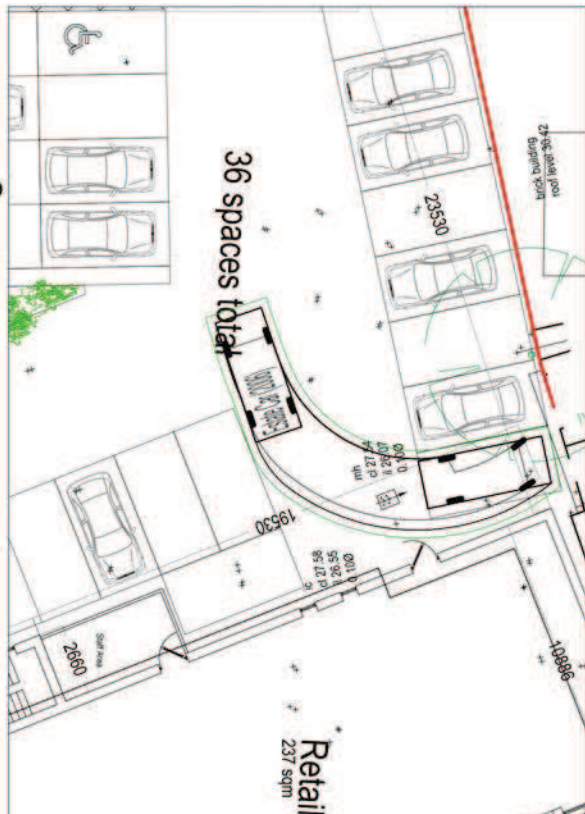
Tracking 5



Tracking 7



Tracking 4



Tracking 6

Blue Bar Station Road, West Drayton
Sweep Path
Plan 2

Scale 3:2000 (As built)
Birmingham Office
+44 (0) 121 521 1500

OPUS
Birmingham Office
+44 (0) 121 521 1500

Discipline	Author	Check	Scale	Date	Revision
Structural	MFC			13/08/14	
MEP					
Architectural					
Services					
Structural					
MEP					
Architectural					
Services					
Structural					
MEP					
Architectural					
Services					

Discipline: Structural
Author: MFC
Check: [blank]
Scale: [blank]
Date: 13/08/14
Revision: [blank]

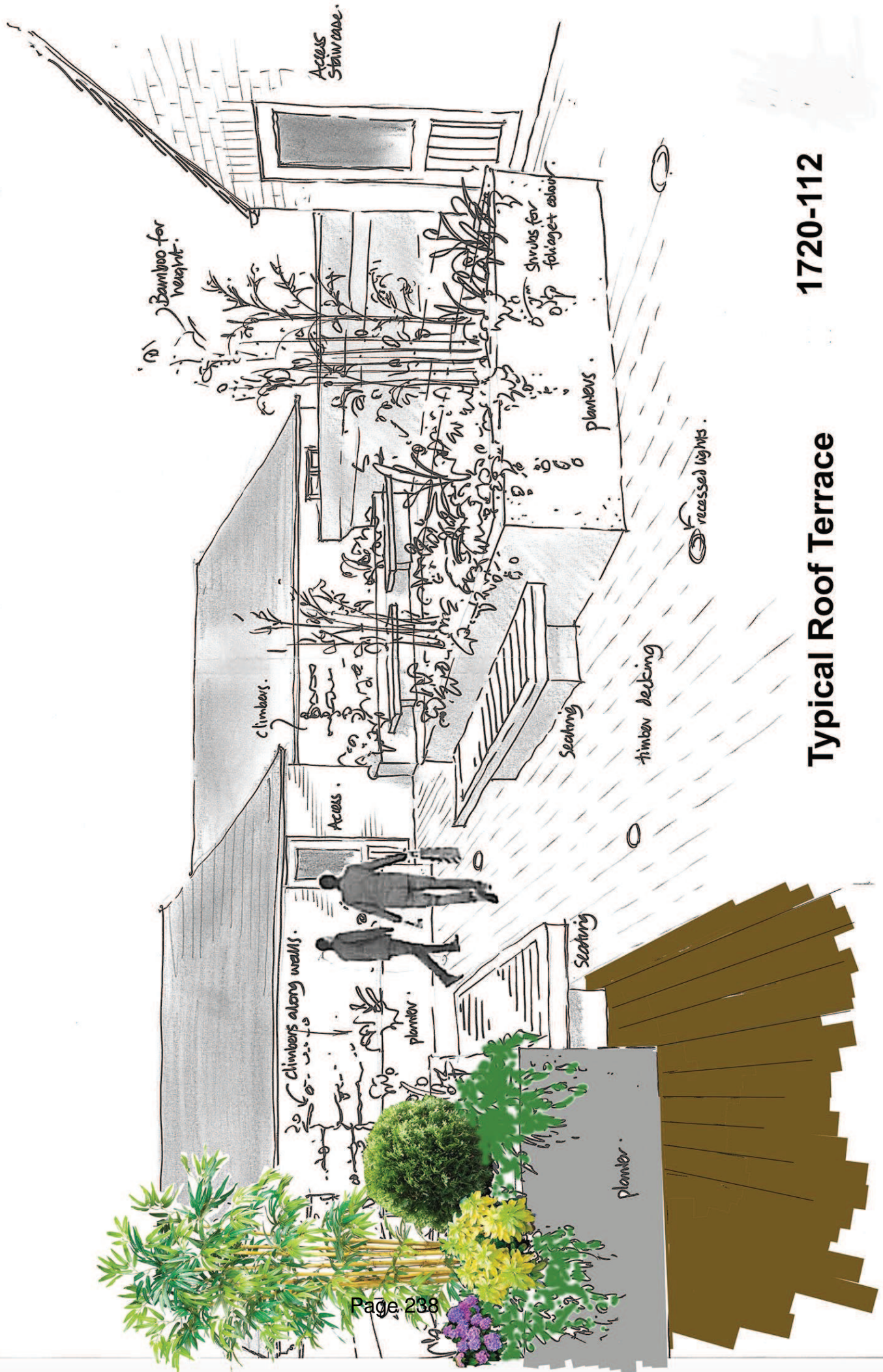


**Sketch of Street Frontage
Blues Bar, Station Road, West Drayton**

1720-110



Sketch From Tachbrook Road 1720-111





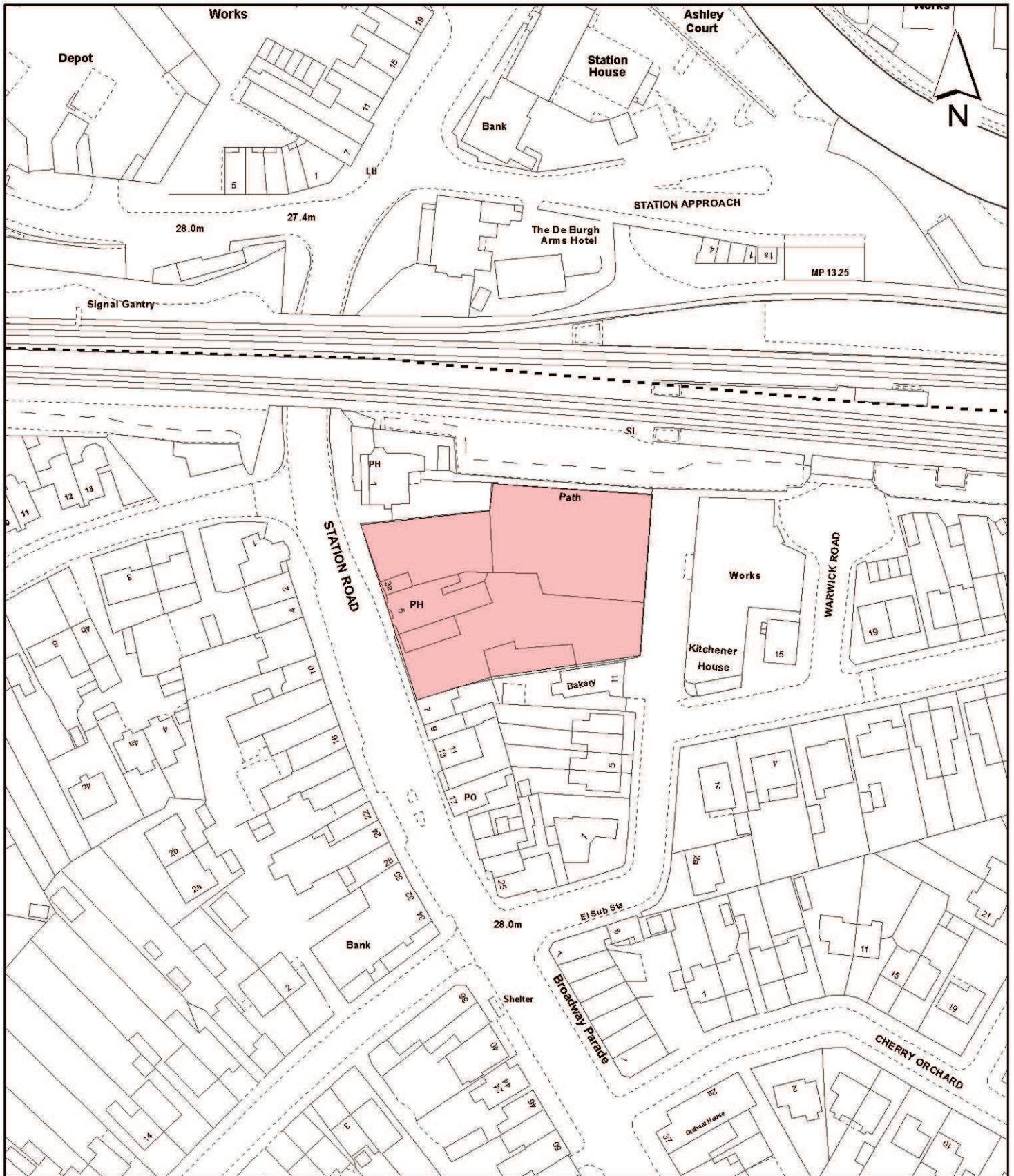
**Sketch of Courtyard
Blues Bar, Station Road, West Drayton**

1720-113

**Parking
Undercroft**

Courtyard

Access



Notes

 Site boundary

For identification purposes only.

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Site Address

**5 Station Road
West Drayton**

**LONDON BOROUGH
OF HILLINGDON**

**Residents Services
Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW
Telephone No.: Uxbridge 250111

Planning Application Ref:

65480/APP/2014/1018

Scale

1:1,250

Planning Committee

Major Page 240

Date

August 2014



HILLINGDON
LONDON